

MEMPHIS INTERNATIONAL AIRPORT
 2491 Winchester Rd, Memphis, TN 38116

TERMINAL A BAGGAGE CAROUSEL REPLACEMENT

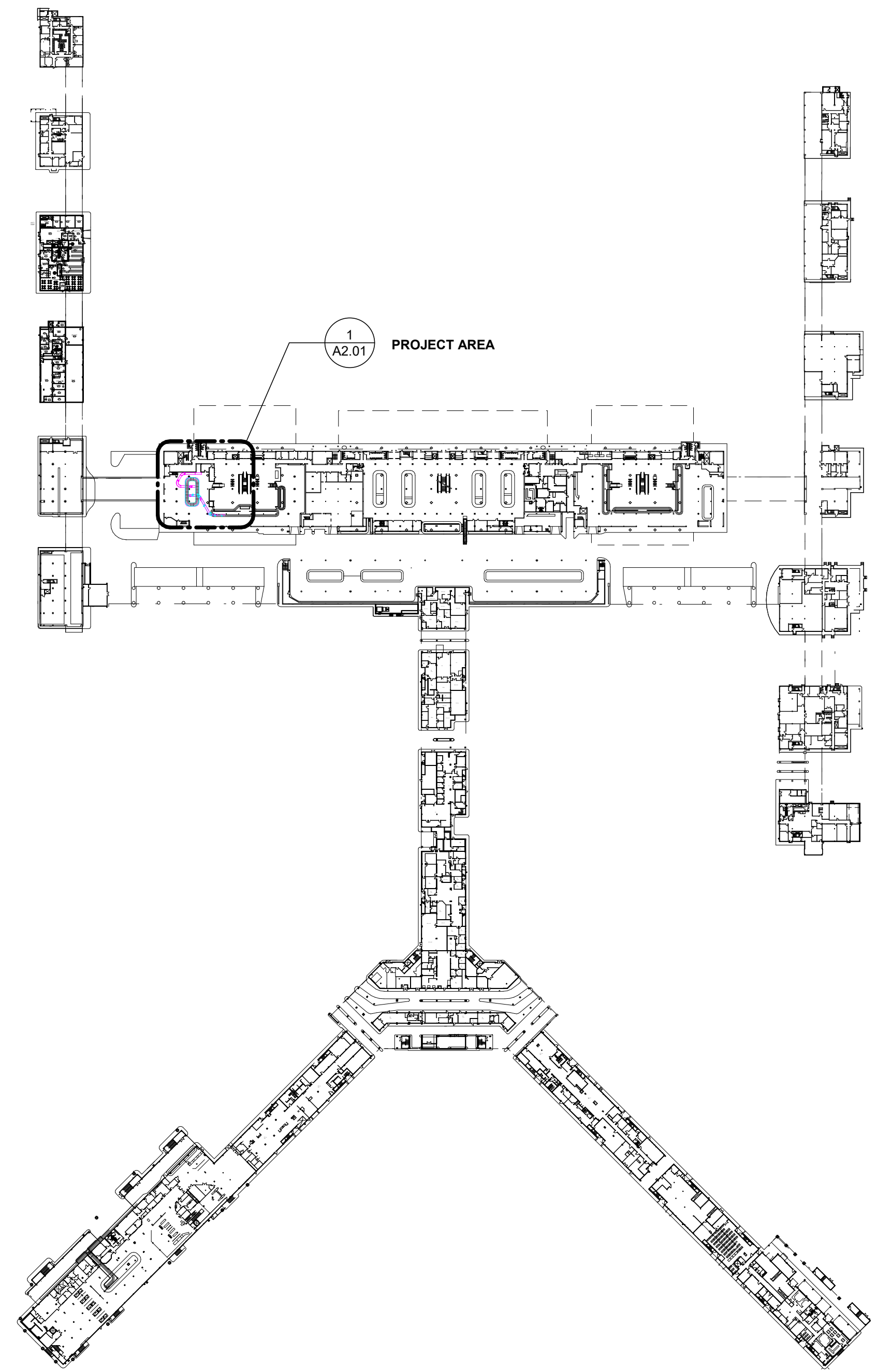
October 04, 2021

07-20-2021

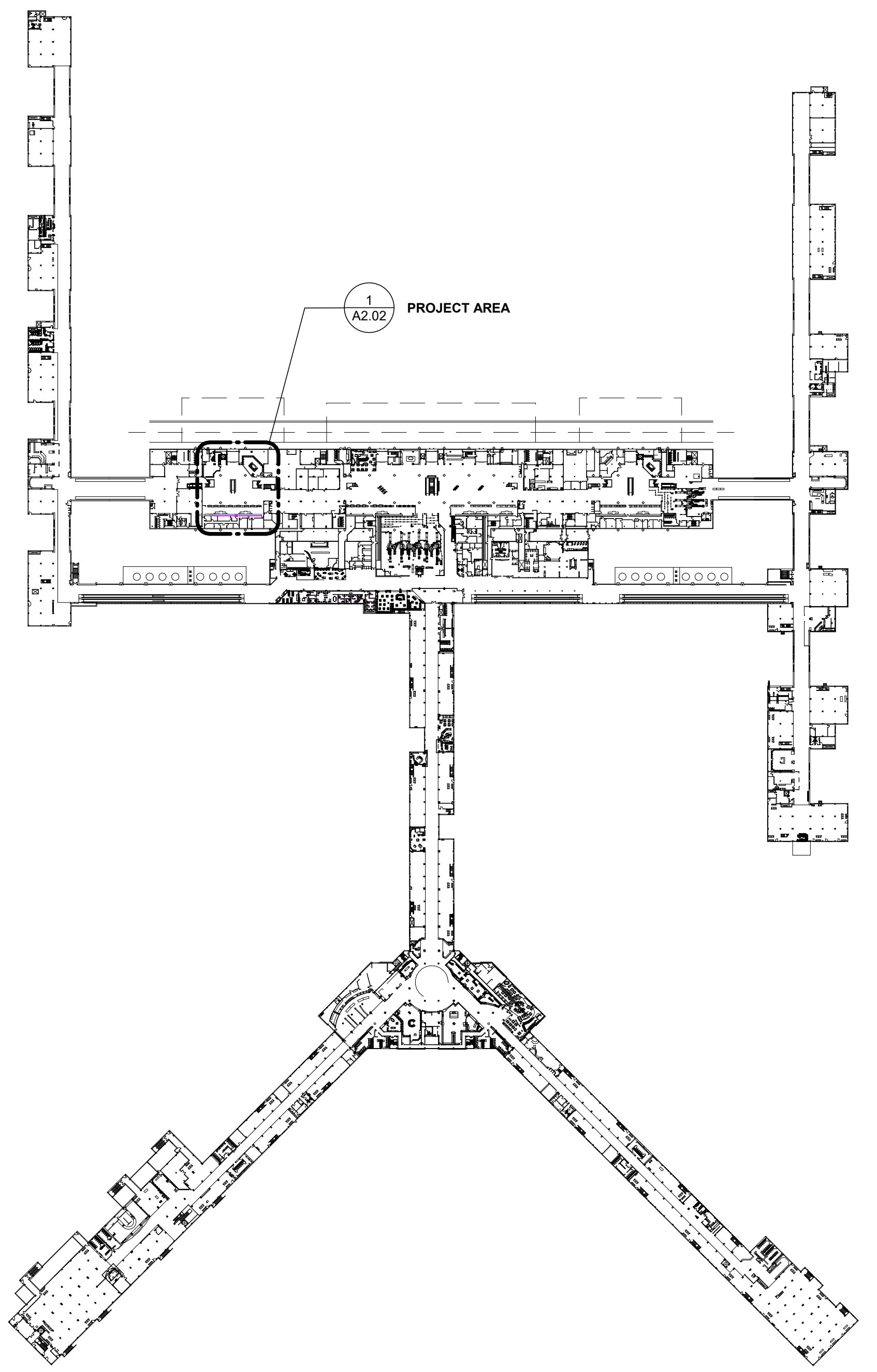
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G0.00	COVER SHEET
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A2.01	APRON LEVEL TERMINAL "A" CAROUSEL
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A001	CONCOURSE A-O/ B NEW CONV PLAN & ELEV
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A002	CONCOURSE A- TICKET COUNTER PLAN

- PROJECT NOTES
- SEE ALSO M DRAWINGS FOR SPECIFICS REGARDING THE DETAILS OF NEW SYSTEM.
 - ALL MATERIAL AND CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF ALL APPLICABLE BUILDING, ELECTRICAL, MECHANICAL, FIRE PROTECTION AND LIFE SAFETY CODE ENFORCED BY LOCAL AUTHORITY. DESIGN BUILD CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, APPROVALS, LICENSES, ETC. FOR ALL TRADES. CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH ALL LOCAL CODES, ORDINANCES, AND REGULATIONS. CONTRACTOR IS RESPONSIBLE FOR FILING AND SECURING ALL NECESSARY PERMITS, APPROVALS, ETC., FOR ALL TRADES.
 - THE CONTRACTOR SHALL PLAN AND SCHEDULE THE PROGRESSION OF THE WORK SO THAT FINISHED AREAS DO NOT HAVE TO BE OPERATED OR OTHERWISE USED AS ROUTES FOR MATERIAL TRANSPORTATION IN ORDER TO REACH REMAINING WORK. SCHEDULE ANY PREPARATION WORK TO MINIMIZE THE RISK OF DAMAGE, COORDINATE WITH OWNER THE SEQUENCE OF DEMOLITION AND CONSTRUCTION PRIOR TO STARTING OF WORK.
 - CONTRACTOR SHALL SCHEDULE THE WORK PER MANUFACTURER RECOMMENDATION AND SPECIFICATION. CONTRACTOR WILL BE FULLY RESPONSIBLE FOR WATER DAMAGE TO THE PROPERTY DUE TO UNFINISHED OPEN WORK.
 - COORDINATE THE COMPATIBILITY OF THE PREPARATION WORK WITH THE REQUIREMENTS OF THE MATERIALS AND PROCEDURES SPECIFIED FOR THE NEW WORK. SCHEDULE ALL PREPARATION WORK TO COINCIDE AS CLOSELY AS PRACTICABLE WITH THE INSTALLATION OF NEW MATERIALS.
 - ANY DAMAGE TO EXISTING FINISH, INCLUDING BUT NOT LIMITED TO WALL FINISHES, FLOORING, ETC., USED AS HAIL ROUTES OR ACCESS ROUTES SHALL BE CONSIDERED THE RESPONSIBILITY OF THE CONTRACTOR AND HE/SHE SHALL PROMPTLY REPAIR ANY DAMAGED TO ITS ORIGINAL CONDITION, TO THE SATISFACTION OF THE OWNER AND ARCHITECT, AT NO ADDITIONAL COST TO THE OWNER.
 - CONTRACTOR MUST FOLLOW THE AIRPORT REQUIREMENTS OF ID BADGING AND ACCESS TO WORK AREA THAT REQUIRE SECURITY CREDENTIALS.
 - VERIFY JOB CONDITIONS PRIOR TO BEGINNING WORK. REPORT TO ENGINEER ANY DISCREPANCIES OR CONDITIONS WHICH MAY INTERFERE WITH SATISFACTORY COMPLETION OF WORK. RESOLVE ANY DISCREPANCIES PRIOR TO BEGINNING WORK. VERIFY ALL FIELD MEASUREMENTS, FIELD CONSTRUCTION, INSTALLATION CRITERIA, COORDINATE ALL TRADES AND THE METHODS OF CONSTRUCTION. ALL WORK IS TO CONFORM WITH DRAWINGS AND SPECS. DRAWINGS ARE NOT TO BE SCALED FOR INFORMATION. CONTRACTOR TO VERIFY THE ACTUAL SIZE FOR PROPER INSTALLATION.
 - ENSURE THE PREMISES ARE MAINTAINED CLEAN AND FREE OF DEBRIS DURING WORK. COORDINATE THE FREQUENCY OF REMOVAL WITH OWNER. FINAL CLEANUP IS THE RESPONSIBILITY OF THE CONTRACTOR. REPAIR, PATCH, TOUCH-UP AND MAINTAIN CLEANING THROUGHOUT THE PROJECT. REMOVE AND DISPOSE OF ALL DEMOLISHED MATERIAL AND OTHER CONSTRUCTION DEBRIS. OWNER'S DUMPSTER(S) OR TRASH CAN(S) SHALL NOT BE FOR DISPOSAL OF ANY CONSTRUCTION WASTE. REPAIR GROUND DAMAGE CAUSED BY TRUCK TRAFFIC.
 - CONTRACTOR TO OVERSEE CLEANING AND ENSURE THAT THE PREMISES ARE MAINTAINED AND SECURED DURING CONSTRUCTION. DAILY CLEANUP AND FINAL CLEANUP IS THE RESPONSIBILITY OF THE CONTRACTOR. REPAIR, PATCH, TOUCH-UP AND MAINTAIN CLEANING UNTIL ACCEPTANCE BY OWNER. OWNER RESERVES THE RIGHT TO REJECT ANY COMPLETED WORK THAT HAS NOT BEEN PROPERLY PROTECTED FROM CONSTRUCTION MATERIAL, UNPROTECTED FOOT TRAFFIC OR ANY CONSTRUCTION ACTIVITY HAS POTENTIALLY COMPROMISED THE INTEGRITY OF THE NEW INSTALLATION.
 - COORDINATE AND OBTAIN APPROVAL FROM OWNER FOR LOCATION OF DUMPSTER, MATERIAL STAGING, ETC.
 - ALL MATERIAL STORED ON SITE SHALL BE PROPERLY PROTECTED TO PREVENT DAMAGE. FAILURE TO PROTECT MATERIAL MAY BE CAUSE FOR REJECTION OF WORK.
 - REMOVE CONSTRUCTION AND GENERAL DEBRIS FROM THE WORK AREA. DISCARD ANY SCREWS, METAL SCRAPS, FOOD, WATER BOTTLES AND MISCELLANEOUS GARBAGE.
 - CONTRACTOR'S FIELD SUPERINTENDENT MUST BE PRESENT AND RESPONSIBLE FOR OVERSEEING THE WORK PROGRESS WHEN ANY WORK IS PERFORMED. FOR THE DURATION OF THE REPAIR PROJECT, CONTRACTOR MUST MAINTAIN AND FURNISH ELECTRONIC DAILY LOG TO PROJECT TEAM AND OWNER DESIGNATED PERSONNEL DAILY. THE LOG DOCUMENTATION SHALL INCLUDE INFORMATION SUCH AS AMOUNT OF WORK COMPLETED, PROGRESS PICTURES, FIELD CHANGES, CONCEALED CONDITIONS, TRANSITION OF MATERIAL, UNFORESEEN DAMAGES, INCIDENTS ETC. THIS INFORMATION SHALL BE INCLUDED AS PART OF THE CLOSEOUT DOCUMENT AT THE END OF THE PROJECT.
 - ALL WORK IS TO CONFORM WITH DRAWINGS AND SPECS. DRAWINGS ARE APPROXIMATE ONLY AND NOT TO BE SCALED FOR INFORMATION. PAYMENT WILL NOT BE MADE TO GENERAL CONTRACTOR ON CHANGES OR EXTRAS UNLESS THEY ARE VERIFIED AND APPROVED BY OWNER.
 - SHOULD CONFLICT OCCUR IN OR BETWEEN DRAWINGS AND SPECIFICATIONS OR WHERE DETAIL REFERENCES OR CONTRACT DRAWINGS HAVE BEEN OMITTED, CONTRACTOR IS DEEMED TO HAVE ESTIMATED THE MOST EXPENSIVE MATERIALS AND CONSTRUCTION INVOLVED UNLESS THEY HAVE ASKED FOR AND OBTAINED WRITTEN PERMISSION FROM ARCHITECT AS TO WHICH MATERIAL OR METHOD WILL BE REQUIRED.
 - NO PORTION OF THE WORK REQUIRING A SHOP DRAWING OR SAMPLE SUBMISSION SHALL BE COMMENCED UNTIL THE SUBMISSION HAS BEEN APPROVED BY THE AIRPORT. ALL SUCH PORTIONS OF THE WORK SHALL BE IN ACCORDANCE WITH THE APPROVED SHOP DRAWINGS AND SAMPLES.
 - CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND EXISTING JOB CONDITIONS AND SHALL REPORT TO ENGINEER ANY DISCREPANCIES OR OMISSIONS WHICH WOULD INTERFERE WITH SATISFACTORY COMPLETION OF WORK. CONTRACTOR IS RESPONSIBLE FOR VERIFICATION OF ALL FIELD MEASUREMENTS, FIELD CONSTRUCTION, INSTALLATION CRITERIA, COORDINATION OF ALL TRADES AND THE METHODS OF CONSTRUCTION.
 - CONTRACTOR SHALL PROVIDE A SCHEDULE OF SHOP DRAWINGS, SAMPLES AND CUT SHEETS TO ENGINEER/OWNER'S APPROVAL PRIOR TO ORDERING OR FABRICATION. CONTRACTOR'S RESPONSIBILITY FOR ERROR OR OMISSIONS IN SUBMITTAL IS NOT RELIEVED BY A REVIEW OF SUBMITTAL. DISRUPTION SHALL BE MINIMAL OUTSIDE PROJECT LIMIT AT ALL TIMES DURING CONSTRUCTION.
 - THE EXACT LIMITS OF THE CONTRACTOR'S STAGING AND STORAGE AREA SHALL BE ESTABLISHED BY THE CONTRACTOR WITH THE APPROVAL OF MSCAA.
 - CONTRACTOR SHALL EXERCISE ALL SAFETY PRECAUTION DURING WORK. DO NOT ALLOW WASTE MATERIAL TO COLLECT IN CONCENTRATED BUILDUPS AROUND THE WORK AREA. FURNISH PROTECTION OF THE BUILDINGS CONTENTS AND PERSONNEL AT ALL TIMES. COLLECT AND REMOVE DEMOLITION AND WASTE MATERIAL FROM THE WORK AREA IN AN ORDERLY MANNER.
 - THE AREAS AROUND THE CONVEYOR REPLACEMENT WORK MAY BE IN USE DURING THE WORK. CONSTRUCTION; THEREFORE, IT IS OF UTMOST IMPORTANCE FOR SAFETY DURING THE WORK. COORDINATE ALL MATERIAL, WORKMEN ACCESS, WITH THE OWNER'S REPRESENTATIVE TO MINIMIZE DISRUPTION OF THE OWNER'S USE OF THE FACILITY AND TO MINIMIZE THE THREAT OF LOSS OR DAMAGE TO THE EXISTING FACILITY, ITS CONTENTS, AND OCCUPANTS. THE CONSTRUCTION SHALL NOT COMPROMISE SECURITY, LIFE SAFETY OR FIRE SAFETY REQUIREMENTS. ANY WORK AFFECTING CODE REQUIREMENTS SHALL BE COMPLETED IN THE SAME WORK DAY OR TEMPORARY MEASURES SHALL BE TAKEN TO ASSURE SAFETY COMPLIANCE AT ALL TIMES.
 - UNLESS NOTED OTHERWISE, CONTRACT TIME FOR THE PROJECT SHALL BE 8 ? WEEKS. SEE TERMS AND CONDITIONS IN PROCUREMENT REQUIREMENT FOR INFORMATION ABOUT LIQUIDATED DAMAGES.
 - ALL WORK THAT WOULD GENERATE EXCESSIVE NOISE SUCH AS DEMOLITION JACK HAMMERING, SAW CUTTING ETC MUST OCCUR AT NIGHT FROM 2200-0500 TO AVOID DISTURBANCE OF THE DAY TIME OPERATION OF THE AIRPORT.
 - SHOULD THERE BE A NEED FOR MASONRY OR CONCRETE CUTTING OR DRILLING, IT IS THE RESPONSIBILITY FOR OF THE CONTRACTOR TO PROTECT BOTH CONSTRUCTION WORKERS AND AIRPORT WORKERS FROM SILICA DUST. CONTRACTORS CAN USE TOOLS FROM TABLE 1 OF THE SILICA STANDARD THAT INVOLVE HEPA VACUUMS OR WET METHODS TO PREVENT SILICA DUST FROM BECOMING AIRBORNE.
 - THE DESIGN-BUILD CONTRACTOR OF THE BELT REPLACEMENT SHALL ENSURE THE FINAL DESIGN AND CONSTRUCTION COMPLIES WITH ALL APPLICABLE OSHA STANDARDS INCLUDING STANDARD OF SAFEGUARDING EQUIPMENT AND PROTECTING EMPLOYEE FROM AMPUTATIONS. CONTRACTOR SHALL ENSURE THE REPLACED SYSTEM DO NOT HAVE ANY PINCH/AMPUTATION POINTS OR PROVIDE AT LEAST A SAFETY SHUT OFF SYSTEM FOR THE MACHINE IF AN EXTREMITY IS BETWEEN THE EQUIPMENT, BAG JAMS, ROLLING DOORS, ETC THE NEW DESIGN AND EQUIPMENT SHOULD BE MARKED APPROPRIATELY ON HOW TO LOG OUT TAG OUT FOR JAMS OR MAINTENANCE REPAIRS.



