

MEMPHIS INTERNATIONAL AIRPORT

2491 Winchester Rd, Memphis, TN 38116

TERMINAL C AND A TICKETING LEVEL RETAIL SPACES DEMOLITION & REPAIR

JANUARY 07, 2022

01-04-2022

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PROJECT LOCATION
MEMPHIS INTERNATIONAL AIRPORT, MEMPHIS, TENNESSEE

JURISDICTION
MEMPHIS AND SHELBY COUNTY CONSTRUCTION ENFORCEMENT OFFICE

APPLICABLE CODE

- 2021 INTERNATIONAL BUILDING CODE W/ LOCAL AMENDMENTS
- 2021 INTERNATIONAL EXISTING BUILDING CODE W/ LOCAL AMENDMENTS
- 2020 INTERNATIONAL ELECTRICAL CODE W/ LOCAL AMENDMENTS
- 2021 INTERNATIONAL MECHANICAL CODE W/ LOCAL AMENDMENTS
- 2021 INTERNATIONAL FUEL GAS CODE W/ LOCAL AMENDMENTS
- 2021 INTERNATIONAL PLUMBING CODE W/ LOCAL AMENDMENTS
- 2021 INTERNATIONAL ENERGY CONSERVATION CODE W/ LOCAL AMENDMENTS
- 2021 INTERNATIONAL FIRE CODE W/ LOCAL AMENDMENTS

PROJECT DESCRIPTION
REVERSION OF SPECIFIC VACANT RETAIL TENANT SPACES BACK TO THE TICKETING LOBBY AREA

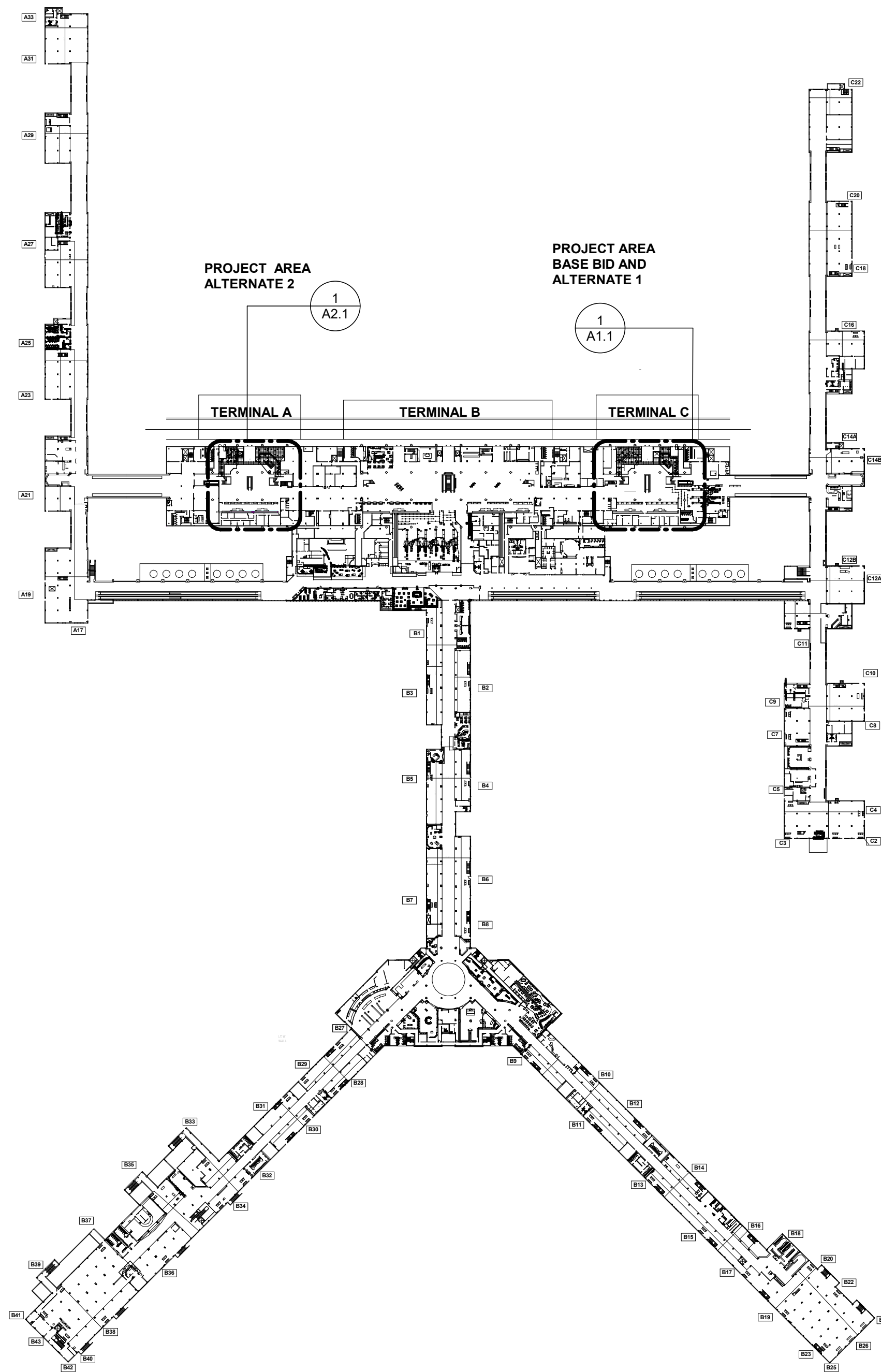
SCOPE OF WORK

- REMOVAL OF EXISTING WALL PARTITIONS AND MILLWORK AT THE VACANT TENANT SPACES
- FLOORING REPAIR AND REFINISHING
- WALL REPAIR AND REFINISHING
- NEW CEILING GRID AND TILE
- RELOCATING LIGHT FIXTURES AND AIR DIFFUSERS

BUILDING DATA

- OCCUPANCY TYPE: GROUP A
- AREA: NO CHANGE IN OVERALL BUILDING FOOTPRINT
- HEIGHT: NO CHANGE IN OVERALL BUILDING HEIGHT
- TYPE OF CONSTRUCTION: NO CHANGE IN CONSTRUCTION TYPE
- MAX TRAVEL DISTANCE TO AN EXIT: NO CHANGE IN EGRESS PATH
- EARTHQUAKE RESISTANCE: NO CHANGE IN BUILDING STRUCTURE
- OCCUPANT LOAD: NO CHANGE
- MEANS OF EGRESS: NO CHANGE IN EGRESS WIDTH
- FIRE EXTINGUISHER: NO CHANGE EXISTING TO REMAIN

- PROJECT NOTES**
- ALL MATERIAL AND CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF ALL APPLICABLE BUILDING, ELECTRICAL, MECHANICAL, FIRE PROTECTION AND LIFE SAFETY CODE ENFORCED BY LOCAL AUTHORITY. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, APPROVALS, LICENSES, ETC., FOR ALL TRADES. CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH ALL LOCAL CODES, ORDINANCES, AND REGULATIONS. CONTRACTOR IS RESPONSIBLE FOR FILING AND SECURING ALL NECESSARY PERMITS, APPROVALS, ETC., FOR ALL TRADES.
 - THE CONTRACTOR SHALL PLAN AND SCHEDULE THE PROGRESSION OF THE WORK SO THAT FINISHED AREAS DO NOT HAVE TO BE OPERATED ON OR OTHERWISE USED AS ROUTES FOR MATERIAL TRANSPORTATION IN ORDER TO REACH REMAINING WORK. SCHEDULE ANY PREPARATION WORK TO MINIMIZE THE RISK OF DAMAGE. COORDINATE WITH OWNER THE SEQUENCE OF DEMOLITION AND CONSTRUCTION PRIOR TO STARTING OF WORK.
 - CONTRACTOR SHALL SCHEDULE THE WORK PER MANUFACTURER RECOMMENDATION AND SPECIFICATION.
 - COORDINATE THE COMPATIBILITY OF THE PREPARATION WORK WITH THE REQUIREMENTS OF THE MATERIALS AND PROCEDURES SPECIFIED FOR THE NEW WORK. SCHEDULE ALL PREPARATION WORK TO COINCIDE AS CLOSELY AS PRACTICABLE WITH THE INSTALLATION OF NEW MATERIALS.
 - ANY DAMAGE TO EXISTING FINISHES, INCLUDING BUT NOT LIMITED TO WALL FINISHES, FLOORING, ETC., USED AS HAUL ROUTES OR ACCESS ROUTES SHALL BE CONSIDERED THE RESPONSIBILITY OF THE CONTRACTOR AND HE/SHE SHALL PROMPTLY REPAIR ANY DAMAGED TO ITS ORIGINAL CONDITION, TO THE SATISFACTION OF THE OWNER AND ARCHITECT, AT NO ADDITIONAL COST TO THE OWNER.
 - UNDER CIRCUMSTANCES THAT ACCESS TO WORK AREA THAT REQUIRE SECURITY CREDENTIALS ARE NEEDED, CONTRACTOR MUST FOLLOW THE AIRPORT REQUIREMENTS OF ID BADGING.
 - VERIFY JOB CONDITIONS PRIOR TO BEGINNING WORK. REPORT TO ENGINEER ANY DISCREPANCIES OR OMISSIONS WHICH MAY INTERFERE WITH SATISFACTORY COMPLETION OF WORK. RESOLVE ANY DISCREPANCIES PRIOR TO BEGINNING WORK. VERIFY ALL FIELD MEASUREMENTS, FIELD CONSTRUCTION, INSTALLATION CRITERIA, COORDINATE ALL TRADES AND THE METHODS OF CONSTRUCTION. ALL WORK IS TO CONFORM WITH DRAWINGS AND SPECS. DRAWINGS ARE NOT TO BE SCALED FOR INFORMATION. CONTRACTOR TO VERIFY THE ACTUAL SIZE FOR PROPER INSTALLATION.
 - ENSURE THE PREMISES ARE MAINTAINED CLEAN AND FREE OF DEBRIS DURING WORK. COORDINATE THE FREQUENCY OF REMOVAL WITH OWNER. FINAL CLEANUP IS THE RESPONSIBILITY OF THE CONTRACTOR. REPAIR, PATCH, TOUCH-UP AND MAINTAIN CLEANING THROUGHOUT THE PROJECT. REMOVE AND DISPOSE OF ALL DEMOLISHED MATERIAL AND OTHER CONSTRUCTION DEBRIS. OWNER'S DUMPSTER(S) OR TRASH CANS SHALL NOT BE USED FOR DISPOSAL OF ANY CONSTRUCTION WASTE. REPAIR GROUND DAMAGE CAUSED BY TRUCK TRAFFIC. CONTRACTOR TO OVERSEE CLEANING AND ENSURE THAT THE PREMISES ARE MAINTAINED AND SECURED DURING CONSTRUCTION. DAILY CLEANUP AND FINAL CLEANUP IS THE RESPONSIBILITY OF THE CONTRACTOR. REPAIR, PATCH, TOUCH-UP AND MAINTAIN CLEANING UNTIL ACCEPTANCE BY OWNER. OWNER RESERVES THE RIGHT TO REJECT ANY COMPLETED WORK THAT HAS NOT BEEN PROPERLY PROTECTED. UNPROTECTED FOOT TRAFFIC OR ANY CONSTRUCTION ACTIVITY HAS POTENTIALLY COMPROMISED THE INTEGRITY OF THE NEW INSTALLATION.
 - COORDINATE AND OBTAIN APPROVAL FROM OWNER FOR LOCATION OF DUMPSTER, MATERIAL STAGING, ETC.
 - ALL MATERIAL STORED ON SITE SHALL BE PROPERLY PROTECTED TO PREVENT DAMAGE. FAILURE TO PROTECT MATERIAL MAY BE CAUSE FOR REJECTION OF WORK.
 - REMOVE CONSTRUCTION AND GENERAL DEBRIS FROM THE WORK AREA. DISCARD ANY SCREWS, METAL SCRAPS, FOOD, WATER BOTTLES AND MISCELLANEOUS GARBAGE.
 - CONTRACTOR'S FIELD SUPERINTENDENT MUST BE PRESENT AND RESPONSIBLE FOR OVERSEEING THE WORK PROGRESS WHEN ANY WORK IS PERFORMED. FOR THE DURATION OF THE PROJECT, CONTRACTOR MUST MAINTAIN AND FURNISH ELECTRONIC DAILY LOG TO PROJECT TEAM AND OWNER DESIGNATED PERSONNEL DAILY. THE LOG DOCUMENTATION SHALL INCLUDE INFORMATION SUCH AS AMOUNT OF WORK COMPLETED, PROGRESS PICTURES, FIELD CHANGES, CONCEALED CONDITIONS, TRANSITION OF MATERIAL, UNFORESEEN DAMAGES, INCIDENTS ETC. THIS INFORMATION SHALL BE INCLUDED AS PART OF THE CLOSETOUT DOCUMENT AT THE END OF THE PROJECT.
 - ALL WORK IS TO CONFORM WITH DRAWINGS AND SPECS. DRAWINGS ARE APPROXIMATE ONLY AND NOT TO BE SCALED FOR INFORMATION. PAYMENT WILL NOT BE MADE TO GENERAL CONTRACTOR ON CHANGES OR EXTRAS UNLESS THEY ARE VERIFIED AND APPROVED BY OWNER.
 - SHOULD CONFLICT OCCUR IN OR BETWEEN DRAWINGS AND SPECIFICATIONS OR WHERE DETAIL REFERENCES OR CONTRACT DRAWINGS HAVE BEEN OMITTED, CONTRACTOR IS DEEMED TO HAVE ESTIMATED THE MOST EXPENSIVE MATERIALS AND CONSTRUCTION INVOLVED UNLESS THEY HAVE ASKED FOR AND OBTAINED WRITTEN PERMISSION FROM ARCHITECT AS TO WHICH MATERIAL OR METHOD WILL BE REQUIRED.
 - NO PORTION OF THE WORK REQUIRING A SHOP DRAWING OR SAMPLE SUBMISSION SHALL BE COMMENCED UNTIL THE SUBMISSION HAS BEEN APPROVED BY THE AIRPORT. ALL SUCH PORTIONS OF THE WORK SHALL BE IN ACCORDANCE WITH THE APPROVED SHOP DRAWINGS AND SAMPLES.
 - CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND EXISTING JOB CONDITIONS AND SHALL REPORT TO ENGINEER ANY DISCREPANCIES OR OMISSIONS WHICH WOULD INTERFERE WITH SATISFACTORY COMPLETION OF WORK. CONTRACTOR IS RESPONSIBLE FOR VERIFICATION OF ALL FIELD MEASUREMENTS, FIELD CONSTRUCTION, INSTALLATION CRITERIA, COORDINATION OF ALL TRADES AND THE METHODS OF CONSTRUCTION.
 - CONTRACTOR SHALL PROVIDE A SCHEDULE OF SHOP DRAWINGS, SAMPLES AND CUT SHEETS FOR ENGINEER/OWNER'S APPROVAL PRIOR TO ORDERING. CONTRACTOR'S RESPONSIBILITY FOR ERROR OR OMISSIONS IN SUBMITTALS IS NOT RELIEVED BY A/E REVIEW OF SUBMITTALS.
 - DISRUPTION SHALL BE MINIMAL OUTSIDE PROJECT LIMITS AT ALL TIMES DURING CONSTRUCTION. THE EXACT LIMITS OF THE CONTRACTOR'S STAGING AND STORAGE AREA SHALL BE ESTABLISHED BY THE CONTRACTOR WITH THE APPROVAL OF MSCAA.
 - CONTRACTOR SHALL EXERCISE ALL SAFETY PRECAUTION DURING WORK. DO NOT ALLOW WASTE MATERIAL TO COLLECT IN CONCENTRATED BUILDUPS AROUND THE WORK AREA. FURNISH PROTECTION OF THE BUILDINGS CONTENTS AND PERSONNEL AT ALL TIMES. COLLECT AND REMOVE DEMOLITION AND WASTE MATERIAL FROM THE WORK AREA IN AN ORDERLY MANNER.
 - THE AREAS AROUND THE WORK WILL BE IN USE DURING THE CONSTRUCTION. THEREFORE, IT IS OF UTMOST IMPORTANCE FOR SAFETY DURING THE WORK. COORDINATE ALL MATERIAL, WORKMEN ACCESS, WITH THE OWNER'S REPRESENTATIVE TO MINIMIZE DISRUPTION OF THE AREA TO MINIMIZE THE THREAT OF LOSS OR DAMAGE TO THE EXISTING FACILITY, ITS CONTENTS, AND OCCUPANTS. THE CONSTRUCTION SHALL NOT COMPROMISE SECURITY, LIFE SAFETY OR FIRE SAFETY REQUIREMENTS. ANY WORK AFFECTING CODE REQUIREMENTS SHALL BE COMPLETED IN THE SAME WORK DAY OR TEMPORARY MEASURES SHALL BE TAKEN TO ASSURE SAFETY COMPLIANCE AT ALL TIMES.
 - UNLESS NOTED OTHERWISE, CONTRACT TIME FOR THE PROJECT SHALL BE 60 DAYS. SEE TERMS AND CONDITIONS IN PROCUREMENT REQUIREMENT FOR INFORMATION ABOUT LIQUIDATED DAMAGES.
 - UNLESS APPROVED OTHERWISE BY THE AIRPORT AUTHORITY, ALL WORK THAT WOULD GENERATE EXCESSIVE NOISE SUCH AS DEMOLITION JACK HAMMERING, SAW CUTTING ETC MUST OCCUR AT NIGHT FROM 2200-0500 TO AVOID DISTURBANCE OF THE DAY TIME OPERATION OF THE AIRPORT.
 - SHOULD THERE BE A NEED FOR MASONRY OR CONCRETE CUTTING OR DRILLING, IT IS THE RESPONSIBILITY FOR OF THE CONTRACTOR TO PROTECT BOTH CONSTRUCTION WORKERS AND AIRPORT WORKERS FROM SILICA DUST. CONTRACTORS CAN USE TOOLS FROM TABLE 1 OF THE SILICA STANDARD THAT INVOLVE HEPA VACUUMS OR WET METHODS TO PREVENT SILICA DUST FROM BECOMING AIRBORNE.
 - A DUMPSTER MAY BE PLACED IN THE FIRE STATION#33 PARKING LOT. DUMPSTER NEED TO BE COVERED.
 - CONTRACTOR EMPLOYEES WILL BE ALLOWED TO PARK IN ONE OF THE AIRPORT EMPLOYEE PARKING LOTS.



THE PROJECT CONSISTS OF INCORPORATING THE SPECIFIC EXISTING VACANT RETAIL SPACES BACK INTO THE PUBLIC OPEN LOBBY AREA

BASE BID IS THE REMOVAL AND REPAIR OF EXISTING RETAIL SPACES AT TERMINAL C TICKETING LEVEL AS IDENTIFIED ON DRAWING A1.1
 ALTERNATE NO.1 IS THE REMOVAL AND REPAIR OF EXISTING MENS' ROOM AT TERMINAL C TICKETING LEVEL AS IDENTIFIED ON DRAWING A1.1
 ALTERNATE NO.2 IS THE REMOVAL AND REPAIR OF EXISTING RETAIL SPACES AT TERMINAL A TICKETING LEVEL AS IDENTIFIED ON DRAWING A2.1

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CONSULTANT JOB NO. ---

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REVISIONS		
NO.	DESCRIPTION	DATE

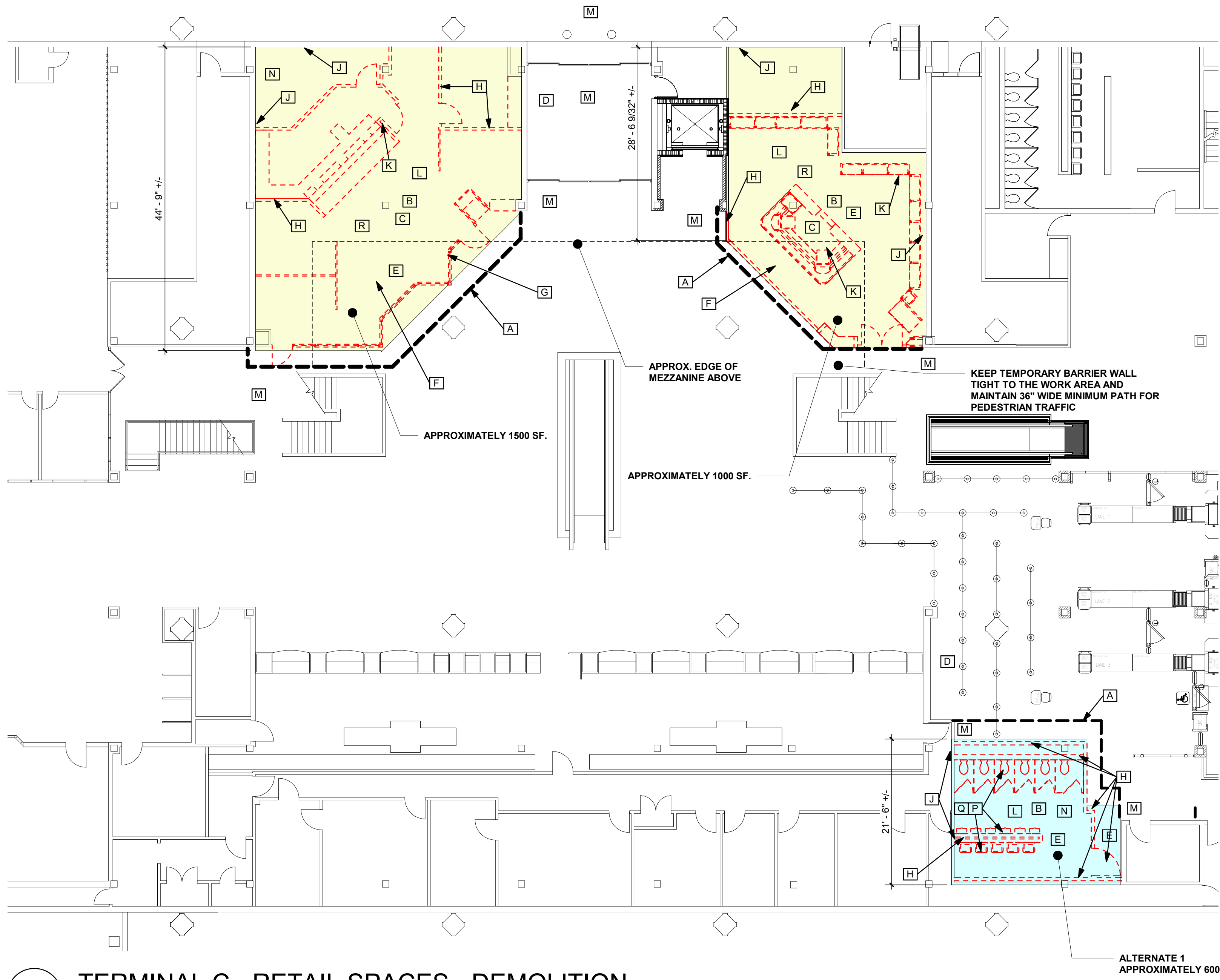
MSCAA NO. 19-1423-00 (19-1423-26)

PROJECT:
**TERMINAL C AND A TICKETING LEVEL
 RETAIL SPACES
 DEMOLITION AND
 REPAIR**

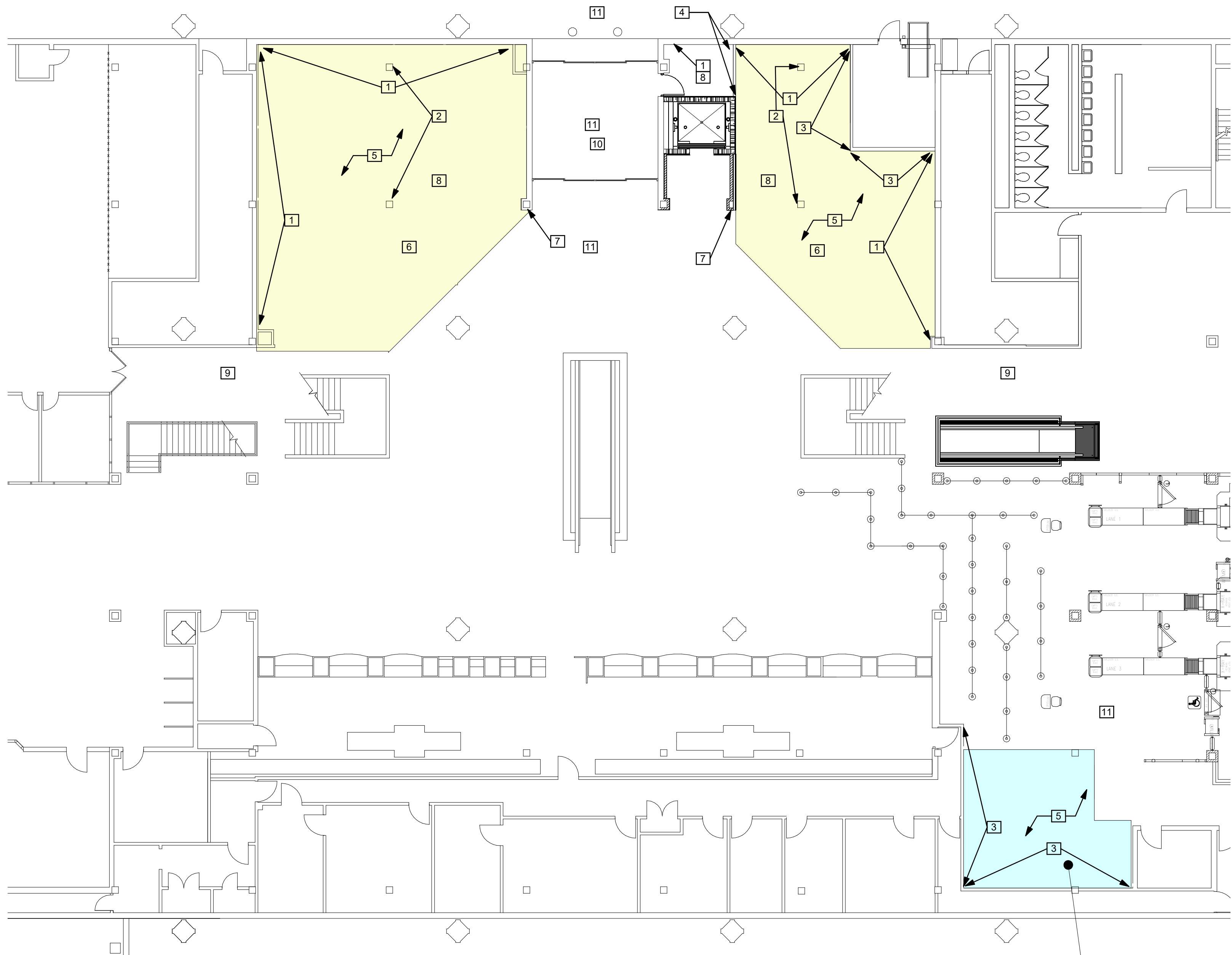
SET:

SHEET TITLE:
COVER SHEET

RVT: FILE NAME
 DATE: 01.07.2022 DWG NO: G0.00
 SCALE: 1" = 160'-0"