1 OVERALL FIRST LEVEL FLOOR PLAN-APRON LEVEL
 1" = 160'-0"



2 OVERALL SECOND LEVEL FLOOR PLAN-CONCOURSE
 1" = 160'-0"

PROJECT DESCRIPTION

THE TURN KEY "DESIGN BUILD" PROJECT CONSISTS OF THE SPECIFIC REMOVAL AND REPLACEMENT OF

- A. THE OUTBOUND CAROUSEL SYSTEM AT TERMINAL A AT APRON LEVEL
- B. THE OUTBOUND CONVEYOR BELT SYSTEM BEHIND THE CHECK IN COUNTER AT TERMINAL A AT CONCOURSE LEVEL
- C. ALL ASSOCIATED WORKS FOR NEW CONTROL AND ELECTRICAL INSTALLATION.

SET NO.

ARCHITECT JOB NO.	20008-T10
DRAWN BY	RGI
CHECKED BY	-
APPROVED BY	-
CONSULTANT JOB NO.	---

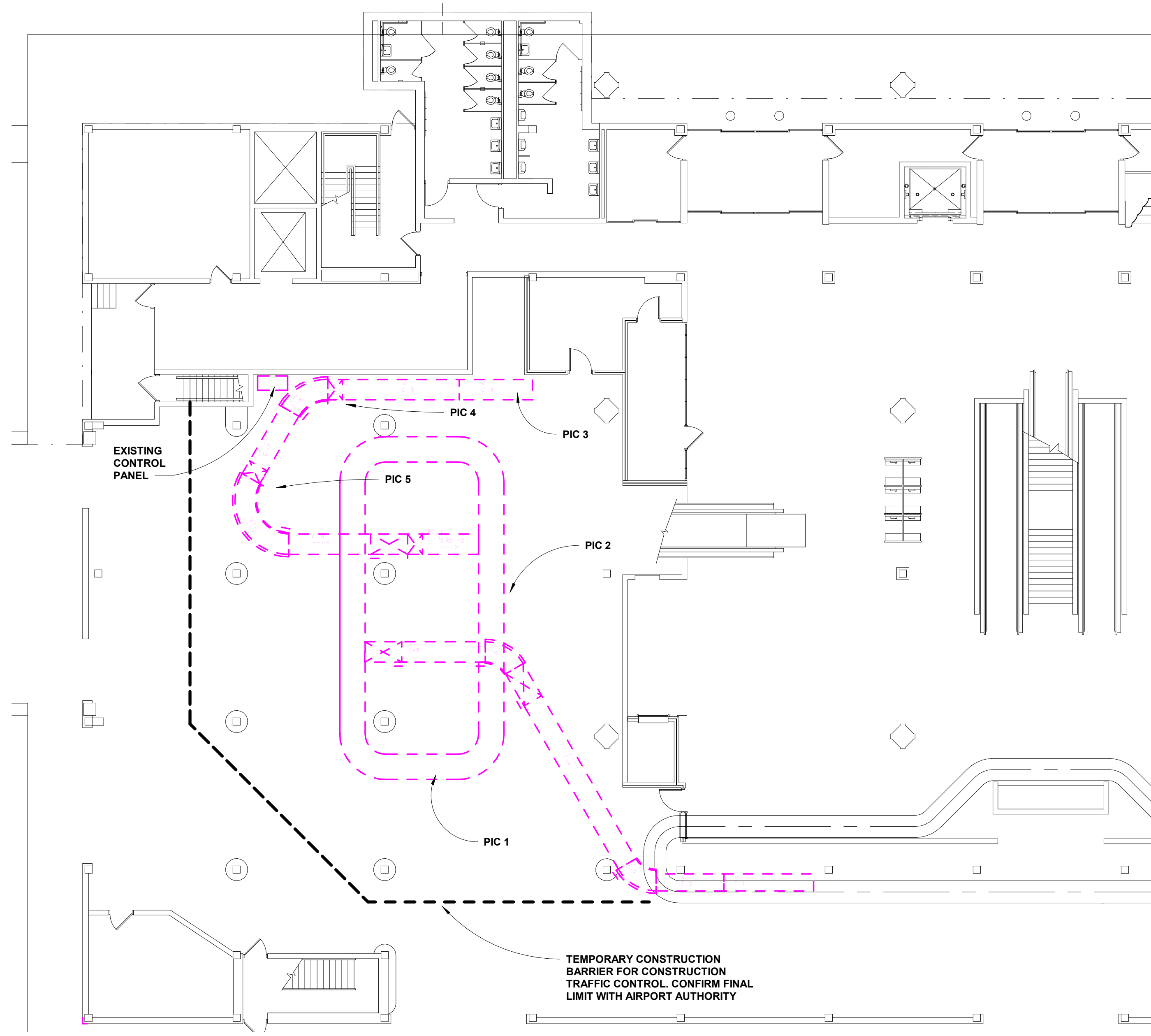
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REVISIONS		
NO.	DESCRIPTION	DATE

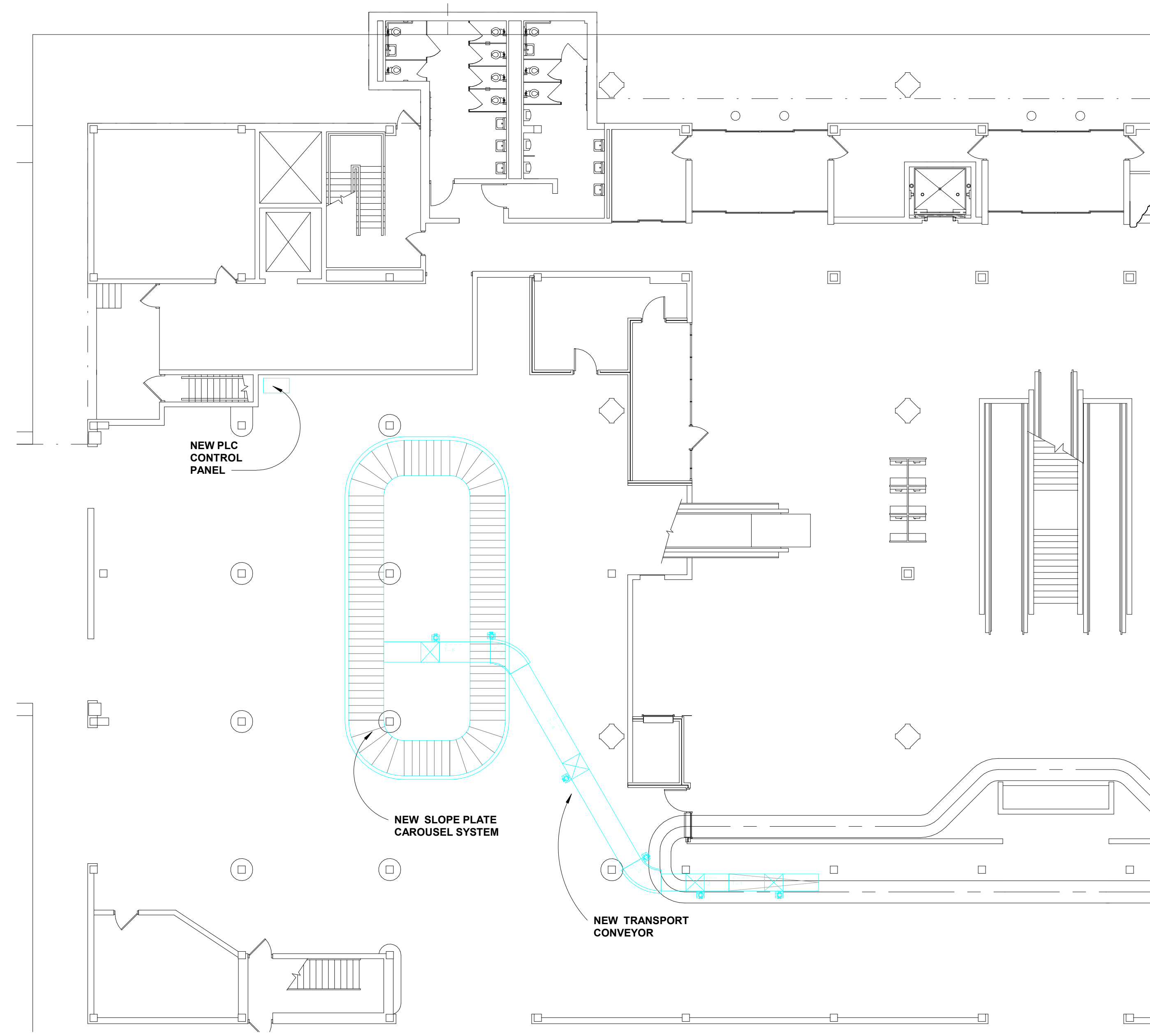
MSCAA No. 19-1423-00 (19-1423-10)
 PROJECT:
**TERMINAL A
 BAGGAGE
 CAROUSEL
 REPLACEMENT**

SHEET TITLE:
COVER SHEET

DATE: 10.04.2021
 SCALE: 1" = 160'-0"
 DWG NO. G0.00



1 TERMINAL A - OUTBOUND CONVEYOR - APRON LEVEL - DEMOLITION
 3/32" = 1'-0"



2 TERMINAL A - OUTBOUND CONVEYOR - APRON LEVEL - NEW
 3/32" = 1'-0"

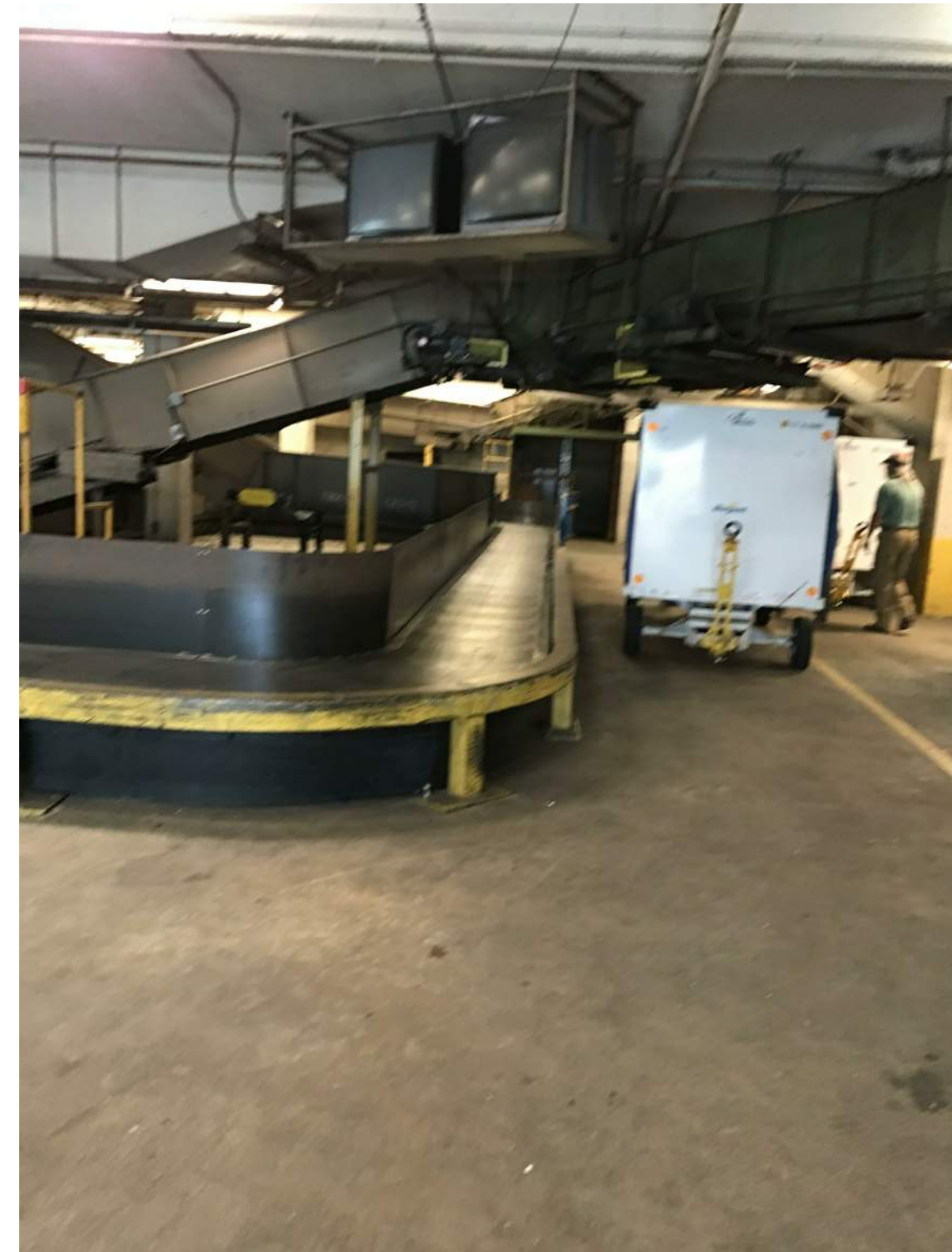
LEGEND

- EXISTING CONVEYOR COMPONENTS TO BE DEMOLISHED
- NEW SLOPED PLATE CONVEYOR SYSTEM. SEE ALSO DRAWING XXX FOR DETAIL

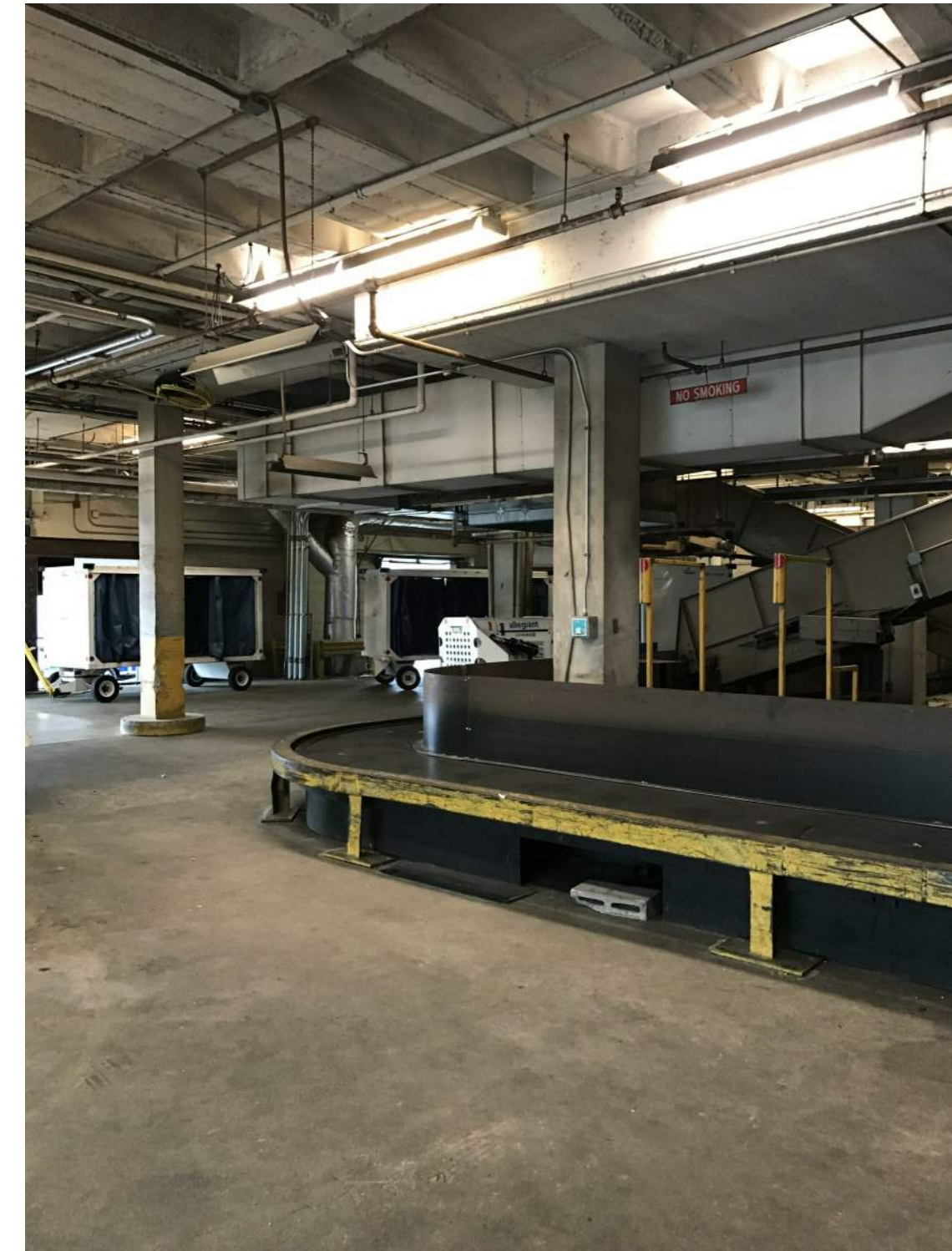
GENERAL NOTES:

- COORDINATE WITH MSCAA FOR ACCESS CONTROL TO THIS WORK AREA.
- ERECT TEMPORARY DUST CONTROL, CONSTRUCTION BARRIER AS NEEDED DURING DEMOLITION AND NEW INSTALLATION FOR CONSTRUCTION TRAFFIC SAFETY. TEMPORARY SAFETY BARRIER MUST BE IN PLACE DURING REPAIR TO SEPARATE CONSTRUCTION TRAFFIC FROM OTHER OPERATION TRAFFIC.
- PROTECT EXISTING BUILDING FROM ANY DAMAGES DUE TO CONSTRUCTION. DO NOT CUT OR CORE INTO CONCRETE WITHOUT SCANNING THE AFFECT AREA.
- COORDINATE SCHEDULE OF REPLACEMENT WORK IN ORDER TO MINIMIZE SERVICE INTERRUPTION.
- PROTECT EXISTING NEARBY LIGHTING FIXTURES AND SPRINKLER SYSTEM. THESE ITEMS TO REMAIN AS IS.
- DEBRIS MUST BE CLEANED UP DAILY.
- CONSTRUCTION HAUL TRAFFIC MUST BE APPROVED BY MSCAA.
- AREA OUTSIDE PROJECT LIMIT MUST BE PROPERLY PROTECTED.
- ANY SENSOR, DETECTOR, ALARM, AND ANY CEILING OR WALL MOUNTED DEVICES AT THE WORK AREA MUST BE PROPERLY PROTECTED FROM CONSTRUCTION ACTIVITIES.
- FIRE AND LIFE SAFETY MUST NOT BE COMPROMISED AT ALL TIMES.
- COORDINATE WITH AIRPORT OPERATION IF TEMPORARY SHUT OFF OF UTILITIES IS NEEDED IN ORDER TO PERFORM THE WORK
- PROTECT AND MAINTAIN ALL EXISTING EGRESS ACCESS UNLESS NOTED OTHERWISE.
- CONTRACTOR TO CONFIRM THE ELECTRICAL CIRCUITS PRIOR TO DEMOLITION AND COMMENCEMENT OF NEW WORK. DO NOT CUT ANY WIRE UNLESS APPROVED BY THE AIRPORT STAFF.
- FINAL CLEAN UP IS THE RESPONSIBILITY OF THE CONTRACTOR.
- PROVIDE SYSTEM TESTING, AND MAINTENANCE TRAINING BEFORE THE CLOSE OUT OF THE PROJECT.

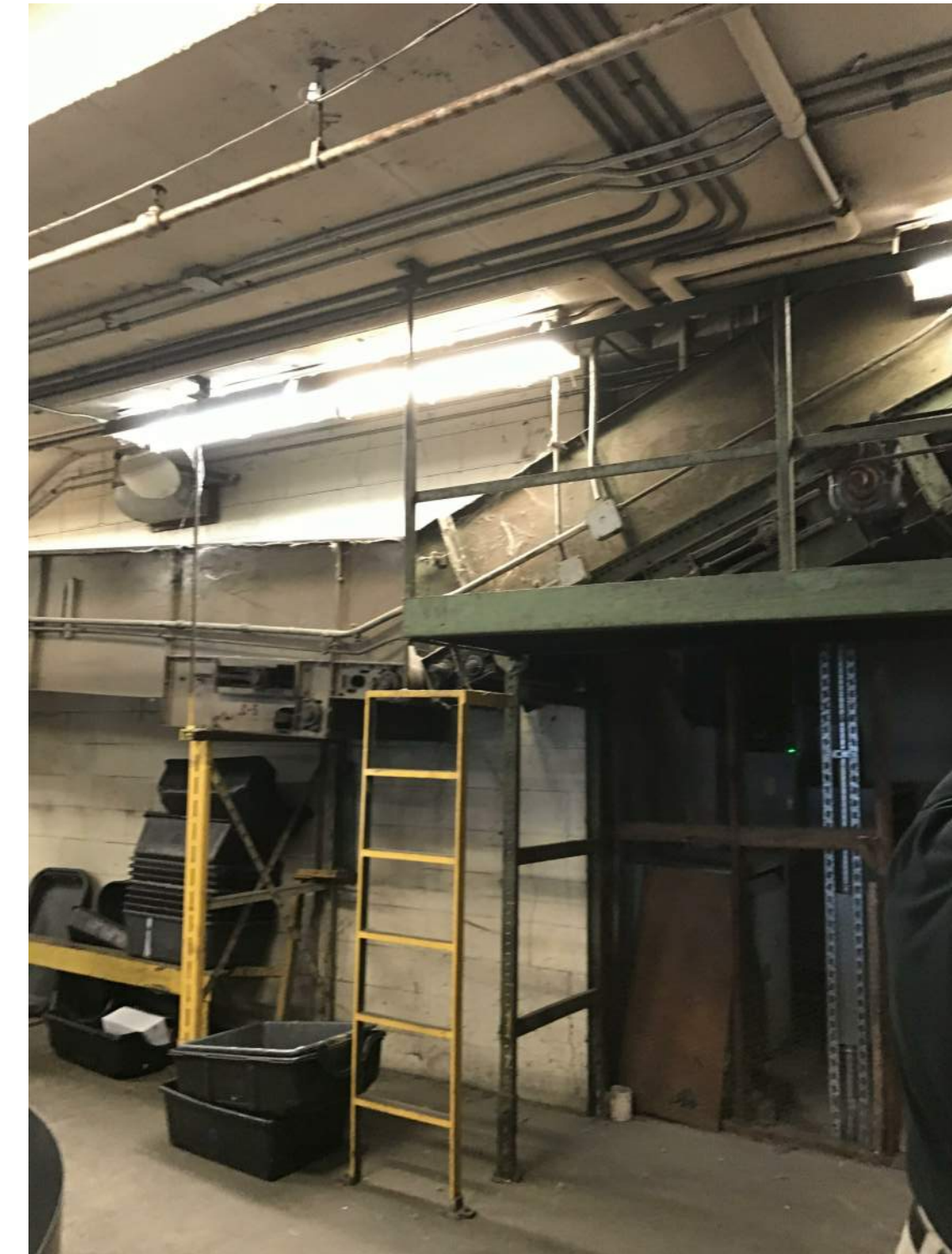
NOTE: SEE MECHANICAL DRAWINGS BY RANEY ENGINEERING AND CONTROL SPECIFICATIONS BY IES FOR ADDITIONAL INFORMATION REGARDING THE SYSTEM



PICTURE 1
 ALL COMPONENT ASSOCIATED WITH THE CURRENT SYSTEM AT THIS AREA TO BE REMOVED COMPLETELY FOR THE INSTALLATION OF THE NEW SYSTEM. PATCH, REPAIR, TOUCH UP CONCRETE SURFACE AS NEEDED. DO NOT MODIFIED ANY EXISTING BUILDING STRUCTURE WITHOUT PRIOR APPROVAL FROM THE AIRPORT. NO CORING OR CUTTING OF BUILDING STRUCTURE IS ALLOWING PRIOR TO SCANNING OF THE CONCRETE.