4 TERMINAL C - RETAIL SPACES - DEMOLITION
3/32" = 1'-0"



1 TERMINAL C - RETAIL SPACES - NEW AND REPAIR
3/32" = 1'-0"

DEMOLITION NOTES:

- A. CONTRACTOR SHALL ERECT TEMPORARY BARRIER TO SEPARATE THE CONSTRUCTION TRAFFIC AND PASSENGER TRAFFIC AS SHOWN IN HEAVY DASHED LINES. ALL TEMPORARY WALLS SHALL BE CONSTRUCTED OF METAL STUDS; ANCHORED ON BOTTOM WITH DOUBLE STICK TAPE AND BRACED TO SOUND STRUCTURAL SUPPORT NEARBY ON TOP (ATTACHMENT TO CEILING TILES OR GRID IS PROHIBITED). THE OUTSIDE (PUBLIC SIDE) OF THE WALL SHALL BE FINISHED FLOOR TO CEILING WITH 5/8" DRYWALL WITH 2 COATS OF FLAT OFF WHITE LATEX PAINT. THE BOTTOM OF THE WALL SHALL BE TRIMMED WITH 4" BLACK COVE BASE. A 2"x6" CHAIR RAIL, BLOCKED 2" OFF THE WALL, SHALL BE INSTALLED 36" TO CENTER ABOVE FINISHED FLOOR. THE CHAIR RAIL SHALL BE SANDED AND PAINTED WITH TWO COATS OF BRONZE PAINT (PPG PITT-TECH ACRYLIC HIGH GLOSS DTM INDUSTRIAL ENAMEL - BRONZE TONE). SEE ATTACHED PICTURE "A" FOR EXAMPLE. THE CONTRACTOR SHALL RETAIN RESPONSIBILITY DURING CONSTRUCTION TO MAINTAIN THE WALL FOR AESTHETIC AND SECURITY ISSUES. MSCAA DEVELOPMENT DIVISION SHALL APPROVE ITS LOCATION. ANY ATTACHMENTS TO TERRAZZO FLOORS AND ALL SIGNAGE AND/OR GRAPHICS. WALK-OFF MATS SHALL BE USED AT ALL ACCESS POINTS TO THE CONSTRUCTION AREA TO PREVENT TRACKING OF DUST AND DEBRIS AND IS RESPONSIBLE FOR CLEANUP IF ANY DIRT, DUST AND/OR DEBRIS GETS OUTSIDE THEIR CONSTRUCTION LIMITS.
- B. CAREFULLY REMOVE ALL EXISTING FLOOR FINISHES WITHIN THE PROJECT LIMIT TO ORIGINAL TERRAZZO OR CONCRETE FLOOR. IT IS THE AIRPORT'S INTENT TO PRESERVE AND REUSE THE ORIGINAL TERRAZZO FLOOR WHEREVER FEASIBLE WITHIN THE PROJECT LIMIT.
- C. THE AIRPORT AUTHORITY WILL REMOVE ALL MOVABLE FURNITURE BEFORE COMMENCEMENT OF DEMOLITION WORK. ANY REMAINING ITEMS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR REMOVAL OR DISPOSAL. RETURN THE LOCK ASSEMBLY TO THE LOCK SHOP.
- D. COORDINATE WITH THE AIRPORT AUTHORITY THE HAUL ROUTE OF DISPOSAL TRAFFIC. IT IS OF UTMOST IMPORTANCE TO ENSURE NO CONSTRUCTION DEBRIS SUCH AS PACKAGING MATERIAL OR SHARP OBJECT BE LEFT AT ANY PUBLIC AREA OR DRIVEWAY. IF HAULING OF CONSTRUCTION DEBRIS MUST COME THRU THE ENTRANCE VESTIBULE, CONTRACTOR MUST PROTECT THE WALK OFF MAT AT THE ROUTE WITH PLYWOOD SO AS NOT TO DAMAGE THE EXISTING WALKOFF MAT.
- E. REMOVE AND DISPOSE ALL EXISTING CEILING GRIDS, GYPSUM BOARD FURR DOWN STRUCTURE CEILING TILES, AND LIGHT FIXTURES WITHIN THE PROJECT LIMIT. CONTRACTOR TO PROTECT THE ELECTRICAL CIRCUITS PRIOR TO DEMOLITION AND COMMENCEMENT OF NEW WORK. DO NOT CUT ANY WIRE UNLESS APPROVED BY THE AIRPORT AUTHORITY. CONTRACTOR SHALL ALSO PRESERVE THE EXISTING HVAC DUCTWORK FOR CAPPING OR REROUTING. DO NOT REMOVE ANY DUCT SYSTEM UNLESS APPROVED BY AIRPORT. BLOCK RETURN AIR TEMPORARILY TO PREVENT CONSTRUCTION DUST AND DEBRIS FROM TRAVELING TO OTHER AIRPORT AREA.
- F. CAREFULLY REMOVE THE EXISTING ADD-ON SECONDARY CEILING FRAMING UNDER AND OUTSIDE THE BALCONY. PROTECT THE ADJACENT CONSTRUCTION WHEREVER POSSIBLE
- G. CAREFULLY REMOVE ALL EXISTING FEATURE GLASS WALL AND CURB.
- H. REMOVE ANY VERTICAL WALL PARTITION AND METAL FRAMING WITHIN THE PROJECT LIMIT.
- I. NOT USED
- J. EXERCISE CAUTION DURING REMOVAL OF VERTICAL PARTITION WHEREVER THERE IS AIRPORT ORIGINAL BRICK WALL BEHIND. IT IS THE OWNER'S INTENT TO PRESERVE THOSE FINISHES WHEREVER POSSIBLE.
- K. REMOVE AND DISPOSE ALL EXISTING RETAIL SHELVING, OWNER MILLWORK, TRANSACTION COUNTER ETC.
- L. PROTECT AND COVER EXISTING RETURN AIR OPENING WITHIN THE PROJECT LIMIT SO THE FINE DUST AND DEBRIS DO NOT TRAVEL THROUGH THE HVAC SYSTEM TO OTHER AREAS.
- M. DOCUMENT ANY EXISTING DAMAGE AT THE AREA WITHIN AND ADJACENT TO THE WORK AREA TO AVOID ANY DISPUTE OF DAMAGES
- N. CAP ANY EXISTING UTILITY AND DRAINLINE WHEREVER POSSIBLE FOR FUTURE REUSE. CAP THE UTILITY BELOW TICKETING LEVEL FLOOR DECK OR BEHIND WALL.
- O. NOT USED.
- P. PROVIDE PLUMBING CUT OFF VALVE BEFORE CUTTING OF ANY UTILITY. COORDINATE WITH AIRPORT MAINTENANCE IF TEMPORARY SHUT OFF OF UTILITIES IS NEEDED IN ORDER TO PERFORM THE WORK
- Q. REMOVE ALL EXISTING TOILET FIXTURES, TOILET PARTITIONS, AND TOILET ACCESSORIES.

LEGEND

- EXISTING CONSTRUCTION, FURNITURE AND MISCELLANEOUS COMPONENTS TO BE REMOVED
- BASE BID PROJECT LIMIT
- ALTERNATE 1 PROJECT LIMIT
- ALTERNATE 2 PROJECT LIMIT
- TEMPORARY BARRIER



PICTURE A
EXAMPLE OF TEMPORARY BARRIER WALL AS DESCRIBED IN NOTE A.

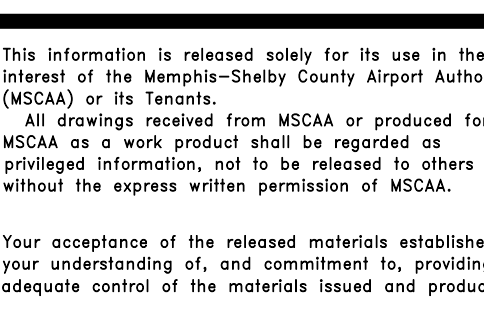
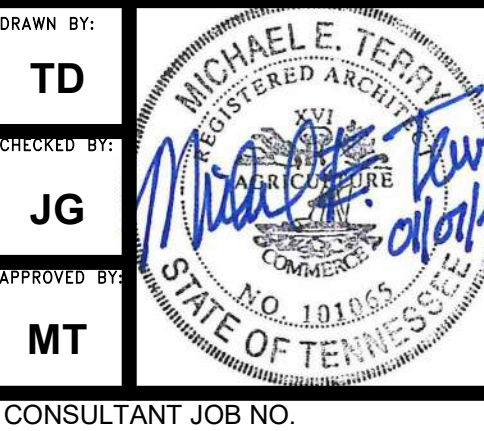


PICTURE B
EXISTING CONDITION OF THE MENS RESTROOM AT TERMINAL C FOR ALTERNATE ONE. THE RESTROOM HAS NOT BEEN IN USE.

NEW WORK AND REPAIR NOTES:

- 1. SPOT REPAIR EXISTING ORIGINAL BRICK VENEER WALL SURFACE. TOOTH IN MASONRY UNITS PROVIDED BY AIRPORT AUTHORITY. COORDINATE WITH AIRPORT FOR OBTAINING THE BRICK FOR REPAIR. CLEAN BRICK WALL FROM CORNER TO CORNER TO RESTORE THE CLEANLINESS OF THE SURFACE. MATCH MORTAR COLOR AT REPAIR.
- 2. EXISTING INTERIOR 2'X2' BRICK WRAPPED COLUMN. REPAIR AS NEEDED. CLEAN THE SURFACE THE SAME MANNER AS OTHER BRICK VENEER.
- 3. IF EXISTING WALL BOARD CANNOT BE RESTORED FOR PAINTING SATISFACTORILY, INSTALL ONE LAYER OF NEW 5/8" GYPSUM WALL BOARD. MUD, SAND AND APPLY 3 COATS OF PAINT COATING. COLOR TO BE SELECTED BY AIRPORT. INSTALL WALL BASE AND BUMPER RAIL. WALL BASE TO BE MATCH EXISTING. BUMPER RAIL TO MATCH EXISTING.
- 4. NEW WALL PARTITION WITH 3 5/8" 22GA METAL STUD FRAMING AT 16" O.C. INSTALL 1 LAYER OF 5/8" GYPSUM WALL BOARD ON BOTH SIDES. FILL WALL CAVITY WITH BATT INSULATION. INSTALL WALL BASE AND BUMPER RAIL. WALL BASE TO BE MATCH EXISTING. BUMPER RAIL TO MATCH EXISTING.
- 5. CLEAN, SAND, AND POLISH AND RESTORE THE UNCOVERED ORIGINAL TERRAZZO FLOOR. SPOT REPAIR AS NEEDED WHERE DAMAGE WAS CAUSED BY PREVIOUS CONSTRUCTION OR UTILITY PENETRATIONS. SPOT REPAIR TO MATCH EXISTING ADJACENT FINISH WHEREVER FEASIBLE.
- 6. INSTALL THE FEATURE PVC COLOR BANDING TO MATCH THE EXISTING ADJACENT TUBELITE 3MM CRANBERRY OR CACTUS GREEN BAND. ADHERE WITH DOUBLE SIDED TAPE.
- 7. STAINLESS STEEL WALL END CAP TO MATCH THE APPEARANCE OF THE END CAP AT OPPOSITE WALL END.
- 8. INSTALL NEW 15/16" 2'X4' CEILING GRID SYSTEM WITH NEW CEILING TILES TO MATCH LAYOUT AND ORIENTATION OF ADJACENT LAY-IN CEILING SYSTEM. BLEND TILES AS NEEDED. THE CEILING TILES TO BE ARMSTRONG 2'X4' CORTEGA SECOND LOOK I NO. 2765. TIE IN TO EXISTING SYSTEM WHEREVER FEASIBLE. UNLESS NOTED OTHERWISE, CEILING HEIGHT TO BE MINIMUM 9' AFF. MATCH ADJACENT.
- 9. PROTECT EXISTING NEARBY LIGHTING FIXTURES AND LIFE SAFETY SYSTEM IMMEDIATELY OUTSIDE PROJECT LIMITS. THESE ITEMS TO REMAIN AS IS. ANY SENSOR, DETECTOR, ALARM, AND ANY CEILING OR WALL MOUNTED DEVICES AT THE WORK AREA MUST BE PROPERLY PROTECTED FROM CONSTRUCTION DAMAGE. FIRE AND LIFE SAFETY MUST NOT BE COMPROMISED AT ALL TIMES.
- 10. PROTECT AND MAINTAIN ALL EXISTING EGRESS ACCESS UNLESS NOTED OTHERWISE
- 11. AREAS IMMEDIATELY OUTSIDE PROJECT LIMIT MUST BE PROPERLY PROTECTED FROM CONSTRUCTION TRAFFIC UNTIL THE COMPLETION OF CONSTRUCTION.

NOTE:
EXCEPT FOR THE WORK RELATED TO THE MENS ROOM WHICH IS ALTERNATE 1. ALL WORK ON THIS DRAWING SHALL BE CONSIDERED AS BASE BID.