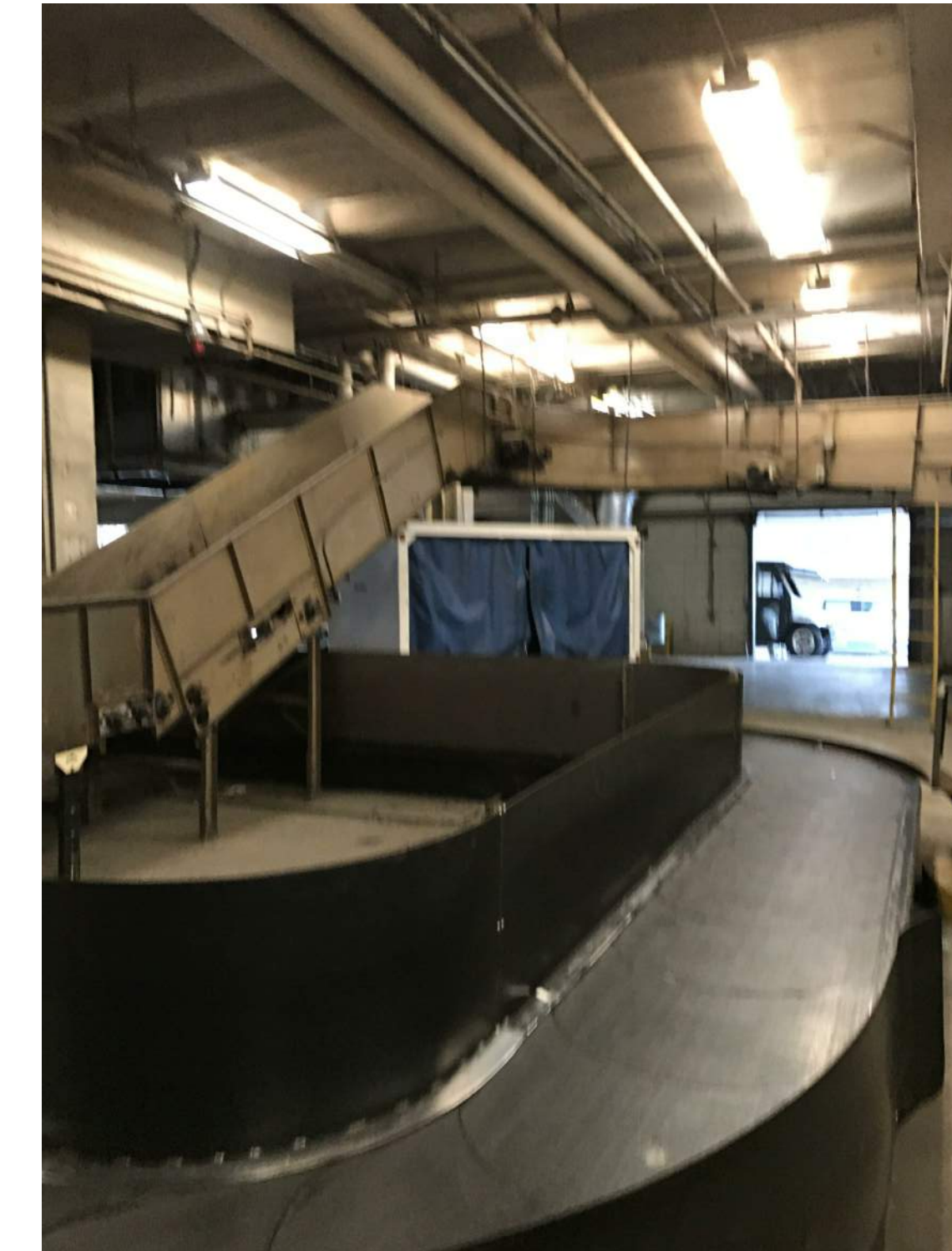
PICTURE 2
 PROTECT EXISTING OVERHEAD DEVICES, LIGHTING FIXTURES AND SPRINKLER, PLUMBING ETC.



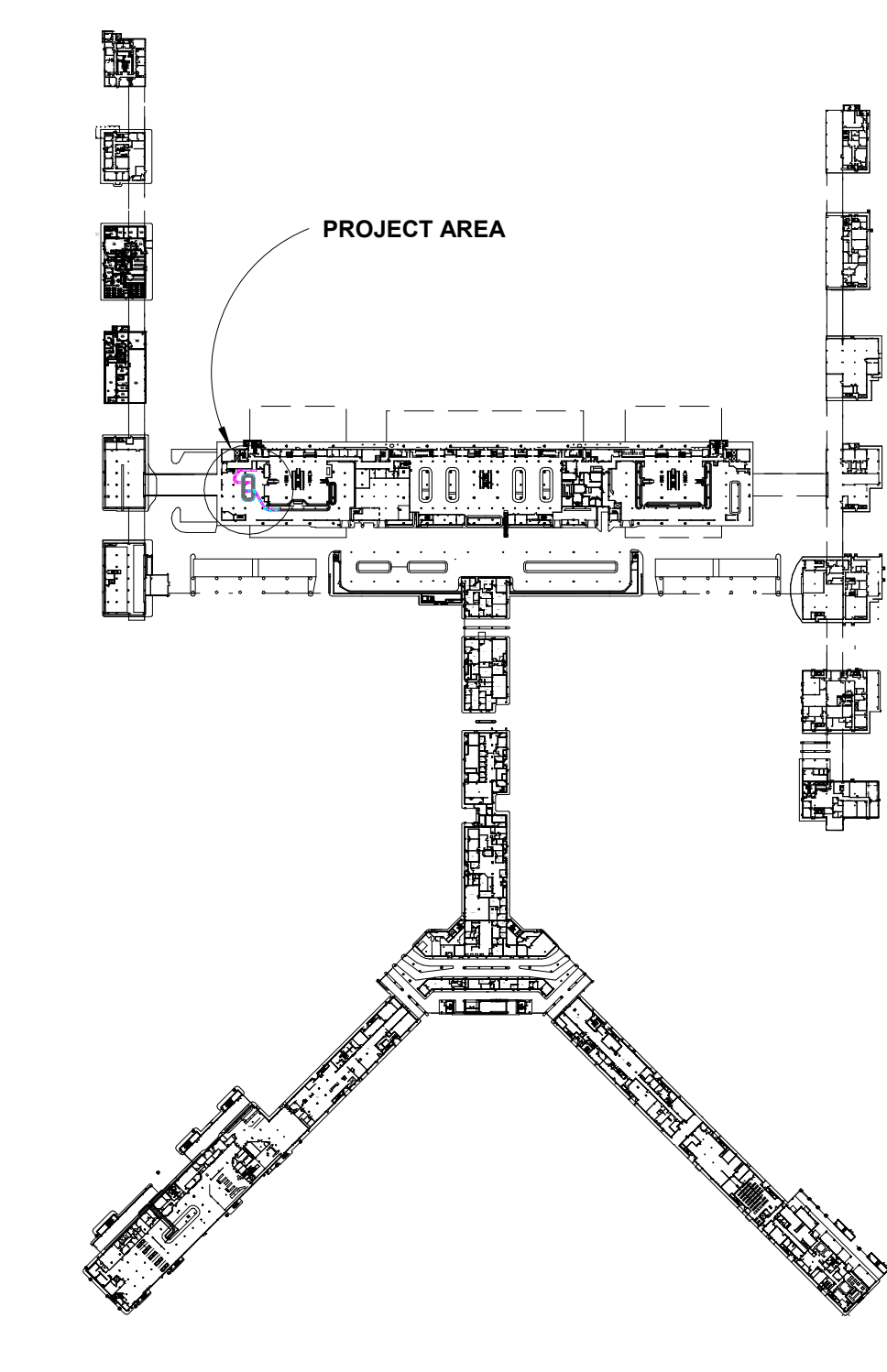
PICTURE 3
 EXISTING CURB SIDE DROP OFF CONVEYOR SYSTEM IS NO LONGER IN OPERATION AND TO BE DECOMMISSIONED AND REMOVED. SEE MECHANICAL DRAWING FOR WORK LIMIT. THE CONTRACTOR SHALL PROVIDE OSHA COMPLIANT FALL PROTECTION BARRIER FOR ANY EXISTING REMAINING PORTION OF CONVEYOR THAT IS NO LONGER IN SERVICE. REMOVE SERVICE PLATFORMS THAT IS NO LONGER NEEDED.



PICTURE 4
 CONTRACTOR SHALL COORDINATE WITH AIRPORT STAFF FOR REMOVAL OF ANY FFE ITEM BELONGS TO AIRLINE OR AIRPORT AUTHORITY. DO NOT CUT OR REMOVE ANY CONDUITS OR PIPING WITHOUT PRIOR CONFIRMATION AND APPROVAL FROM THE AIRPORT AUTHORITY.



PICTURE 5
 CONTRACTOR SHALL COORDINATE WITH AIRPORT STAFF FOR REMOVAL OF ANY FFE ITEM BELONGS TO AIRLINE OR AIRPORT PROPERTY. ANY CURRENT SYSTEM COMPONENTS AND CONTROL THAT IS NO LONGER IN USE SHALL BE REMOVED AND DOCUMENTED.



TAKE OFF WITH US
Memphis
 INTERNATIONAL AIRPORT
 MEMPHIS INTERNATIONAL AIRPORT
 2497 WINCHESTER RD. SUITE 113
 MEMPHIS, TENNESSEE
 38116-9366

RE RANEY ENGINEERING
 INDUSTRIAL CONVEYOR DIVISION
 4857 Normandy Lane,
 Memphis, Tennessee 38117
 901-767-3130
 www.raneyengineering.com

IES
 INNOVATIVE ENGINEERING
 SERVICES, LLC
 2787 Stage Center Dr., Suite 101
 Bartlett, Tennessee 38134
 901-379-0500
 www.innovativeies-llc.com

R
 Renaissance
 Group
 9700 Village Circle, Suite 100
 Lakeland, Tennessee 38002
 901-332-5533
 www.rgroup.biz

ARCHITECT JOB NO.
 20008-T10

DRAWN BY
RGI
 CHECKED BY
 -
 APPROVED BY
 -

CONSULTANT JOB NO.

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REVISIONS		
NO.	DESCRIPTION	DATE

MSCAA NO.
 19-1423-00 (19-1423-10)

PROJECT:
**TERMINAL A
 BAGGAGE
 CAROUSEL
 REPLACEMENT**

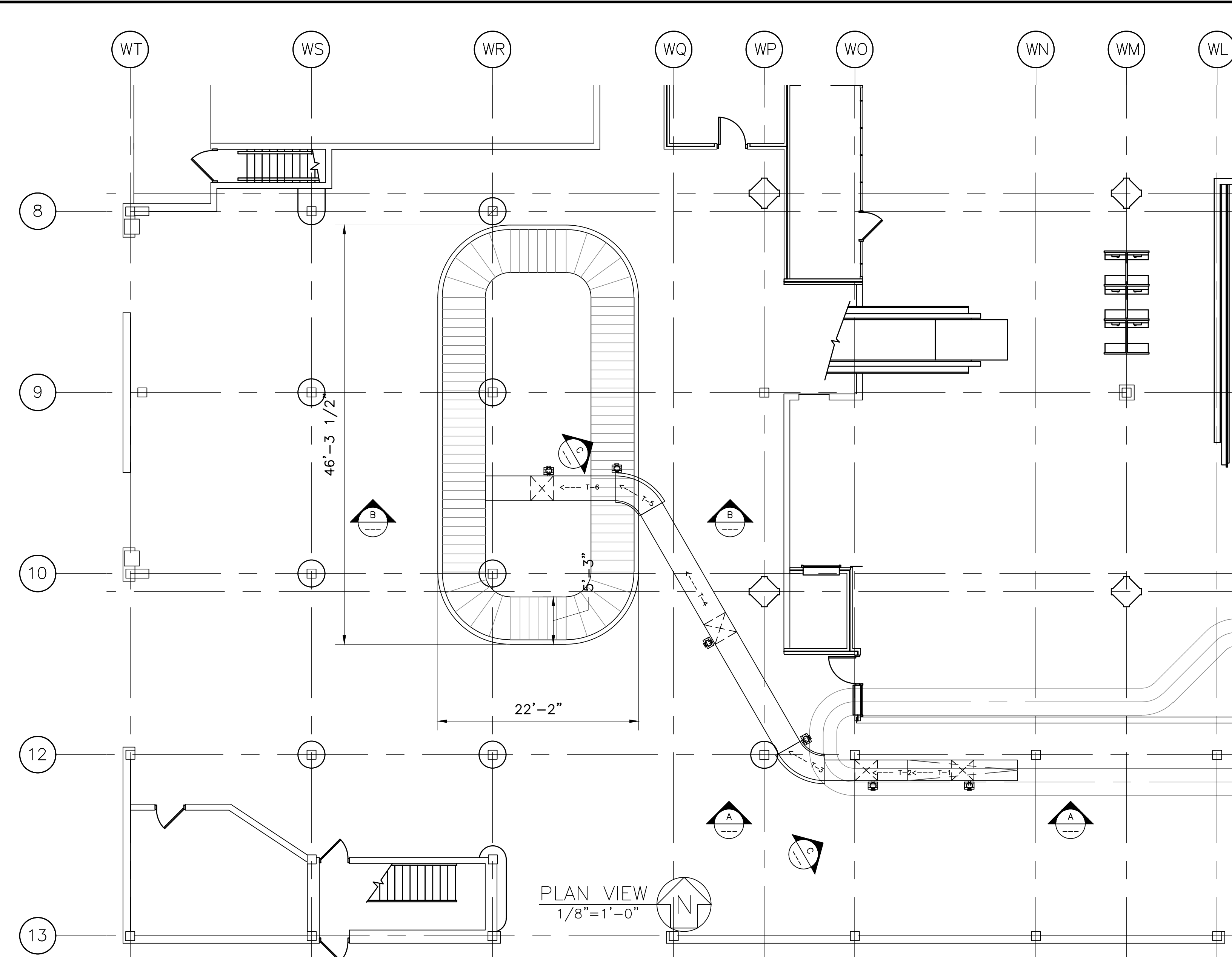
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SHEET TITLE:
**APRON LEVEL
 TERMINAL "A"
 CAROUSEL**

REV. FILE NAME

DATE: 10.04.2021 DWG NO: A2.01
 SCALE: As Indicated

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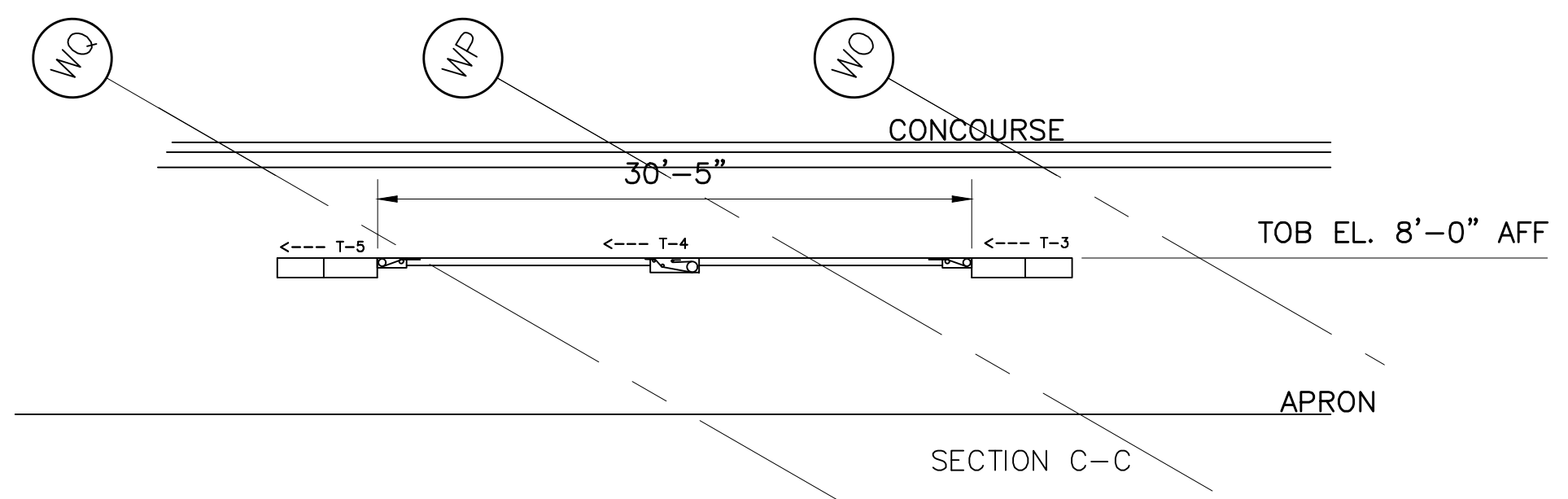


PLAN VIEW
1/8"=1'-0"

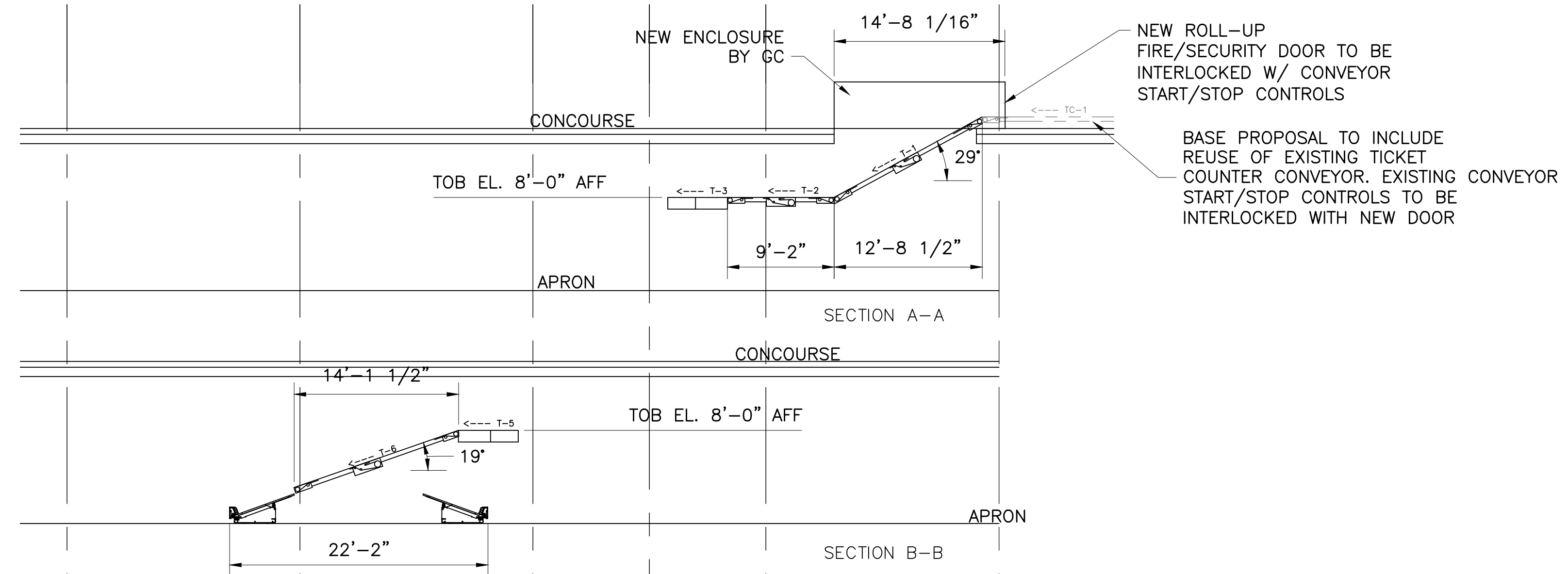
DWG #	DESCRIPTION	REV	ISSUED DATE
A001	CONCOURSE A - O/B NEW CONV PLAN & ELEV	1	07/05/21
A001A	CONCOURSE A - OUTBOUND DEMO PLAN	1	07/05/21
A002	CONCOURSE A - TICKET COUNTER PLAN	1	07/05/21
B001	CONCOURSE B - BAG CLAIM B (BACK, O/S)	1	07/05/21

NEW NAME	TYPE	ANGLE	SLOPE	LENGTH (FT)	ELEV. CHANGE (FT)	BELT WIDTH (IN)	SPEED (FPM)	HP
T-1	Transport	Decline	29 Deg	12.0	-6.5	30	90	1.5
T-2	Transport			9.0		30	100	1.5
T-3	Transport - powerturn			---		30	95	1.5
T-4	Transport			30.5		30	100	2
T-5	Transport - powerturn			---		30	100	1.5
T-6	Transport	Decline	20 Deg	14.5	-5.0	30	100	3
T-7	Slope Plate Makeup carousel			---		66.5	90	5
DOOR #1A	Fire/security door			---		---	---	0.5

- NOTES:
- INSTALL NEW SLOPE PLATE MAKEUP UNIT TO REPLACE DEMOLISHED FLAT PLATE (SEE A001A).
 - INSTALL NEW CONVEYORS FROM TICKET COUNTER TO NEW SLOPE PLATE DEVICE. (CONVEYORS T-1 THROUGH T-6)
 - OPTIONAL TICKET COUNTER REPLACEMENT (SEE A002)
 - ALL NEW CONVEYORS TO BE SUPPORTED WITH NEW SUPPORT STRUCTURE PER RELEVANT BUILDING CODES
 - NEW CONVEYOR INSTALLATION TO REMAIN WITHIN THE CONFINES OF THE EXISTING CONVEYOR FOOTPRINT AND CLEARANCES
 - NEW ROLLUP FIRE/SECURITY DOOR TO BE INSTALLED BY CONVEYOR CONTRACTOR IN NEW ENCLOSURE (BY GC) AT CONCOURSE LEVEL



Product	Manufacturer	
Power units	SEW	Dodge Quantus
Motors	Baldor	
Powerturns	Portec	
Pulleys & rollers	Precision, Inc.	Chantland
Bearings	Dodge	Sealmaster
Beltting	Habisit	
Claim Unit	Siemens	
Fire/security door	Cookson	
Stainless Cladding finish	#4	
Color	Tan or Brown	
Carpet	TBD	
All Products	Alternate manufacturers to be preapproved before	




NEW ROLL-UP FIRE/SECURITY DOOR TO BE INTERLOCKED W/ CONVEYOR START/STOP CONTROLS

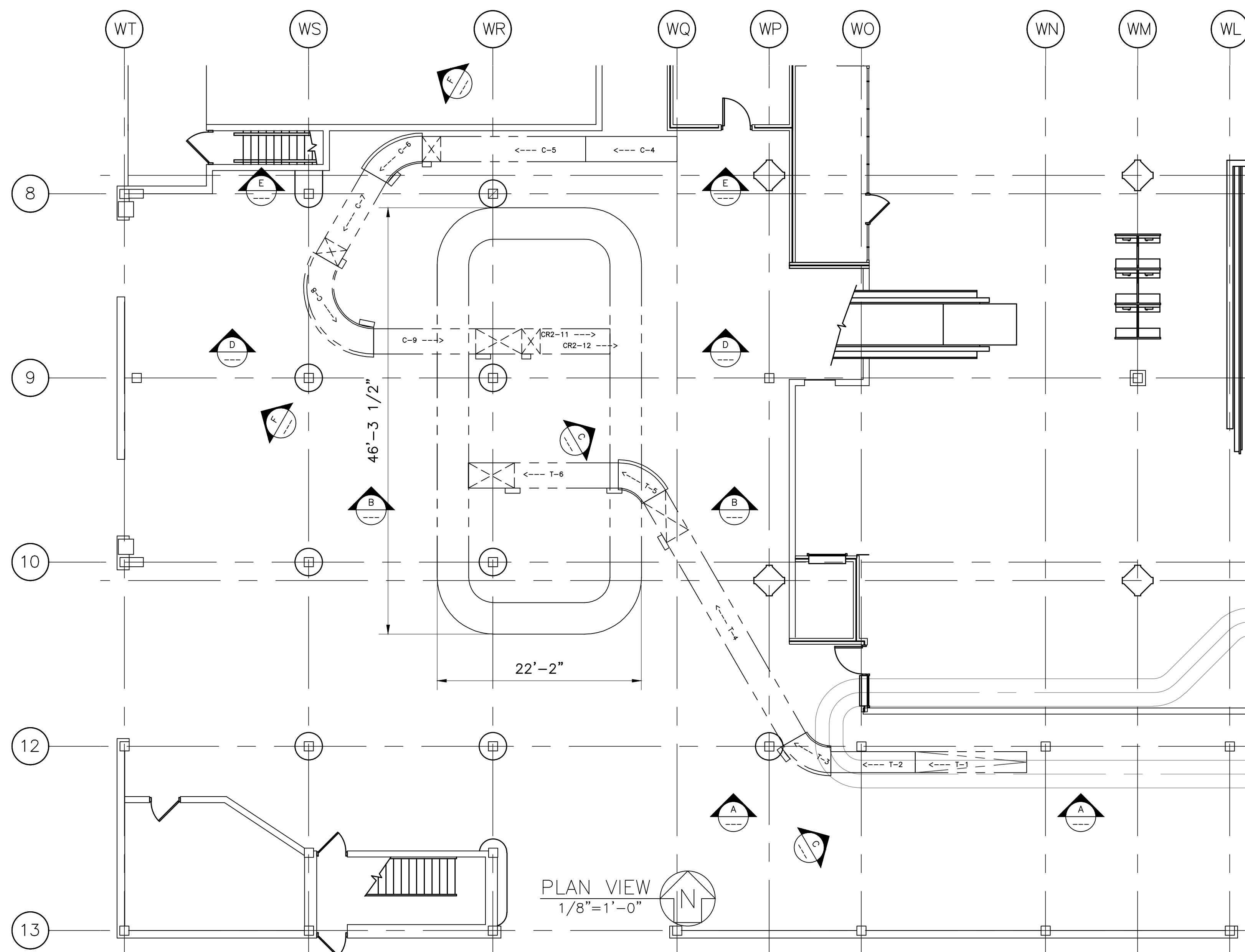
BASE PROPOSAL TO INCLUDE REUSE OF EXISTING TICKET COUNTER CONVEYOR. EXISTING CONVEYOR START/STOP CONTROLS TO BE INTERLOCKED WITH NEW DOOR

ABBREVIATIONS	
GENERAL ABBR	DESCRIPTION
AFF	ABOVE FINISHED FLOOR
BHS	BAGGAGE HANDLING SYSTEM
BLDG	BUILDING
BOS	BOTTOM OF STEEL
BOT	BOTTOM
CL	CENTERLINE
CLR	CLEAR
CONC	CONCRETE
CONT	CONTINUOUS
CONV	CONVEYOR
DWG	DRAWING
EL	ELEVATION
ENGR	ENGINEER
EQUIP	EQUIPMENT
FF	FINISHED FLOOR
FIN	FINISH
I/B	INBOUND
MAINT	MAINTENANCE
MAX	MAXIMUM
MIN	MINIMUM
MCP	MOTOR CONTROL PANEL
N/A	NOT APPLICABLE
NTS	NOT TO SCALE
OA	OVERALL
O/B	OUTBOUND
OPNG	OPENING
REF	REFERENCE
REQ'D	REQUIRED
TBD	TO BE DETERMINED
TEMP	TEMPORARY
TOB	TOP OF BELT
TOC	TOP OF CONCRETE
TOS	TOP OF STEEL

- GENERAL NOTES:
- ALL ELEVATION DIMENSIONS SHOWN ARE ABOVE FINISHED FLOOR
 - CONCOURSE AREA EL. 311'-5"
 - BAGGAGE / CUSTOMER AREA EL. 298'-0"
 - APRON / SECURE AREA EL. 297'-6"
 - [CONV NAME] INDICATES EXISTING CONV. NAME
 - CONV. SUPTS NOT TO EXCEED 10'-0" U.N.O. SUPPORT TO LAND W/IN 1'-0" OF BED SPLICE.
 - SUPPORTS SHOWN FOR REFERENCE ONLY
 - NO DIMENSIONS SHALL BE SCALED. DRAWING DIMENSIONS SHOWN SHALL BE USED.
 - MAINTAIN 2'-6" BAG CLEARANCE, EXCEPT AS NOTED
 - ALL LOAD AND TRANSPORTATION CONVEYORS TO HAVE 21" HIGH SIDE GUARD
 - CONV NAMES TO BE CLEARLY LABELED W/ MIN. 4" TALL LETTERING MIN. TWO LOCATIONS ALONG LENGTH OF CONV.
 - ALL LUBRICATION POINTS AGAINST WALLS SHALL BE FITTED WITH GREASE TUBE TO ALLOW FOR ACCESS FROM THE OPPOSITE SIDE.
 - EACH BELT/CAROUSEL SYSTEM WILL NEED ITS OWN CONTROL PANEL, A TOTAL OF 4 CONTROL PANELS.
 - ALL JAMB RESETS AND ANY OTHER CONTROL BUTTONS SHALL BE VISIBLE AND EASILY ACCESSIBLE FROM THE MAINTENANCE SIDE OF THE EQUIPMENT.