NO.	DESCRIPTION	DATE

MSCAA No: 19-1423-00 (19-1423-26)
PROJECT: **TERMINAL C AND A TICKETING LEVEL RETAIL SPACES DEMOLITION AND REPAIR**

SET: _____

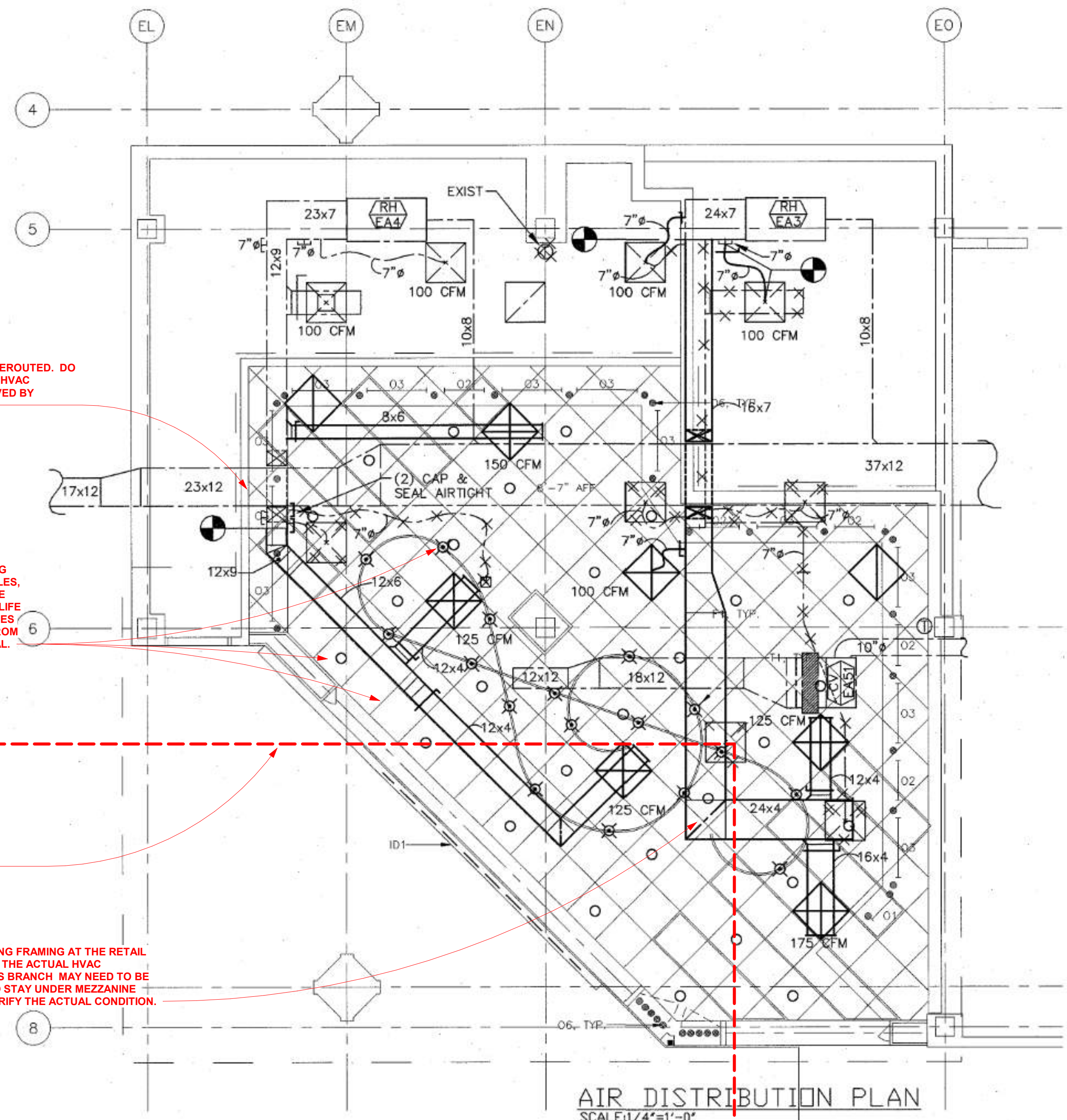
SHEET TITLE: **TERMINAL "C" RETAIL AREAS DEMOLITION**
REV. FILE NAME: _____
DATE: 01.07.2022 DWG NO: A1.1
SCALE: As Indicated

HVAC DUCTWORK TO BE REROUTED. DO NOT REMOVE OR MODIFY HVAC DUCTWORK UNTIL APPROVED BY AIRPORT MAINTENANCE.

ALL EXISTING SECONDARY CEILING FRAMING, EXISTING CEILING GRIDS, CEILING TILES, AND LIGHT FIXTURES TO BE REMOVED. PROTECT ANY LIFE SAFETY SENSOR OR DEVICES AND OBTAIN APPROVAL FROM AIRPORT BEFORE REMOVAL.

APPROXIMATE EDGE OF MEZZANINE ABOVE

SINCE THE ORIGINAL CEILING FRAMING AT THE RETAIL SPACE WILL BE REMOVED, THE ACTUAL HVAC DUCTWORK ROUTE AT THIS BRANCH MAY NEED TO BE MODIFIED OR REMOVED TO STAY UNDER MEZZANINE WALKWAY ABOVE. FIELD VERIFY THE ACTUAL CONDITION.

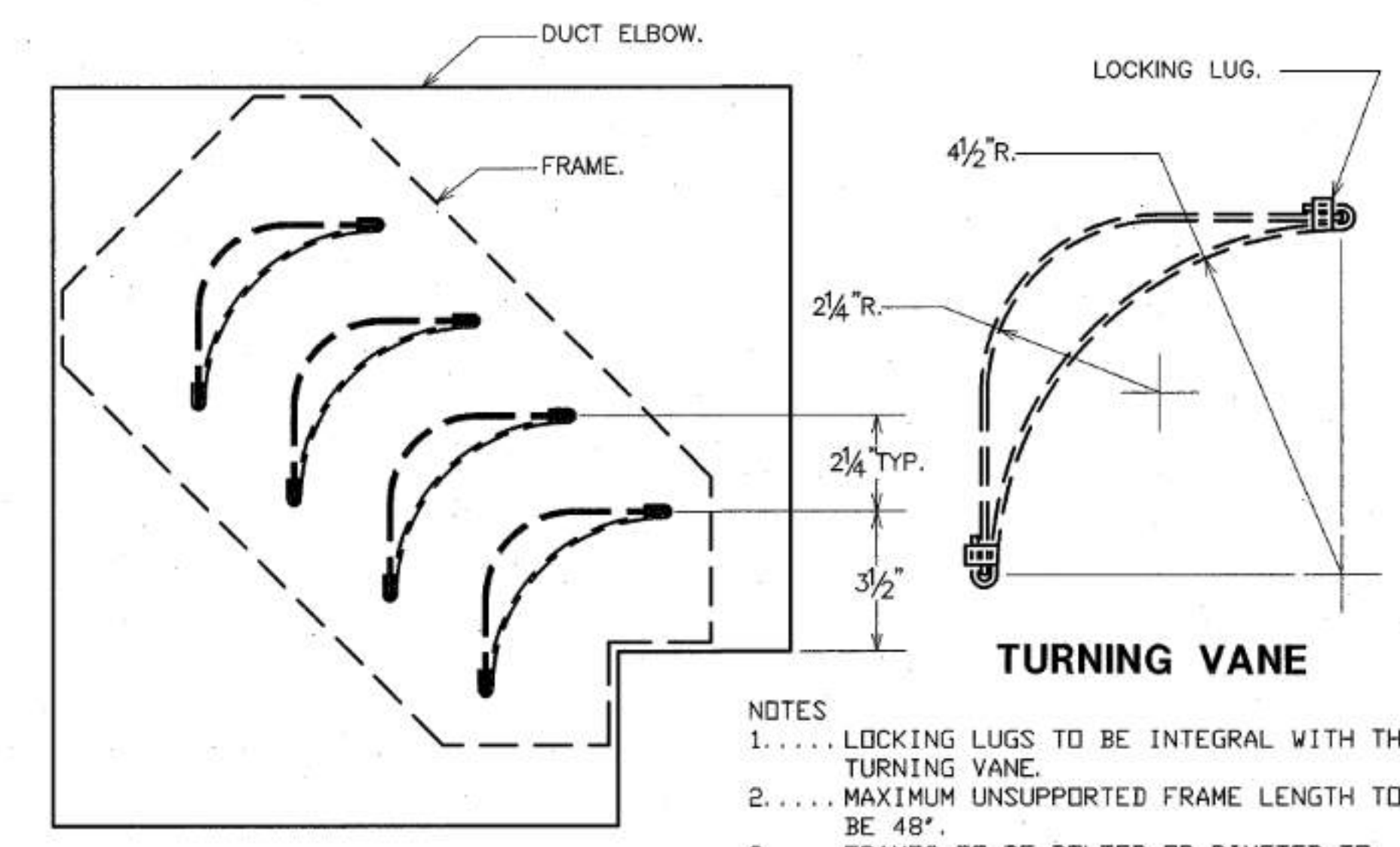


AIR DISTRIBUTION PLAN
SCALE: 1/4" = 1'-0"

NOTES:

- SEE PLANS FOR DUCT SIZES.
- SEE BALANCING DAMPER CONSTRUCTION DETAIL.