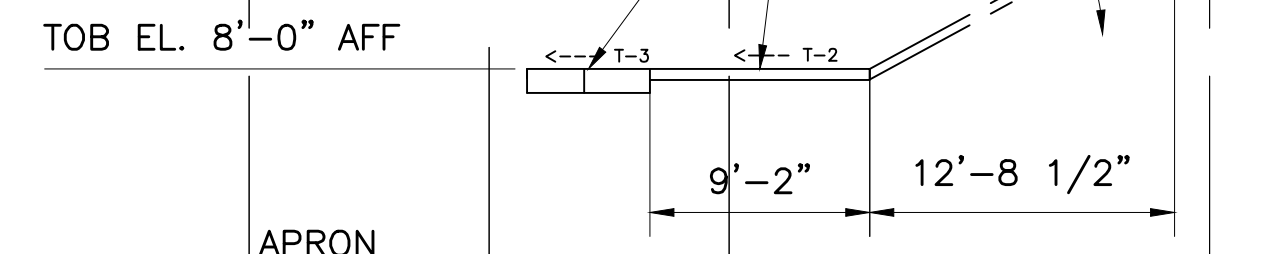
0	ISSUED FOR REVIEW	07/05/21	KHR
#	REVISION DESCRIPTION	REV. DATE	BY

OWNER: MEMPHIS SHELBY COUNTY AIRPORT AUTHORITY	PROJ. NO. 006-002					
LOCATION: MEMPHIS INTERNATIONAL AIRPORT 2491 WINCHESTER ROAD MEMPHIS, TN 38116	DATE: 06/29/21 SCALE: 1/8"=1'-0"					
SUBJECT: CONCOURSE A - O/B NEW CONV PLAN & ELEV	DR. BY: RELCC CK. BY:	<table border="1"> <tr> <td>SHEET #</td> <td>REV.</td> </tr> <tr> <td>1 of 3</td> <td>0</td> </tr> </table>	SHEET #	REV.	1 of 3	0
SHEET #	REV.					
1 of 3	0					

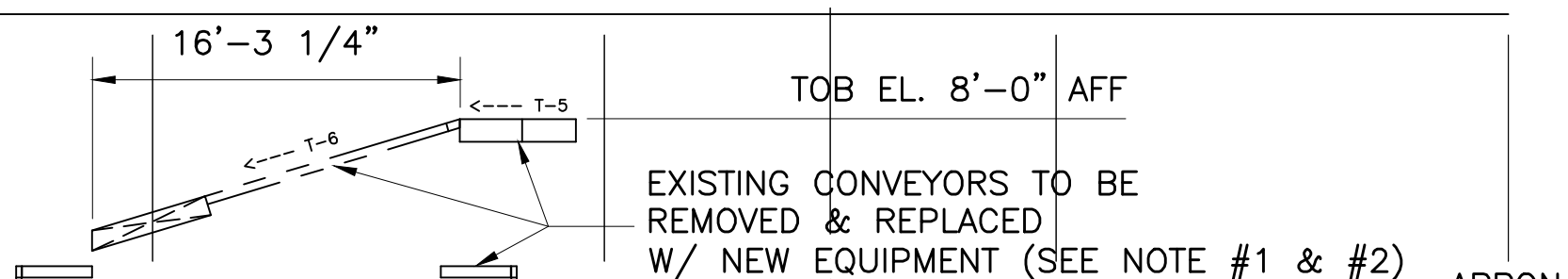


PLAN VIEW
1/8"=1'-0"

EXISTING CONVEYORS TO BE REMOVED & REPLACED W/ NEW EQUIPMENT (SEE NOTE #2)

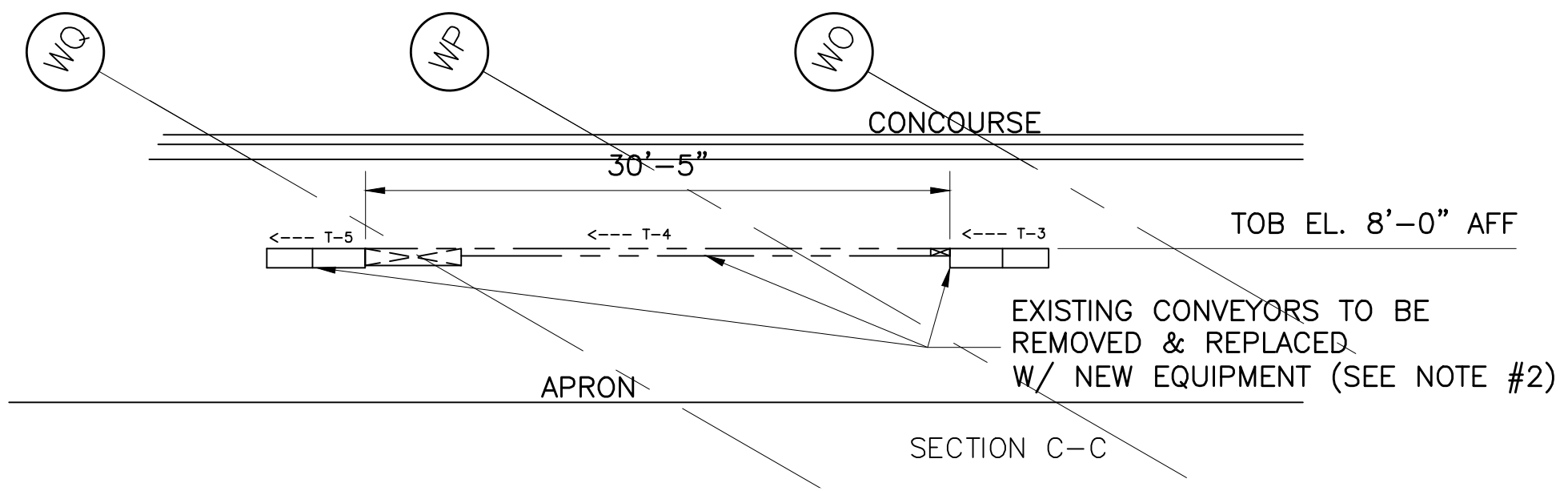


SECTION A-A

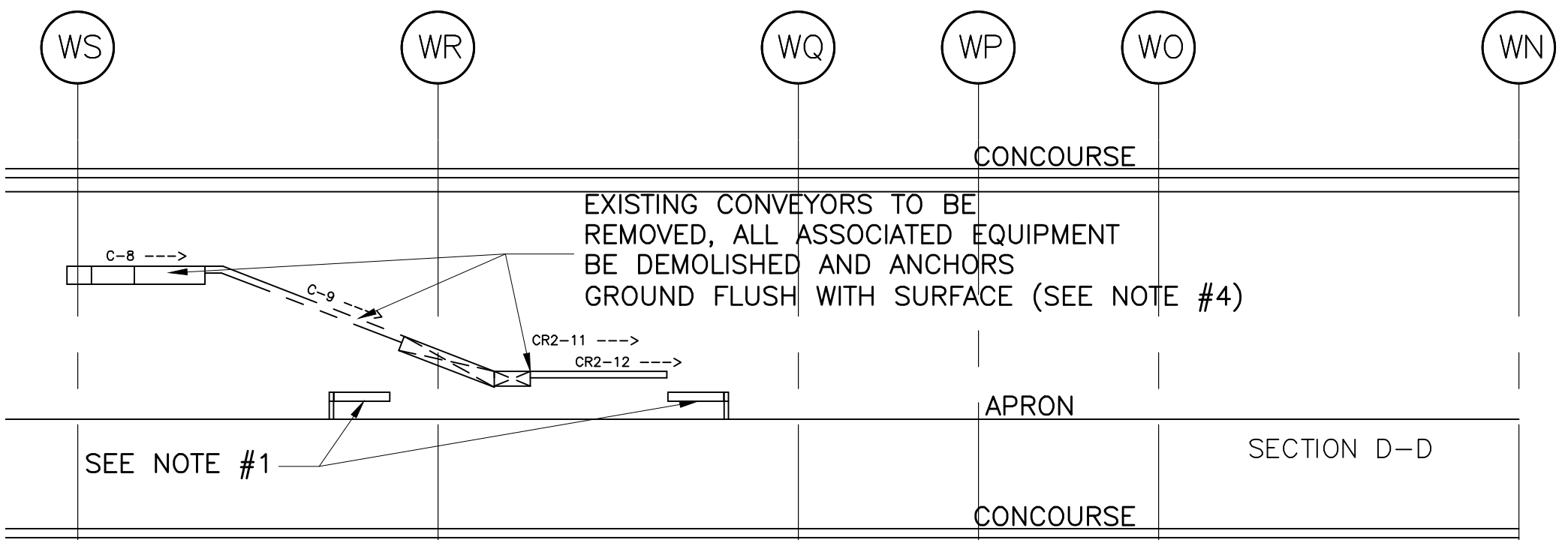


SECTION B-B

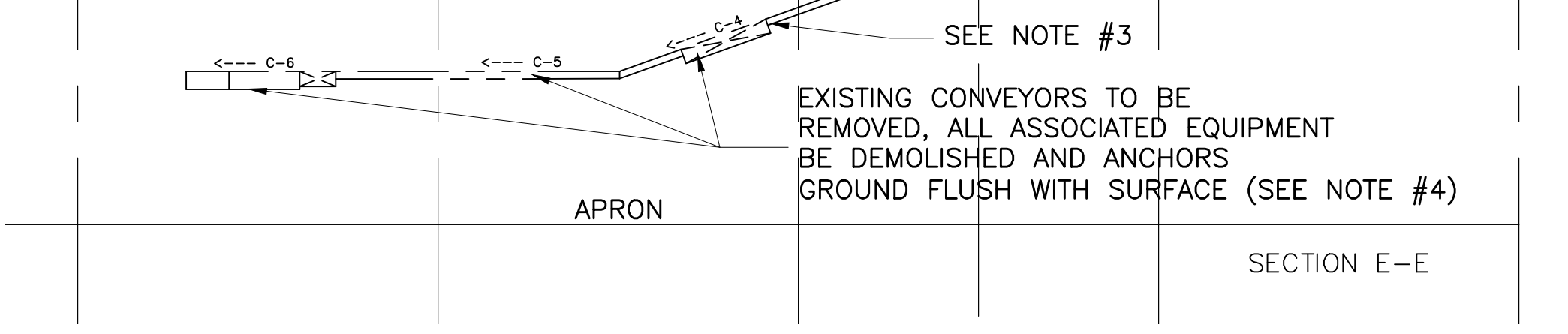
- NOTES:
1. REMOVE EXISTING FLAT PLATE MAKEUP DEVICE.
 2. REMOVE EXISTING CONVEYORS FROM EXISTING TICKET COUNTER CONVEYOR TO FLAT PLATE MAKE UP UNIT. (CONVEYORS T-1 THROUGH T-6)
 3. REMOVE CONVEYOR C-4 FROM FIRST SUPPORT AFTER EXIT FROM CEILING TO CONVEYOR C-5 (DRIVE AND TAIL). THIS IS TO INCLUDE PLATFORM.
 4. REMOVE CONVEYORS C-5, C-6, C-7, C-8, C-9, CR2-11 & CR2-12.



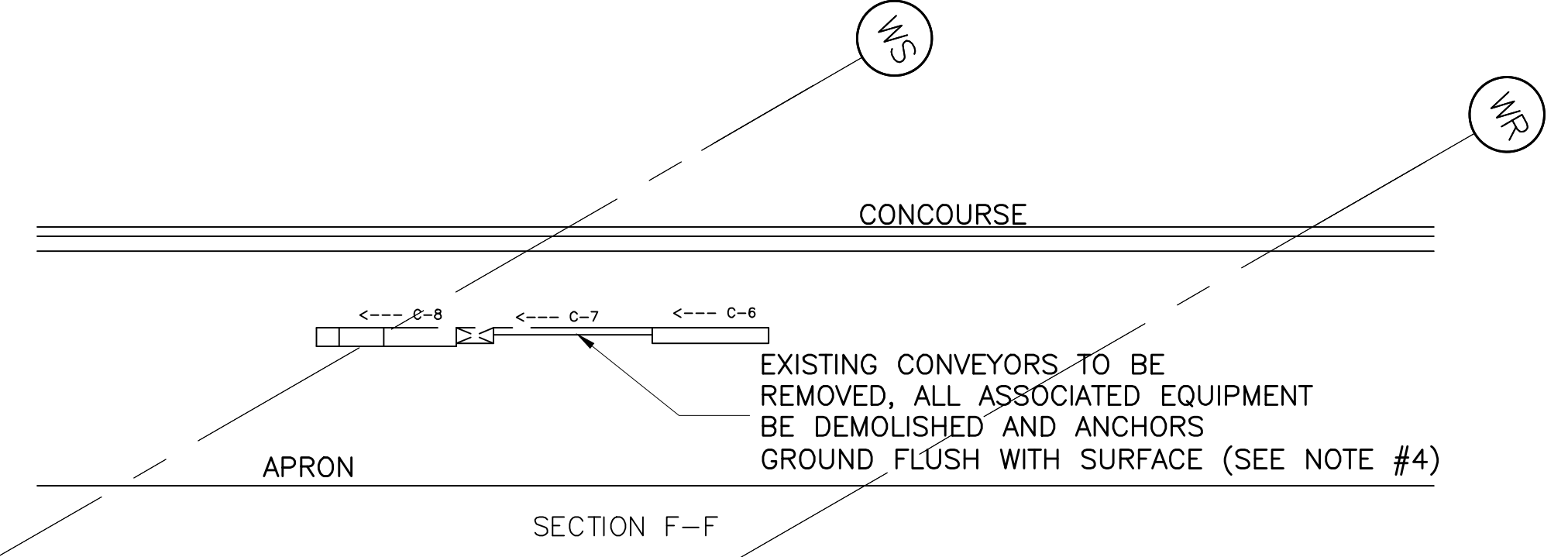
SECTION C-C



SECTION D-D



SECTION E-E



SECTION F-F

- GENERAL NOTES:
1. ALL ELEVATION DIMENSIONS SHOWN ARE ABOVE FINISHED FLOOR
 - 1.1. CONCOURSE AREA EL. 311'-5"
 - 1.2. BAGGAGE / CUSTOMER AREA EL. 298'-0"
 - 1.3. APRON / SECURE AREA EL. 297'-6"
 2. [CONV NAME] INDICATES EXISTING CONV. NAME
 3. CONV. SUPTS NOT TO EXCEED 10'-0" U.N.O. SUPPORT TO LAND W/IN 1'-0" OF BED SPLICE.
 - 3.1. SUPPORTS SHOWN FOR REFERENCE ONLY
 4. NO DIMENSIONS SHALL BE SCALED. DRAWING DIMENSIONS SHOWN SHALL BE USED.
 5. MAINTAIN 2'-6" BAG CLEARANCE, EXCEPT AS NOTED
 6. ALL LOAD AND TRANSPORTATION CONVEYORS TO HAVE 21" HIGH SIDE GUARD
 7. CONV NAMES TO BE CLEARLY LABELED W/ MIN. 4" TALL LETTERING MIN. TWO LOCATIONS ALONG LENGTH OF CONV.
 8. ALL LUBRICATION POINTS AGAINST WALLS SHALL BE FITTED WITH GREASE TUBE TO ALLOW FOR ACCESS FROM THE OPPOSITE SIDE.
 9. EACH BELT/CAROUSEL SYSTEM WILL NEED ITS OWN CONTROL PANEL, A TOTAL OF 4 CONTROL PANELS.
 10. ALL JAMB RESETS AND ANY OTHER CONTROL BUTTONS SHALL BE VISIBLE AND EASILY ACCESSIBLE FROM THE MAINTENANCE SIDE OF THE EQUIPMENT.