TYPICAL DETAIL OF RECTANGULAR DUCT TAP (WITH VOLUME DAMPERS)
NOT TO SCALE

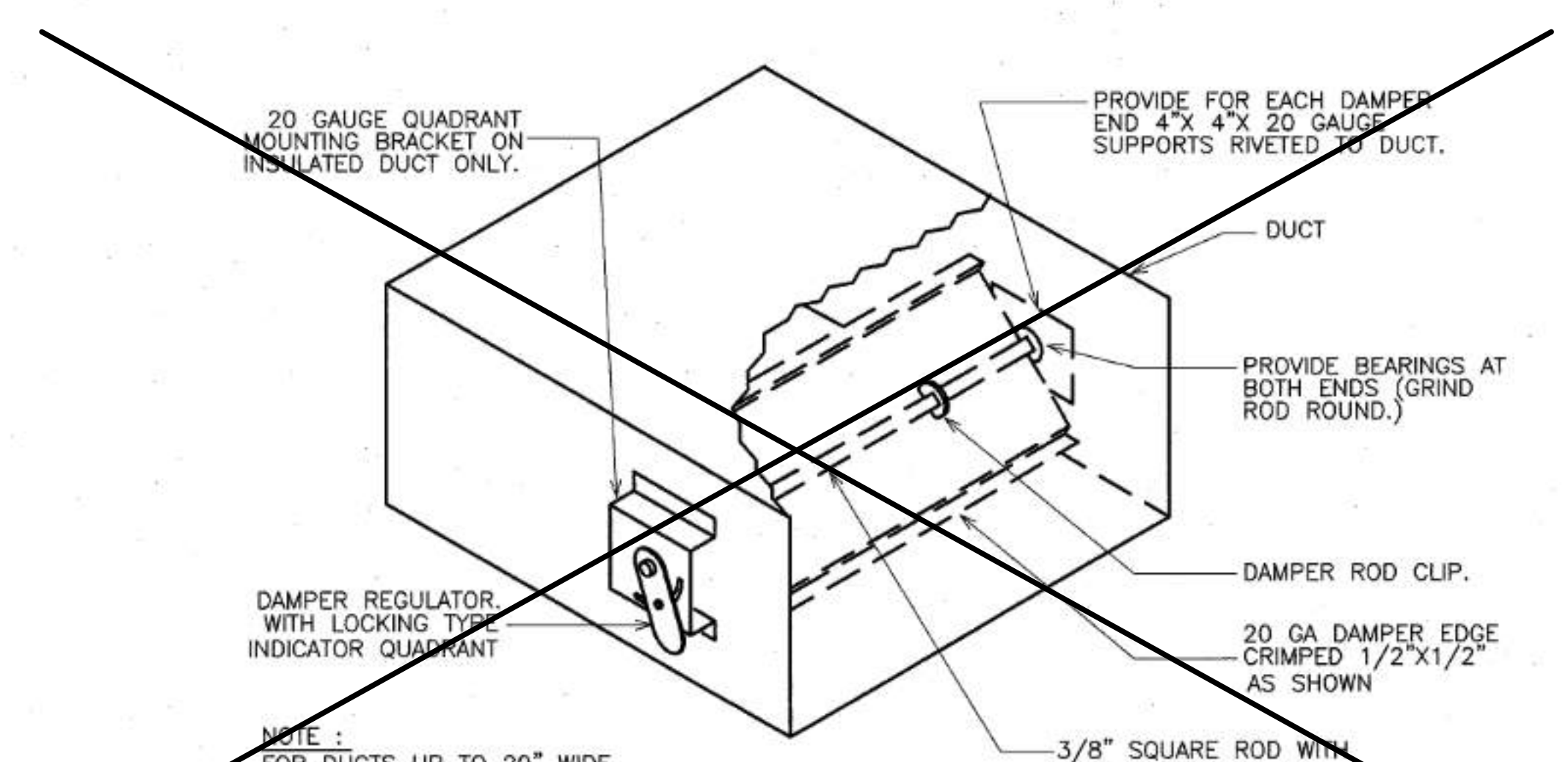


VANED ELBOW

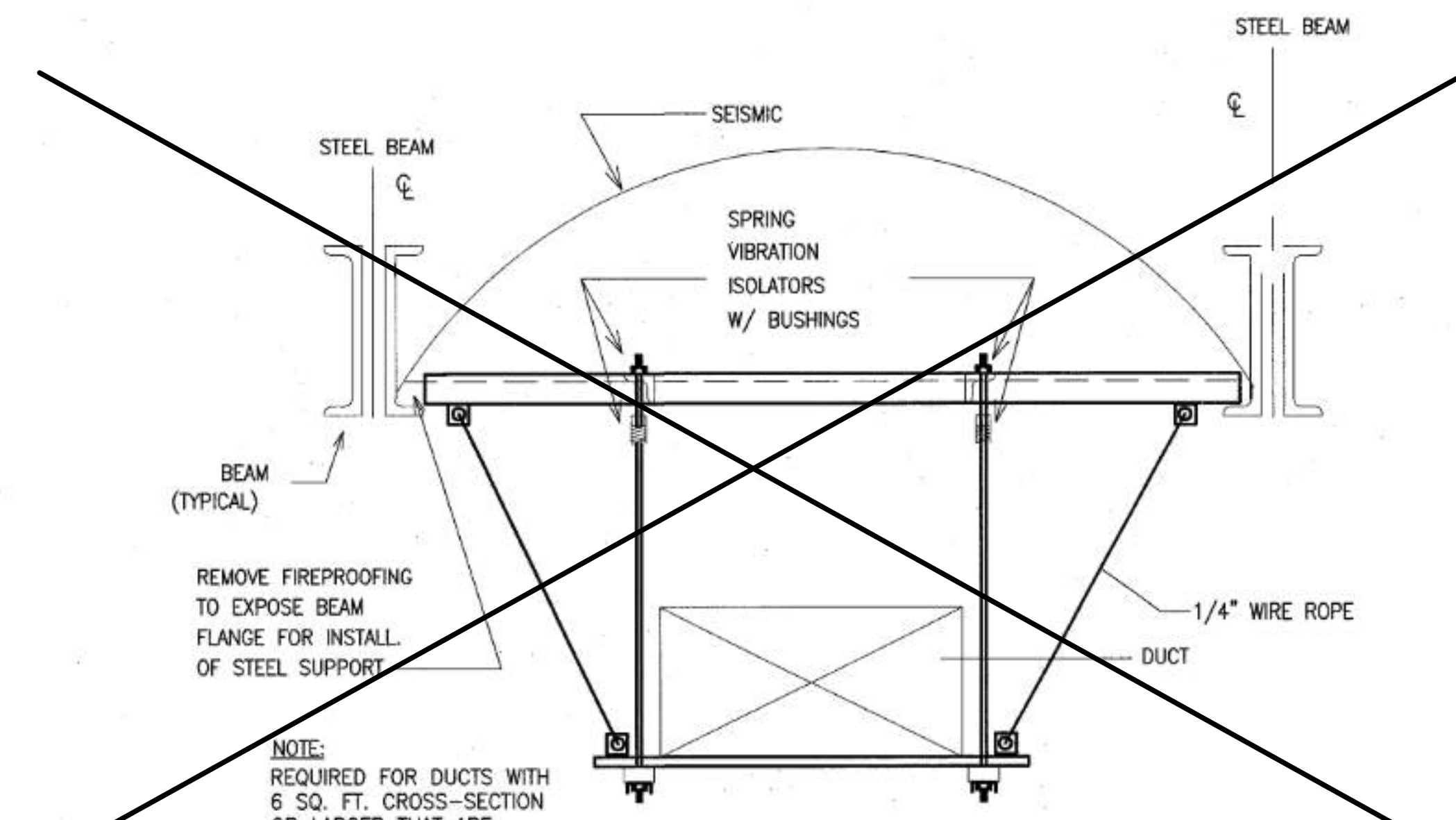
TURNING VANE

- NOTES**
- ... LOCKING LUGS TO BE INTEGRAL WITH THE TURNING VANE.
 - ... MAXIMUM UNSUPPORTED FRAME LENGTH TO BE 48\"
 - ... FRAMES TO BE BOLTED OR RIVETED TO THE ELBOW.
 - ... VANES AND FRAMES TO BE SAME GAUGE AS THE ELBOW. (MINIMUM 20 GAUGE)
 - ... SQUARE ELBOWS ARE TO BE USED ONLY WHEN SPECIFICALLY INDICATED ON THE DESIGN DRAWINGS.

DOUBLE THICKNESS TURNING VANES FOR MITERED ELBOWS
NOT TO SCALE



LOW PRESSURE BALANCING DAMPER
NOT TO SCALE



SEISMIC DUCT HANGING
(NOT TO SCALE)

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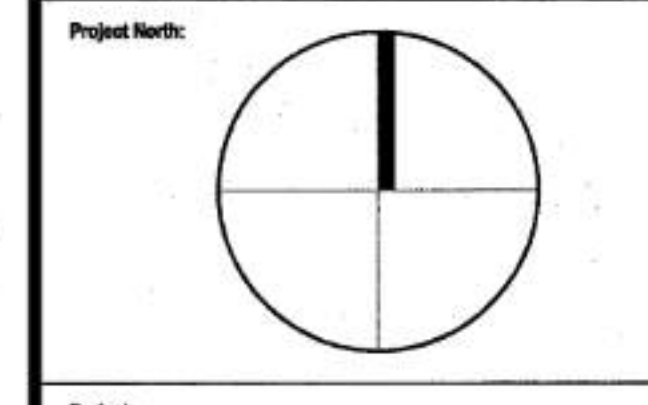
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ARCHITECT
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ENGINEER
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CONSULTANT
[Signature]
04/01/05

Date:	Issue to:	Revision / O Clarification	For:
02/25/05	GC		CM BID
04/01/05	MSCAA/MSCCCE		PERMIT



Project:
HUDSON GROUP
SPACES 220
MAIN TERMINAL LAND SIDE
MEMPHIS INT'L AIRPORT
MEMPHIS, TN 38116

Sheet Title:
HVAC
AIR DISTRIBUTION PLAN
& DETAILS

Date:	Project No.:
02/25/05	24235
Scale:	Sheet No.:
AS NOTED	H
Drawn by:	
AB	
Checked by:	
Date Released:	
	101

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REVISIONS		
NO.	DESCRIPTION	DATE

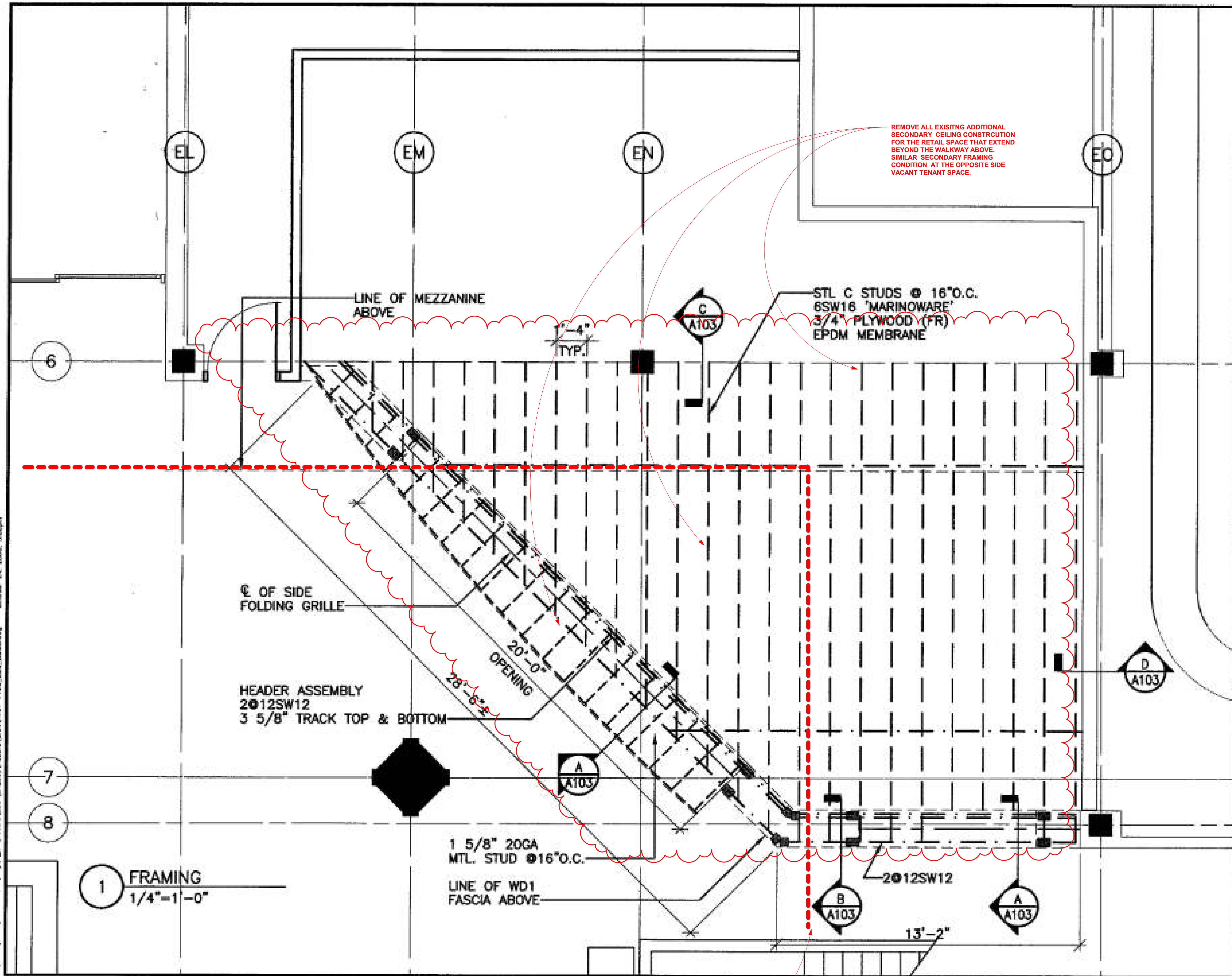
MSCAA No. 19-1423-00 (19-1423-26)

PROJECT:
TERMINAL C AND A TICKETING LEVEL RETAIL SPACES DEMOLITION AND REPAIR

SET:

SHEET TITLE:
TERMINAL "C" RETAIL AREA (EAST) INFO FROM AIRPORT

THE DRAWING ABOVE IS PROVIDED FROM THE AIRPORT'S ARCHIVED RECORDS. IT IS INCLUDED IN THE SET FOR CONTRACTOR GENERAL REFERENCE OF EXISTING CONSTRUCTION. CONTRACTOR SHALL FIELD VERIFY THE ACCURACY OF EXISTING CONDITIONS.



APPROXIMATE EDGE OF MEZZANINE ABOVE

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ARCHITECT
TRANZSYSTEMS CORPORATION

Date: Issued: Δ Revised / C Clarification Per:
 05/31/05 MSCA/GC BULL.N02

Project:
**HUDSON GROUP
 SPACES 220
 MAIN TERMINAL LAND SIDE
 MEMPHIS INT'L AIRPORT
 MEMPHIS, TN 38118**

Sheet Title:
**ARCHITECTURAL
 FRAMING
 STOREFRONT & ROOF
 RECORD DRAWINGS**

Date: 05/31/05
 Scale: AS NOTED
 Drawn by: MS
 Checked by:
 Date Released:

Project No.: A703423500
 Sheet No.: SK
 101.1

ARCHITECT JOB NO.
20008-T26

FOR REFERENCE ONLY

CONSULTANT JOB NO. ---

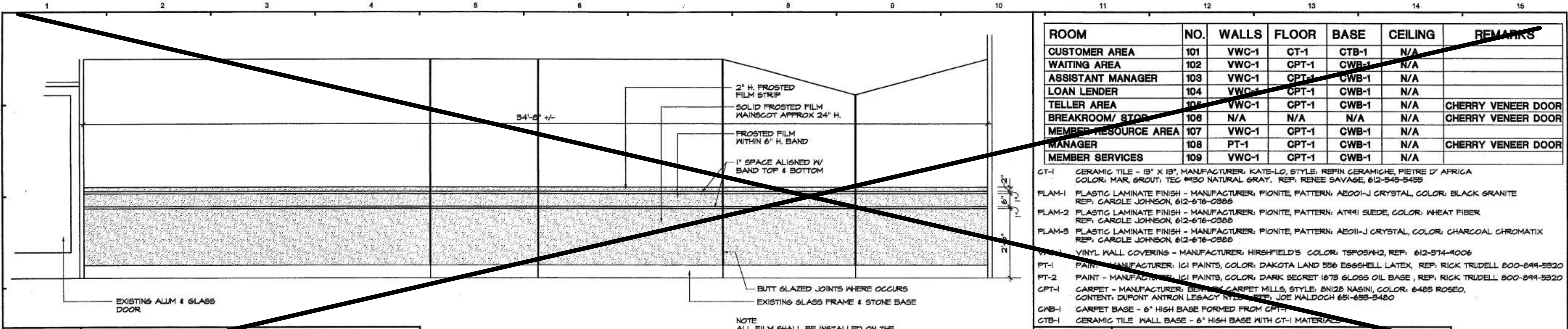
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REVISIONS	
NO.	DESCRIPTION

MSCAA No.: 19-1423-00 (19-1423-26)

PROJECT:
**TERMINAL C AND A
 TICKETING LEVEL
 RETAIL SPACES
 DEMOLITION AND
 REPAIR**

SET:
 SHEET TITLE:
**TERMINAL "C" RETAIL
 AREAS INFO FROM
 AIRPORT**



ROOM	NO.	WALLS	FLOOR	BASE	CEILING	REMARKS
CUSTOMER AREA	101	VWC-1	CT-1	CTB-1	N/A	
WAITING AREA	102	VWC-1	OPT-1	CWB-1	N/A	
ASSISTANT MANAGER	103	VWC-1	OPT-1	CWB-1	N/A	
LOAN LENDER	104	VWC-1	OPT-1	CWB-1	N/A	
TELLER AREA	105	VWC-1	OPT-1	CWB-1	N/A	CHERRY VENEER DOOR
BREAKROOM/STOR.	106	N/A	N/A	N/A	N/A	CHERRY VENEER DOOR
MEMBER RESOURCE AREA	107	VWC-1	OPT-1	CWB-1	N/A	
MANAGER	108	PT-1	OPT-1	CWB-1	N/A	CHERRY VENEER DOOR
MEMBER SERVICES	109	VWC-1	OPT-1	CWB-1	N/A	

CT-1 CERAMIC TILE - 18" X 18", MANUFACTURER: KATE-LO, STYLE: REFIN CERAMICHE, PIERRE D'AFRICA, COLOR: MAR, GROUT: TEC 950 NATURAL GRAY, REP: RENEE SAVAGE, 612-545-5455

FLAM-1 PLASTIC LAMINATE FINISH - MANUFACTURER: PIONITE, PATTERN: A5001-J CRYSTAL, COLOR: BLACK GRANITE, REP: GAROLE JOHNSON, 612-676-0389

FLAM-2 PLASTIC LAMINATE FINISH - MANUFACTURER: PIONITE, PATTERN: ATR91 SUEDE, COLOR: WHEAT FIBER, REP: GAROLE JOHNSON, 612-676-0389

FLAM-3 PLASTIC LAMINATE FINISH - MANUFACTURER: PIONITE, PATTERN: A501-J CRYSTAL, COLOR: CHARCOAL CHROMATIX, REP: GAROLE JOHNSON, 612-676-0389

VT-1 VINYL WALL COVERING - MANUFACTURER: HIRSHFIELD'S, COLOR: TSP08W2, REP: 612-874-4006

PT-1 PAINT - MANUFACTURER: ICI PAINTS, COLOR: DAKOTA LAND 556 EGGSHELL LATEX, REP: RICK TRUDELL, 800-844-5820

PT-2 PAINT - MANUFACTURER: ICI PAINTS, COLOR: DARK SECRET 1675 GLOSS OIL BASE, REP: RICK TRUDELL, 800-844-5820

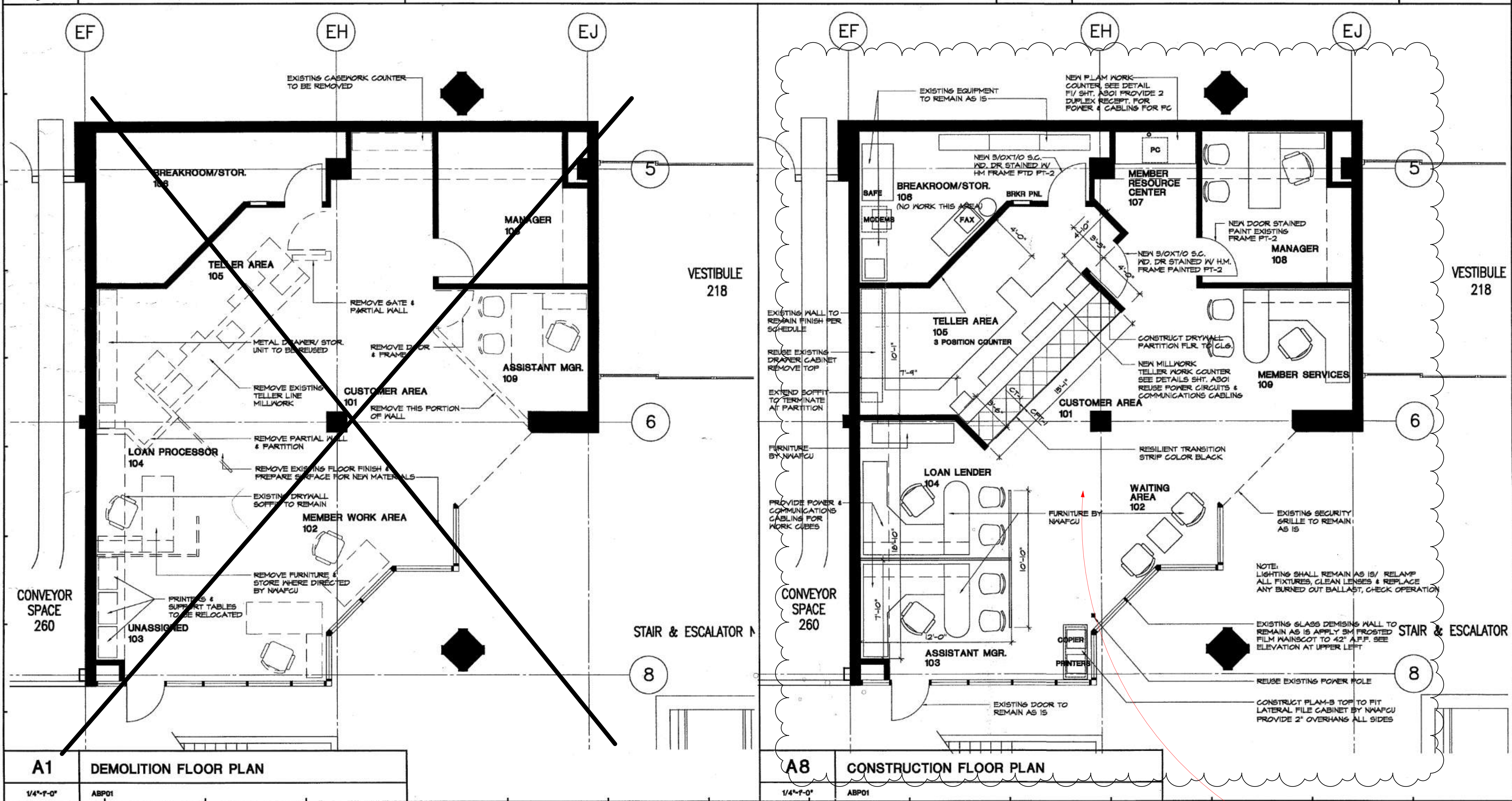
CPT-1 CARPET - MANUFACTURER: BENNETT CARPET MILLS, STYLE: 8N128 NASHI, COLOR: 6485 ROSEO, CONTENT: DUPONT ANTRON LEGACY, MANUFACTURER: JOE WALDOCH 651-635-5400

CWB-1 CARPET BASE - 6" HIGH BASE FORMED FROM CPT-1

CTB-1 CERAMIC TILE WALL BASE - 6" HIGH BASE WITH CT-1 MATERIAL

K11	FINISH SCHEDULE AND LEGEND
N.T.S.	ABP01

K1 STOREFRONT ELEVATION
1/2" = 1'-0"
MW-003



A1 DEMOLITION FLOOR PLAN
1/4" = 1'-0"
ABP01

A8 CONSTRUCTION FLOOR PLAN
1/4" = 1'-0"
ABP01

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ARCHITECT JOB NO.
20008-T26

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NO.	DATE	DESCRIPTION

**RENOVATION FOR:
NORTHWEST FEDERAL CREDIT UNION
MEMPHIS INTERNATIONAL AIRPORT
MEMPHIS, TENNESSEE**

SHEET TITLE
DEMOLITION FLOOR & FINISH PLAN DETAILS

DESIGNED BY: LES
CHECKED BY: LES
DATE: 12-16-2002

PROJECT NO.: 02003
SHEET NO.: **A201**

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NO.	DESCRIPTION	DATE

MSCAA No.
19-1423-00 (19-1423-26)

PROJECT:
**TERMINAL C AND A
TICKETING LEVEL
RETAIL SPACES
DEMOLITION AND
REPAIR**

SHEET TITLE:
**TERMINAL "C" RETAIL
AREA (WEST) INFO
FROM AIRPORT**

DATE:
01.07.2022

DWG NO.:
A1.4

THE DRAWING ABOVE IS PROVIDED FROM THE AIRPORT'S ARCHIVED RECORDS. IT IS INCLUDED IN THE SET FOR CONTRACTOR GENERAL REFERENCE OF EXISTING CONSTRUCTION. CONTRACTOR SHALL FIELD VERIFY THE ACCURACY OF EXISTING CONDITIONS.

SEE DRAWING A1.1 FOR SCOPE OF WORK AT THIS AREA



EXISTING GLASS PARTITION WALL AND BOTTOM CURBS TO BE REMOVED. REMOVE FLOOR TILES AND RESTORE THE ORIGINAL TERRAZZO FLOOR SYSTEM. SPOT REPAIR AS NEEDED.



EXISTING MILLWORK TO BE REMOVED. REMOVE FLOOR FINISHES AND RESTORE THE ORIGINAL TERRAZZO FLOOR SYSTEM. SPOT REPAIR AS NEEDED.



COORDINATE WITH AIRPORT AUTHORITY REGARDING MOVING OF EXISTING FURNITURE. THE AIRPORT AUTHORITY SHALL BE RESPONSIBLE TO REMOVE ANY REQUIRED FFE PRIOR TO DEMOLITION.



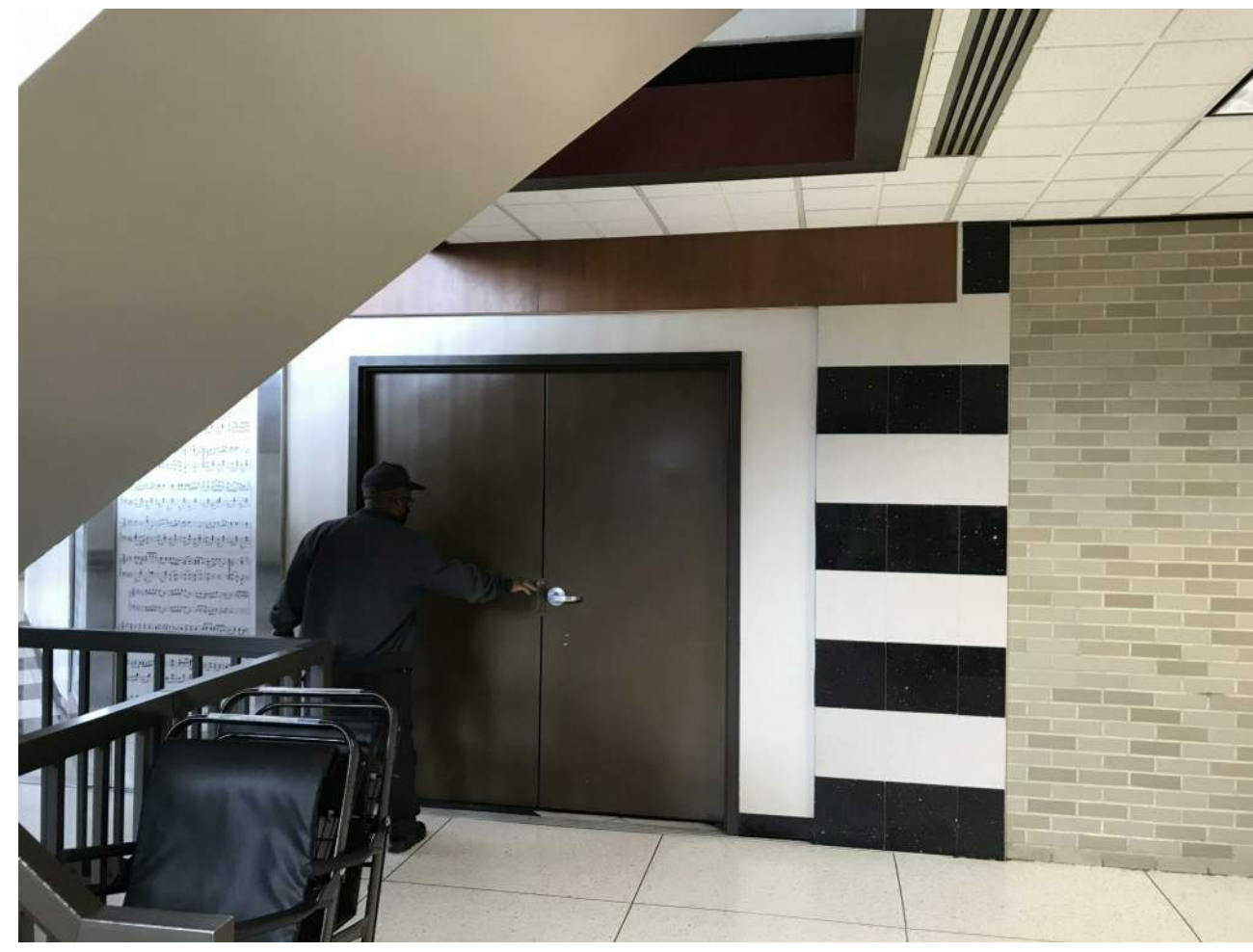
COORDINATE WITH AIRPORT AUTHORITY AND OBTAIN APPROVAL BEFORE REMOVING ANY UTILITY PANEL. OBSERVE OSHA LOCK OUT TAG OUT REQUIREMENTS AT ALL TIMES.



MAINTAIN A MINIMUM OF 36" WIDE PATH FOR PEDESTRIAN TRAFFIC.