0	ISSUED FOR REVIEW	07/05/21	KHR
#	REVISION DESCRIPTION	REV. DATE	BY
OWNER: MEMPHIS SHELBY COUNTY AIRPORT AUTHORITY		PROJ. NO. 006-002	
LOCATION: MEMPHIS INTERNATIONAL AIRPORT 2491 WINCHESTER ROAD MEMPHIS, TN 38116		DATE: 06/29/21 SCALE: 1/8"=1'-0"	
SUBJECT: CONCOURSE A - O/B DEMO PLAN & ELEV		DR. BY: RELLC CK. BY:	SHEET # 2 of 2 REV. 0 DWG. NO. A001A



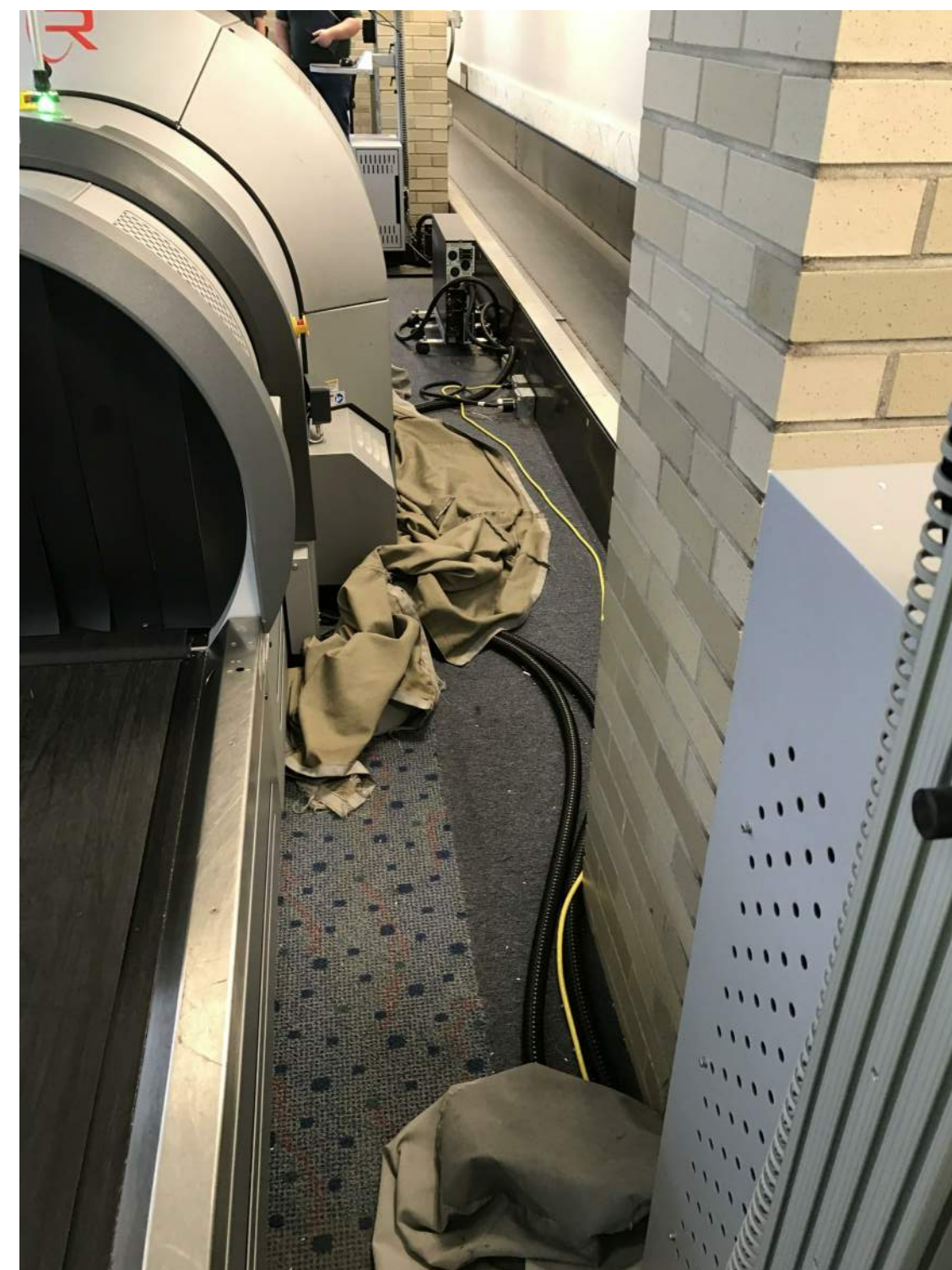
PICTURE 1
 DESIGN BUILD CONTRACTOR TO PROTECT EXISTING FLOOR DURING REPLACEMENT. CONSTRUCT TEMPORARY BARRIER UNTIL PROJECT COMPLETION. COORDINATE WITH AIRPORT FOR TEMPORARY RELOCATION OF ANY EQUIPMENT. IT IS THE RESPONSIBILITY OF THE DESIGN BUILD CONTRACTOR TO PROTECT EXISTING DEVICES FROM DAMAGE DUE TO CONSTRUCTION ACTIVITIES. BLOCK OR FILTER RETURN AIR IF EXCESSIVE DUST WILL BE GENERATED BY THE WORK.



PICTURE 2
 REMOVE AND REPLACE THE ENTIRE BELT SYSTEM AND ALL ASSOCIATED TRIMS. THE SCOPE OF WORK SHALL INCLUDE REPAINTING OF THE ENTIRE WALL BEHIND THE NEW REPLACEMENT.



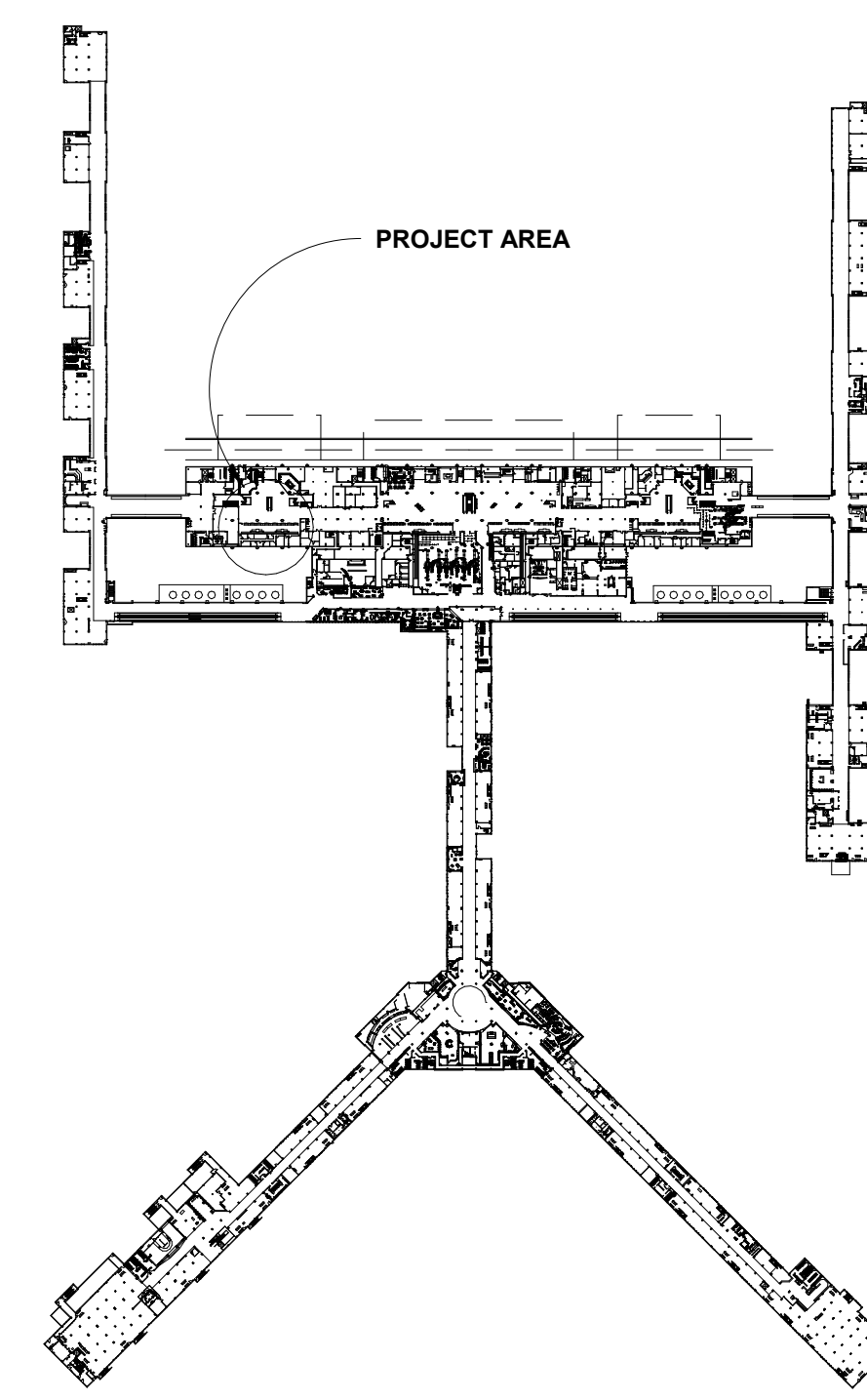
PICTURE 3
 DESIGN BUILD CONTRACTOR TO FIELD VERIFY THE EXISTING CONSTRAINTS IMPOSED BY THE EXISTING BUILDING STRUCTURE. DO NOT MODIFY ANY EXISTING BUILDING STRUCTURE WITHOUT AIRPORT APPROVAL.



PICTURE 4
 DESIGN BUILD CONTRACTOR TO FIELD VERIFY AND COORDINATE THE ELECTRICAL CONNECTION OF NEW AND EXISTING ELECTRICAL CIRCUIT AND CONTROLS



PICTURE 5
 THE SCOPE OF WORK FOR DESIGN BUILD CONTRACTOR SHALL INCLUDE THE REPLACEMENT OF THE SECURITY ROLLUP GATE AS WELL AS THE INCLINED BELT AS PART OF THE BELT REPLACEMENT PACKAGE AT THE CONCOURSE LEVEL. THE NEW GATE SHALL BE HEAVY INDUSTRIAL DUTY, FIRE AND SECURITY ELECTRICAL ROLL UP DOOR WITH INTEGRATED CONTROL WITH THE BELT SYSTEM. CONTRACTOR ALSO TO PROVIDE AND INSTALL OSHA COMPLIANT GUARD/BARRIER AT INCLINED BELT FOR FALL PROTECTION.

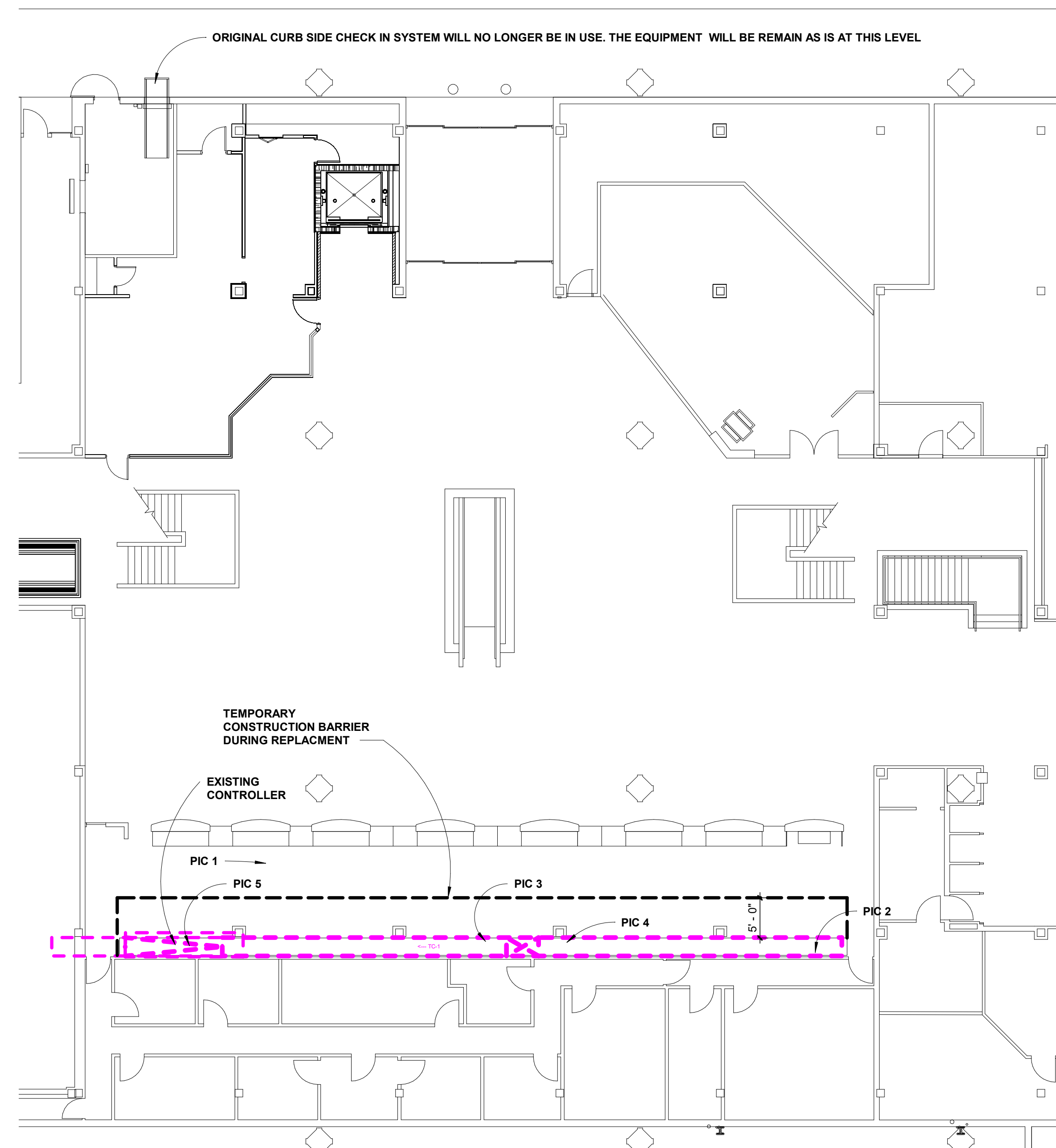


LEGEND

- EXISTING CONVEYOR COMPONENTS TO BE DEMOLISHED
- NEW SLOPED PLATE CONVEYOR SYSTEM. SEE ALSO MECHANICAL DRAWING FOR ADDITIONAL DETAIL