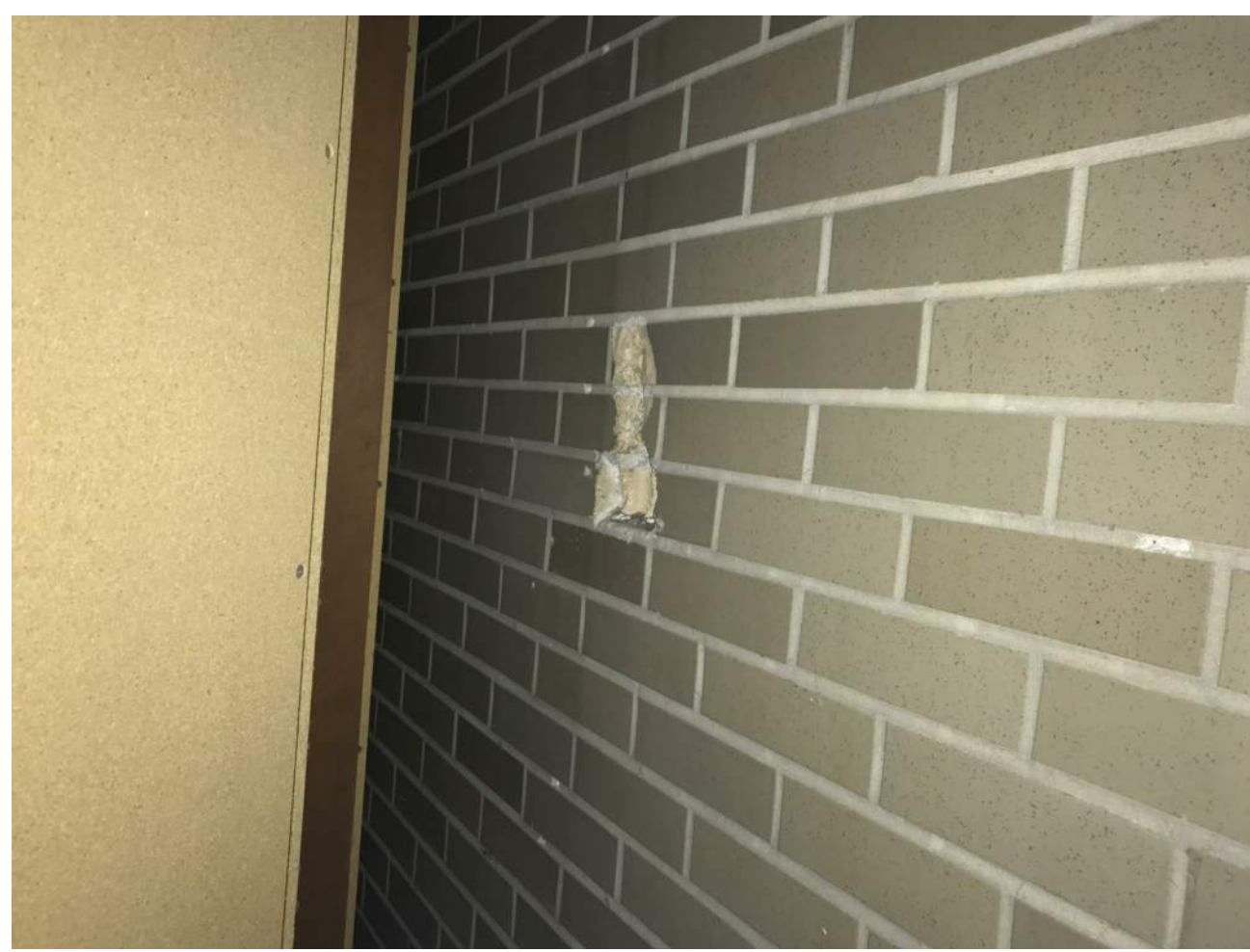
REMOVE ALL EXISTING SHELVING AND DISPLAY MILLWORK. REMOVE FLOOR AND CEILING TILE SYSTEM.



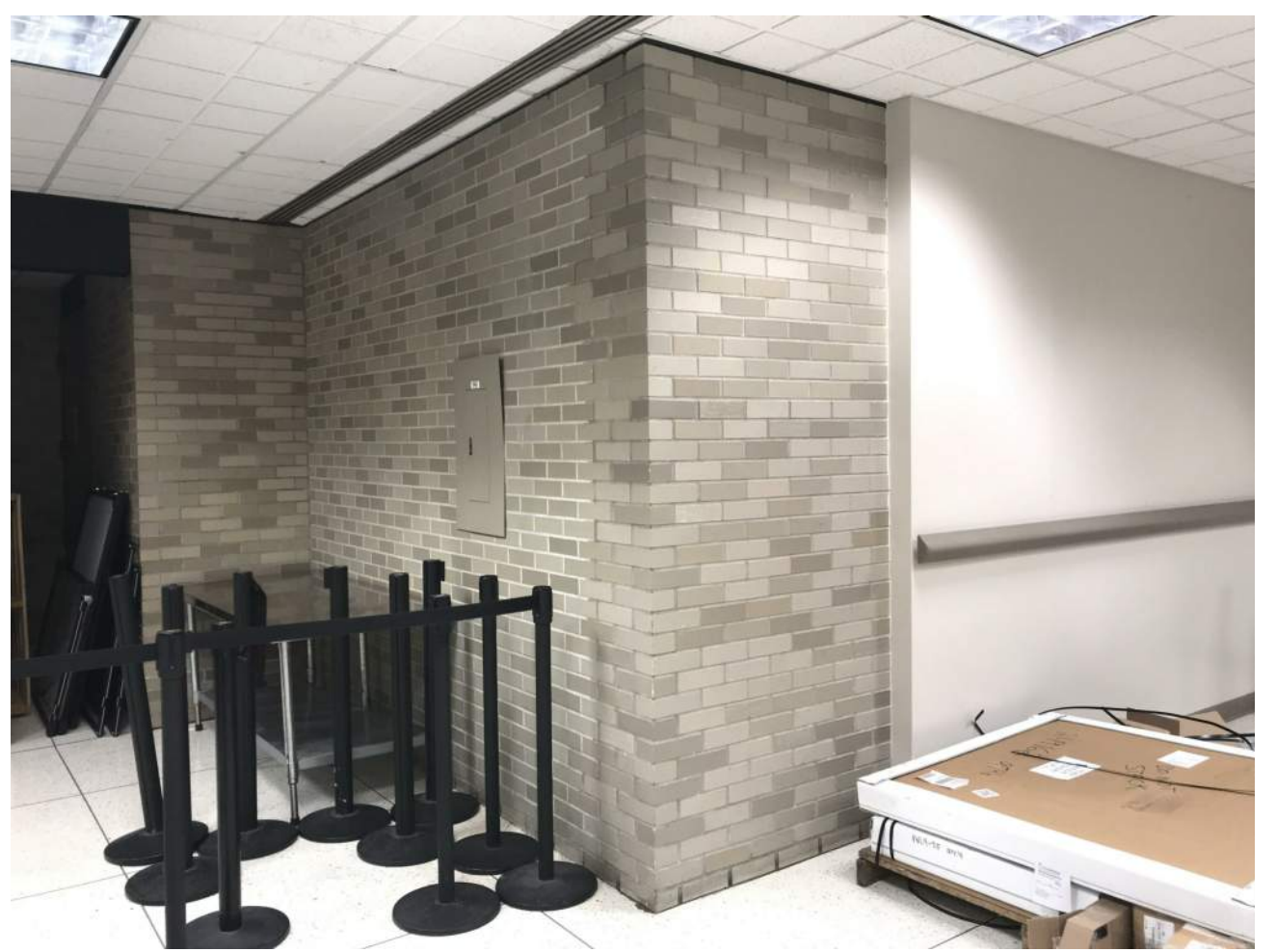
REMOVE VACANT RETAIL SPACE CONSTRUCTION UP TO THE BRICK VENEER LINE. TIE IN NEW CEILING GRID SYSTEM TO MATCH EXISTING 2X4 GRID SYSTEM, TILE TYPE AND ORIENTATION.



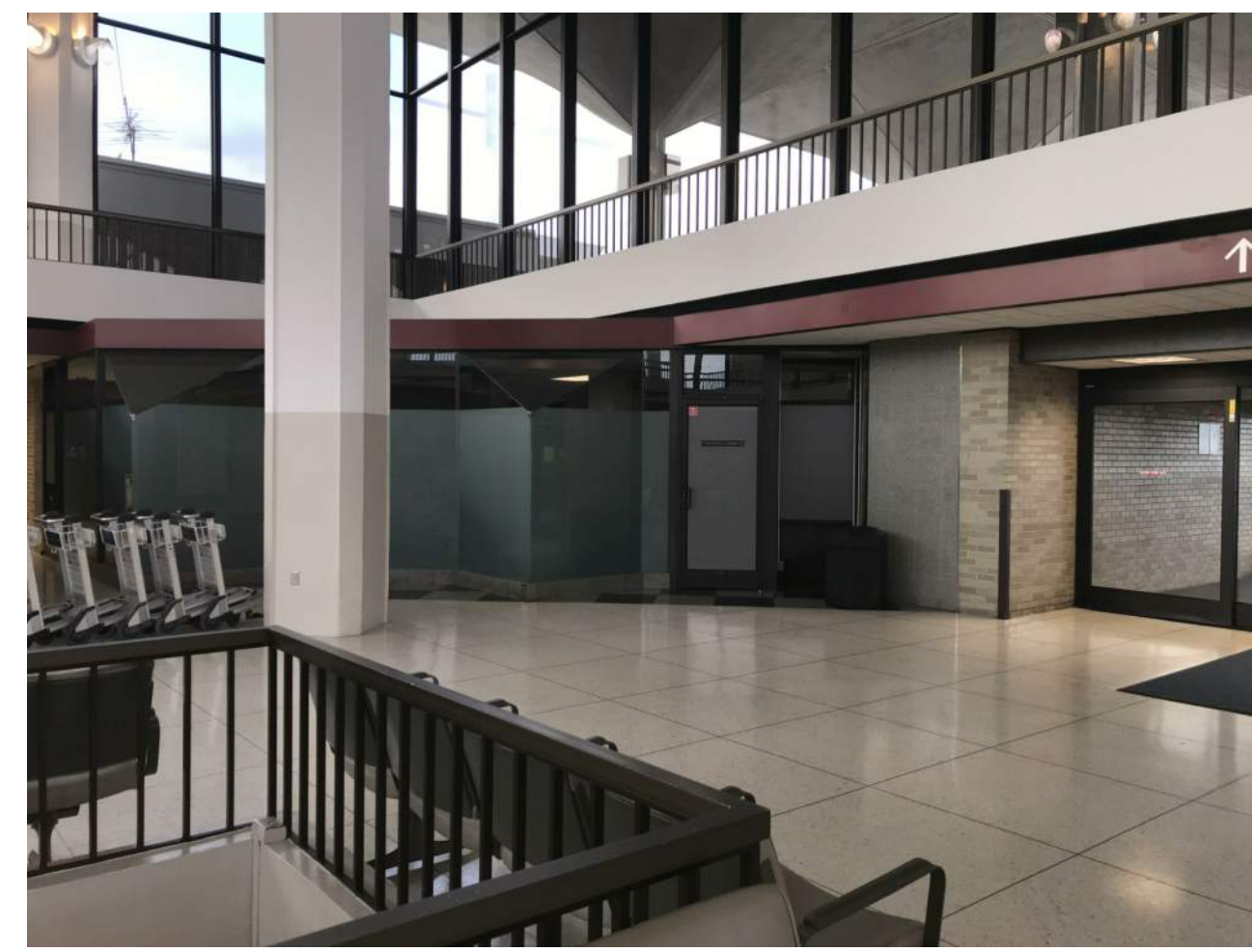
CONTRACTOR SHALL DETERMINE THE TIMING OF REMOVAL OF THE EXISTING BARRIER DUE TO FLOORING REMOVAL WORK AND CEILING STRUCTURE REMOVAL WORK.



SPOT REPAIR EXISTING BRICK VENEER FINISH AFTER REMOVAL OF INTERIOR FINISHES. THE AIRPORT AUTHORITY SHALL PROVIDE THE BRICK MASONRY UNITS NEEDED FOR THE REPAIR.



THE MEN'S ROOM WALL IS BRICK MASONRY CONSTRUCTION, IT WILL BE REMOVED AS PART OF THE SCOPE OF WORK OF WALL REMOVAL IN ALTERNATE 1. RETURN BRICK TO AIRPORT ONLY IF THE BRICK ARE SALVAGEABLE.



REMOVE SECONDARY CEILING FRAMING AT THE VACANT SPACE. INSTALL NEW COLOR CODED BANDING AT THE BOTTOM OF THE MEZZANINE TO MATCH EXISTING ADJACENT.



INSTALL WING WALL STAINLESS STEEL END CAP TO MATCH OPPOSITE WALL. PROTECT AREA OUTSIDE PROJECT LIMIT FROM DAMAGE DUE TO CONSTRUCTION ACTIVITIES. LIFE SAFETY AND EGRESS SHALL BE MAINTAINED AND SHALL NOT BE COMPROMISED AT ALL TIME.



REMOVE SECONDARY CEILING FRAMING AT THE VACANT SPACE. INSTALL NEW COLOR CODED BANDING AT THE BOTTOM OF THE MEZZANINE TO MATCH EXISTING ADJACENT.



CLOSE UP VIEW OF THE COLOR BAND. INSTALL NEW COLOR BANDING AT THE BOTTOM OF THE MEZZANINE TO MATCH EXISTING ADJACENT.



REMOVE ABANDONED NETWORK WIRING. CONFIRM WITH AIRPORT MAINTENANCE BEFORE REMOVAL. CAP OFF THE UTILITY CONNECTION FOR ORIGINAL APPLIANCE.



TIE IN NEW CEILING GRID SYSTEM IN THE NEWLY RESTORED SPACE TO MATCH EXISTING 2X4 GRID SYSTEM, TILE TYPE AND ORIENTATION. THE NEW LIGHT FIXTURES SHALL MATCH THE EXISTING 2X2 FIXTURES WITH THE 9 GRID PARABOLIC LENS.



CLEAN AND RESTORE THE ORIGINAL TERRAZZO FLOOR SYSTEM. SPOT REPAIR AS NEEDED.



REROUTE THE EXISTING DUCT WORK AND VAV BOX HOT WATER LINE AS NEEDED IN THE STORAGE ROOM BEHIND TO PROVIDE VERTICAL CLEARANCE FOR THE APPROXIMATE 9'-0" LAY IN CEILING HEIGHT TO MATCH ALL EXISTING ADJACENT CEILING.