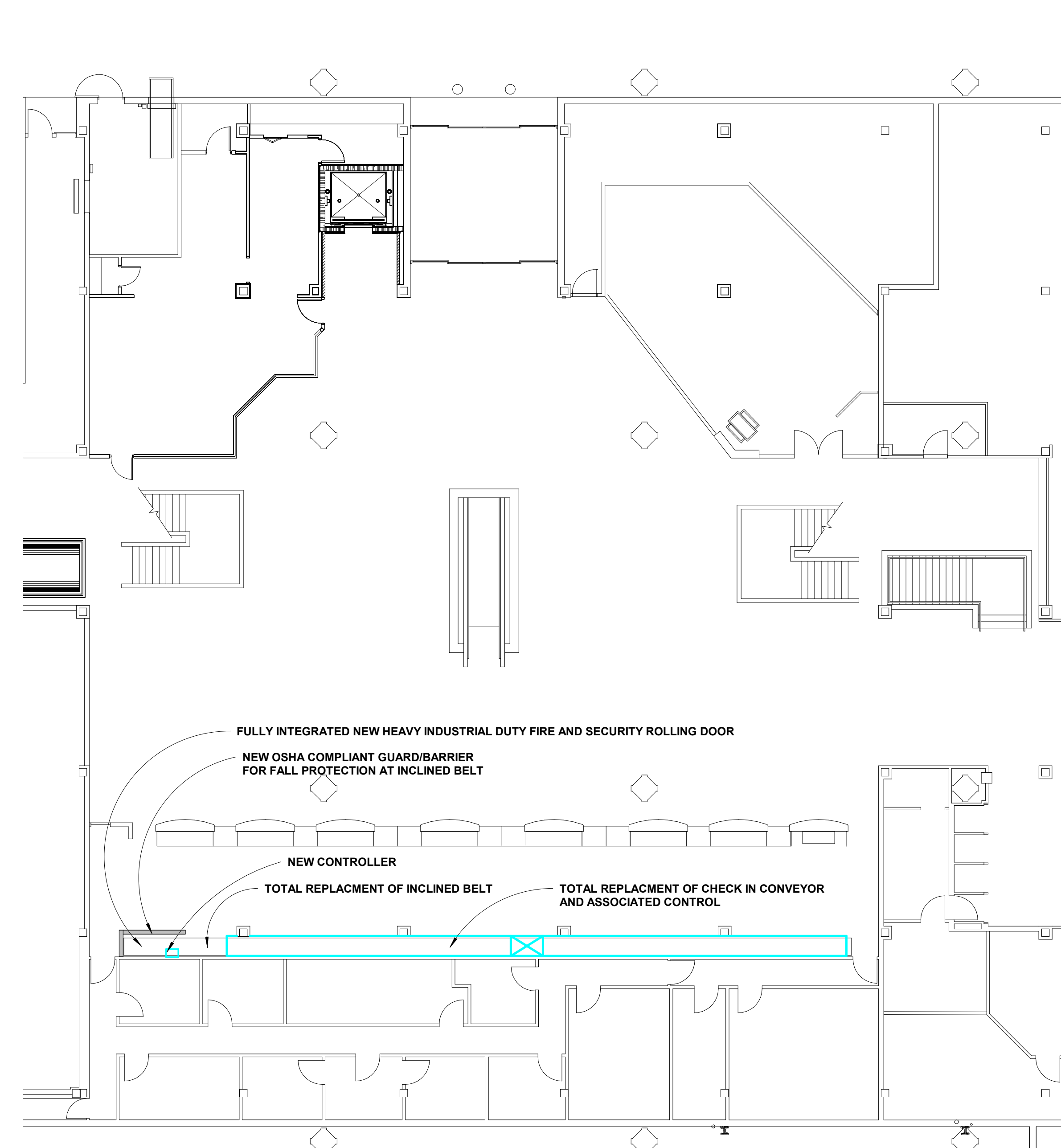
GENERAL NOTES:

1. COORDINATE WITH MSCAA FOR ACCESS CONTROL TO THIS WORK AREA.
2. ERECT TEMPORARY DUST CONTROL, CONSTRUCTION BARRIER AS NEEDED DURING DEMOLITION AND NEW INSTALLATION FOR CONSTRUCTION TRAFFIC SAFETY. TEMPORARY SAFETY BARRIER MUST BE IN PLACE DURING REPAIR TO SEPARATE CONSTRUCTION TRAFFIC FROM OTHER OPERATION TRAFFIC.
3. PROTECT EXISTING BUILDING FROM ANY DAMAGES DUE TO CONSTRUCTION. DO NOT CUT OR CORE INTO CONCRETE WITHOUT SCANNING THE AFFECT AREA.
4. COORDINATE SCHEDULE OF REPLACEMENT WORK IN ORDER TO MINIMIZE SERVICE INTERRUPTION.
5. PROTECT EXISTING NEARBY LIGHTING FIXTURES AND SPRINKLER SYSTEM. THESE ITEMS TO REMAIN AS IS.
6. DEBRIS MUST BE CLEANED UP DAILY.
7. CONSTRUCTION HAUL TRAFFIC MUST BE APPROVED BY MSCAA.
8. AREA OUTSIDE PROJECT LIMIT MUST BE PROPERLY PROTECTED.
9. ANY SENSOR, DETECTOR, ALARM, AND ANY CEILING OR WALL MOUNTED DEVICES AT THE WORK AREA MUST BE PROPERLY PROTECTED FROM CONSTRUCTION ACTIVITIES.
10. FIRE AND LIFE SAFETY MUST NOT BE COMPROMISED AT ALL TIMES.
11. COORDINATE WITH AIRPORT OPERATION IF TEMPORARY SHUT OFF OF UTILITIES IS NEEDED IN ORDER TO PERFORM THE WORK
12. PROTECT AND MAINTAIN ALL EXISTING EGRESS ACCESS UNLESS NOTED OTHERWISE
13. CONTRACTOR TO CONFIRM THE ELECTRICAL CIRCUITS PRIOR TO DEMOLITION AND COMMENCEMENT OF NEW WORK. DO NOT CUT ANY WIRE UNLESS APPROVED BY THE AIRPORT STAFF.
14. FINAL CLEAN UP IS THE RESPONSIBILITY OF THE CONTRACTOR.
15. PROVIDE SYSTEM TESTING, AND MAINTENANCE TRAINING BEFORE THE CLOSE OUT OF THE PROJECT.



1 TERMINAL A - CHECK IN CONVEYOR - CONCOURSE LEVEL - DEMOLITION
 3/32" = 1'-0"

NOTE:
 SEE MECHANICAL DRAWING FOR THE SCOPE OF THE REPLACEMENT OF MECHANICAL CONVEYOR BELT AT CONCOURSE LEVEL CONTRACTOR SHALL BE RESPONSIBLE FOR A COMPLETE TURN KEY REPLACEMENT OF THE CONVEYOR SYSTEM, INCLINED BELT, ALL ASSOCIATED CONTROL, AND THE INTEGRATED HEAVY DUTY SECURITY GATE

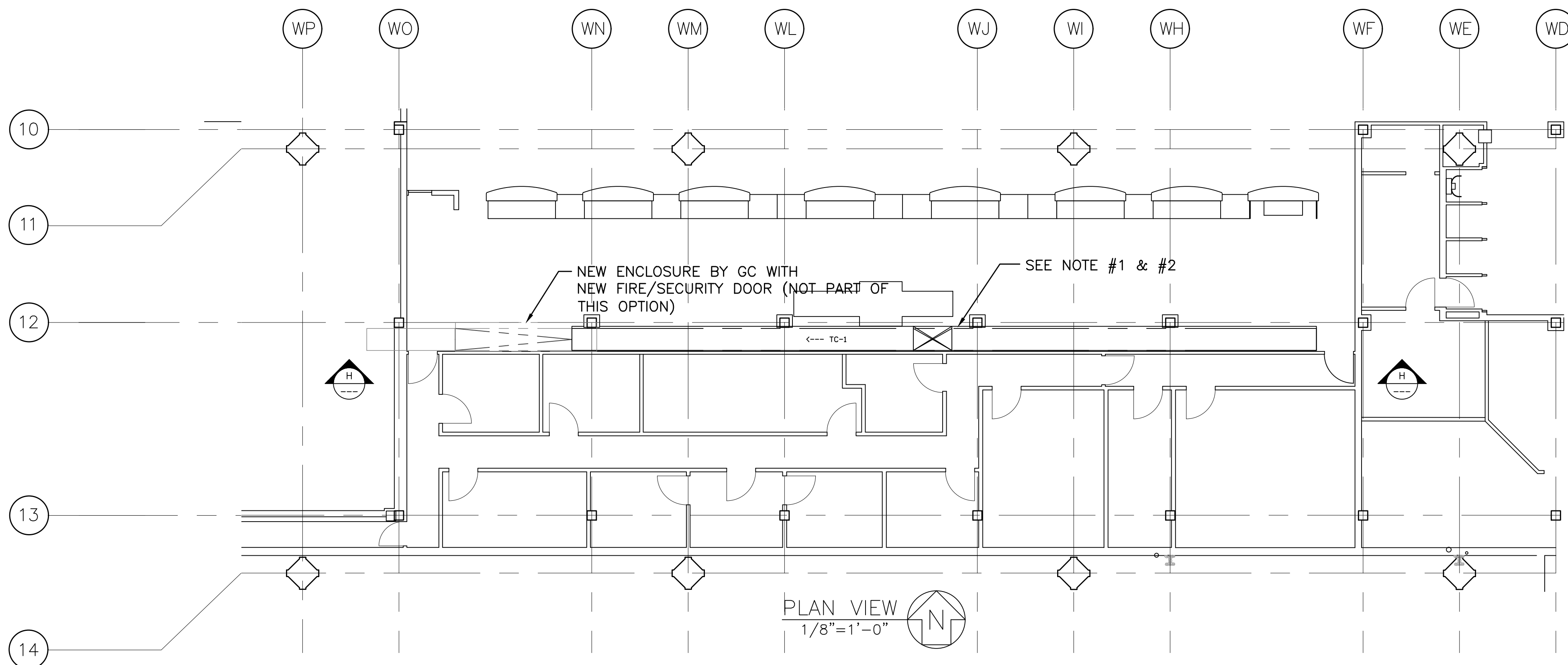


2 TERMINAL A - CHECK IN CONVEYOR - CONCOURSE LEVEL - NEW
 3/32" = 1'-0"

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REVISIONS

NO.	DESCRIPTION	DATE

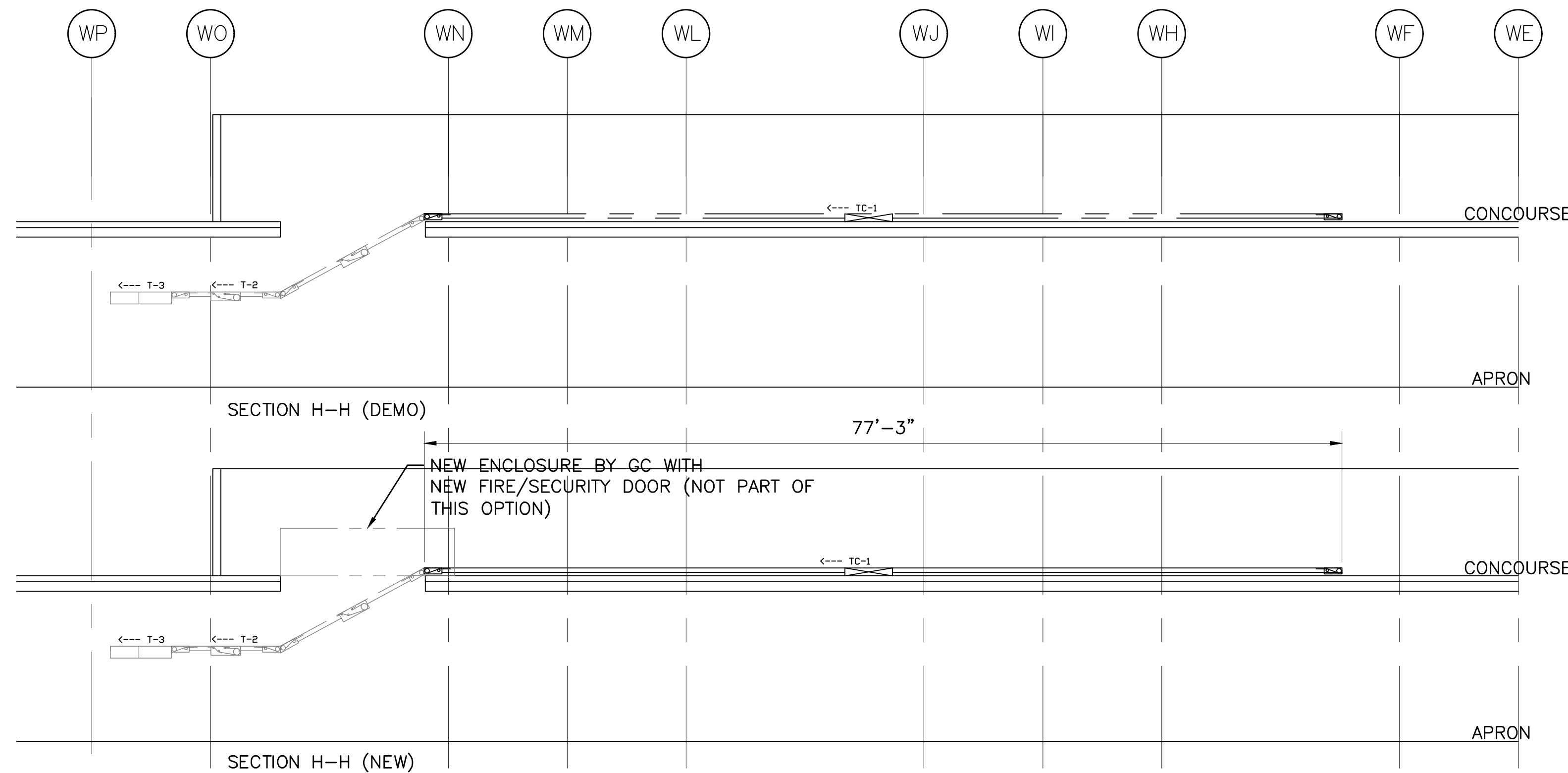


NOTES OPTION 1:

1. REMOVE AND REPLACE EXISTING TICKET COUNTER CONVEYOR
2. REPLACE ALL STAINLESS STEEL SKIRTS AND COVERS
3. BASE PROPOSAL INCLUDES NEW ENCLOSURE & ROLL-UP FIRE/SECURITY DOOR, NOT PART OF THIS OPTION.
- 3.1. NEW CONVEYOR WILL NEED TO BE INTERLOCKED WITH NEW DOOR

NEW NAME	TYPE	ANGLE	SLOPE	LENGTH (FT)	ELEV. CHANGE (FT)	BELT WIDTH (IN)	SPEED (FPM)	HP
TC	Ticket counter			77.3		27	55	1.5

Product	Manufacturer
Power units	SEW Dodge Quantus
Motors	Baldor
Powerturns	Portec
Pulleys & rollers	Precision, Inc. Chantland
Bearings	Dodge Sealmaster
Belting	Habist
Claim Unit	Siemens
Fire/security door	Cookson
Stainless Cladding finish	#4
Color	Tan or Brown
Carpet	TBD
All Products	Alternate manufacturers to be preapproved before



GENERAL NOTES:

1. ALL ELEVATION DIMENSIONS SHOWN ARE ABOVE FINISHED FLOOR
 - 1.1. BAGGAGE / CUSTOMER AREA EL. 298'-0"
 - 1.2. APRON / SECURE AREA EL. 297'-6"
 - 1.3. TUNNEL / SECURE AREA EL. VARIES
2. [CONV NAME] INDICATES EXISTING CONV. NAME
3. CONV. SUPTS NOT TO EXCEED 10'-0" U.N.O. SUPPORT TO LAND W/IN 1'-0" OF BED SPLICE.
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SUBJECT: CONCOURSE A -TICKET COUNTER PLAN	SHEET # 1 of 1	REV. 0
	DWG. NO. A002	