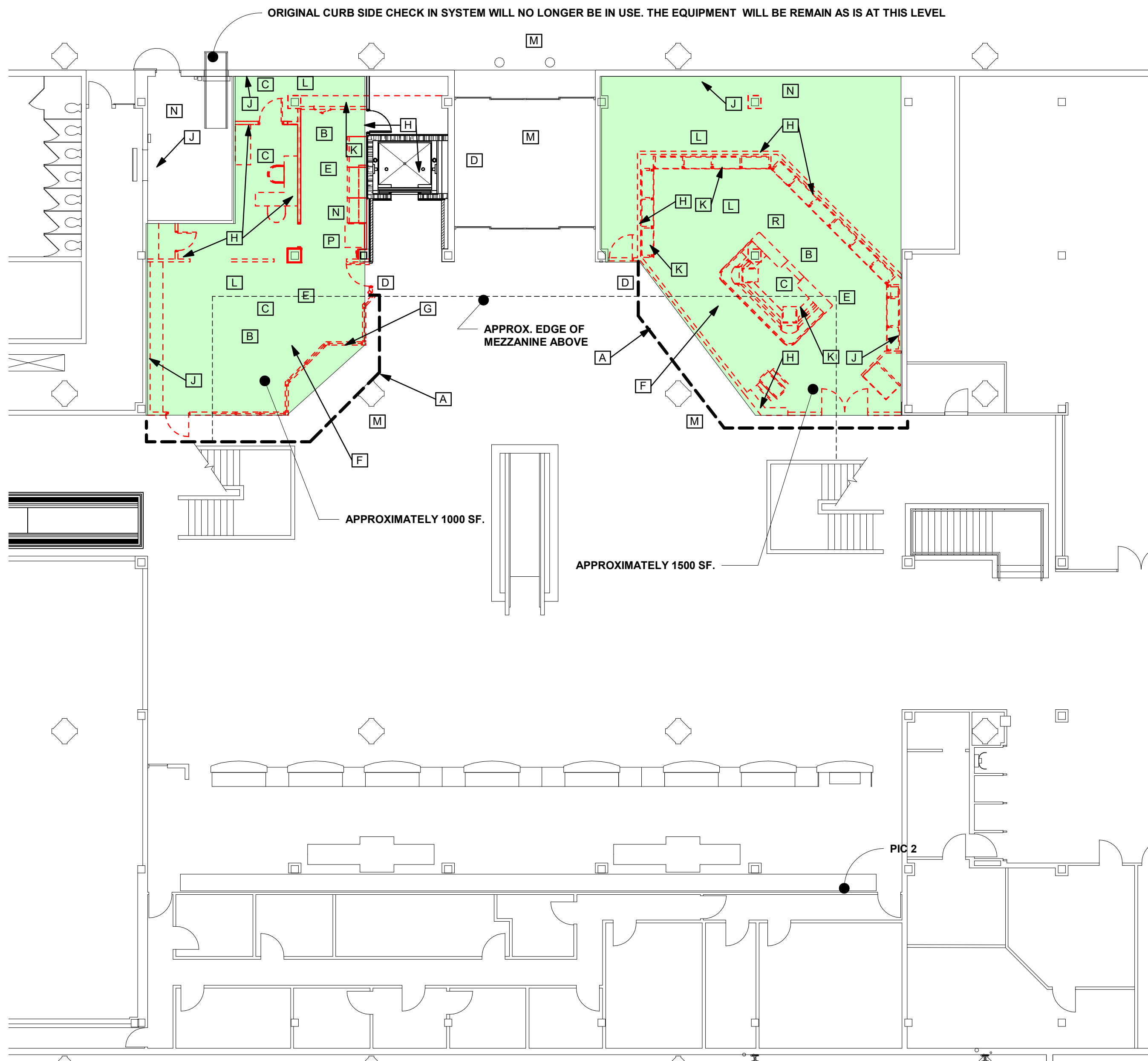


SPOT REPAIR EXISTING BRICK VENEER FINISH AFTER REMOVAL OF INTERIOR FINISHES. THE AIRPORT AUTHORITY SHALL PROVIDE THE BRICK MASONRY UNITS NEEDED FOR THE REPAIR.



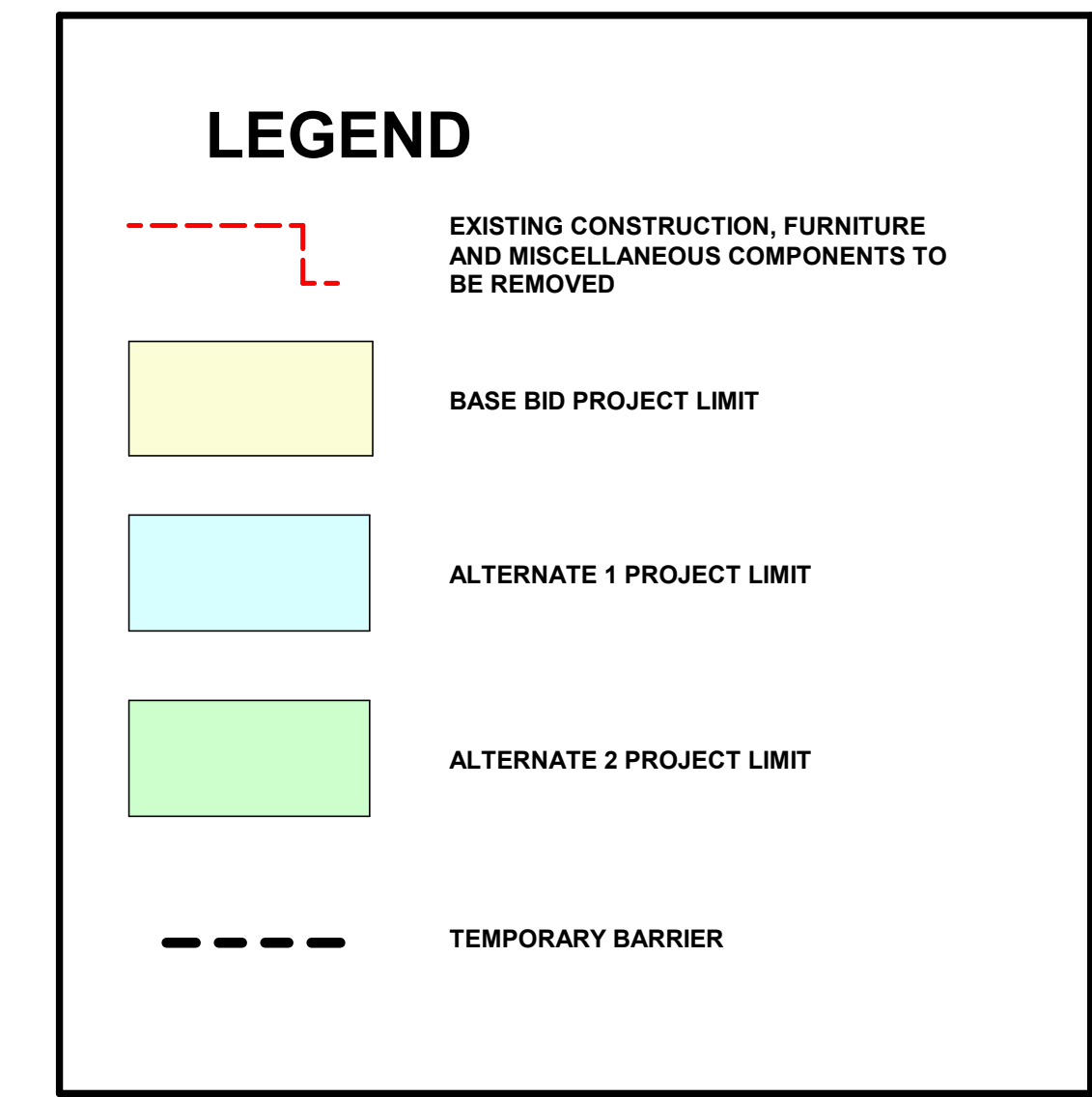
SPOT REPAIR EXISTING BRICK VENEER FINISH AFTER REMOVAL OF INTERIOR FINISHES. THE AIRPORT AUTHORITY SHALL PROVIDE THE BRICK MASONRY UNITS NEEDED FOR THE REPAIR.

Table with 3 columns: NO., DESCRIPTION, DATE. Under the heading REVISIONS.

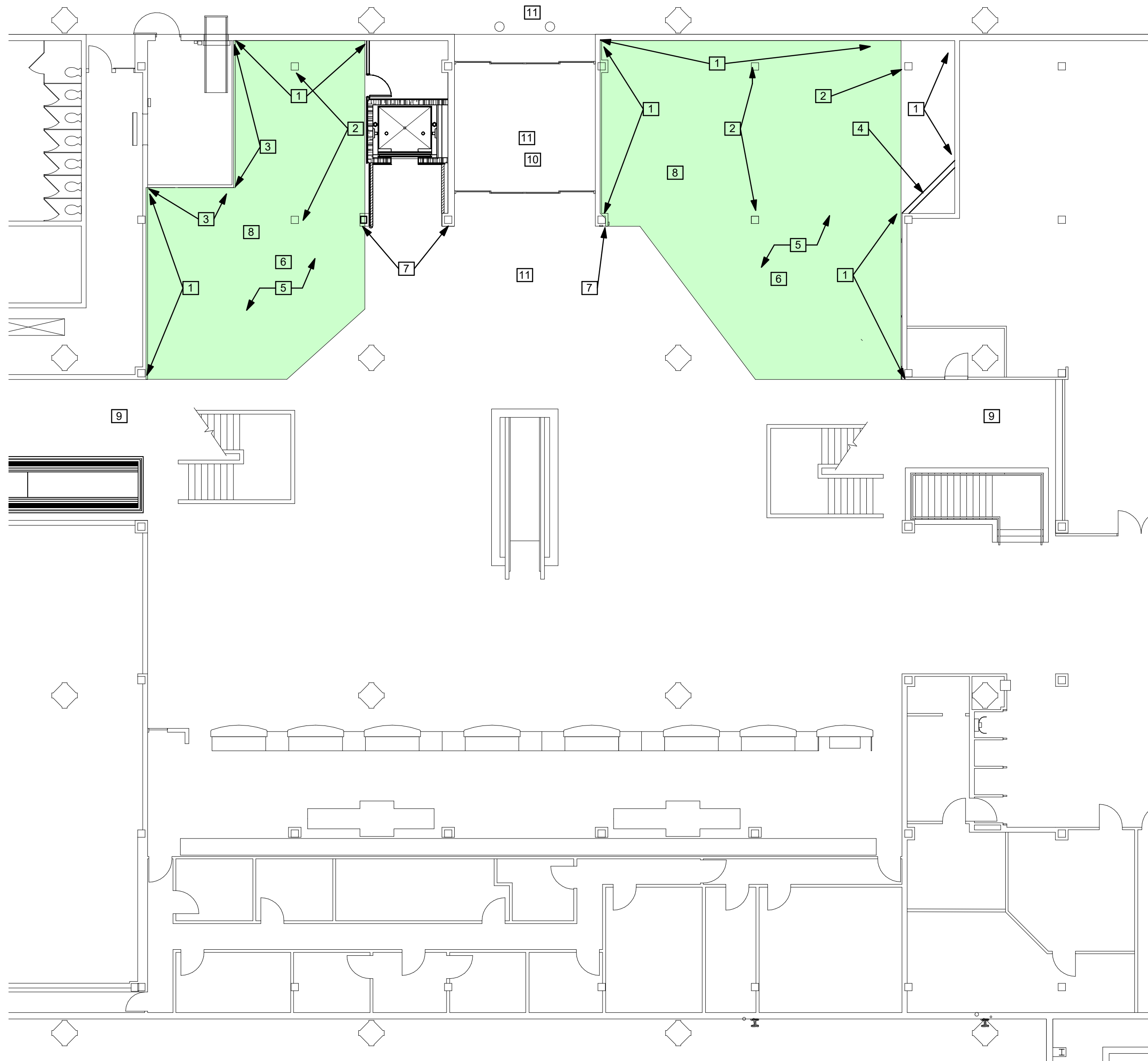


DEMOLITION NOTES:

- A. CONTRACTOR SHALL ERECT TEMPORARY BARRIER TO SEPARATE THE CONSTRUCTION TRAFFIC AND PASSENGER TRAFFIC AS SHOWN IN HEAVY DASHED LINES. ALL TEMPORARY WALLS SHALL BE CONSTRUCTED OF METAL STUDS; ANCHORED ON BOTTOM WITH DOUBLE STICK TAPE AND BRACED TO SOUND STRUCTURAL SUPPORT NEARBY ON TOP (ATTACHMENT TO CEILING TRILYS OR GRID IS PROHIBITED). THE OUTSIDE (PUBLIC SIDE) OF THE WALL SHALL BE FINISHED FLOOR TO CEILING WITH 5/8" DRYWALL WITH 2 COATS OF FLAT OFF WHITE LATEX PAINT. THE BOTTOM OF THE WALL SHALL BE TRIMMED WITH 4" BLACK COVE BASE. A 2"X6" CHAIR RAIL, BLOCKED 2" OFF THE WALL, SHALL BE INSTALLED 36" TO CENTER ABOVE FINISHED FLOOR. THE CHAIR RAIL SHALL BE SANDED AND PAINTED WITH TWO COATS OF BRONZE PAINT (PPG PITT-TECH ACRYLIC HIGH GLOSS DTM INDUSTRIAL ENAMEL - BRONZE TONE). SEE ATTACHED PICTURE "A" FOR EXAMPLE. THE CONTRACTOR SHALL RETAIN RESPONSIBILITY DURING CONSTRUCTION TO MAINTAIN THE WALL FOR AESTHETIC AND SECURITY ISSUES. MSCAA DEVELOPMENT DIVISION SHALL APPROVE ITS LOCATION, ANY ATTACHMENTS TO TERRAZZO FLOORS AND ALL SIGNAGE AND/OR GRAPHICS. WALK-OFF MATS SHALL BE USED AT ALL ACCESS POINTS TO THE CONSTRUCTION AREA TO PREVENT TRACKING OF DUST AND DEBRIS AND IS RESPONSIBLE FOR CLEANUP IF ANY DIRT, DUST AND/OR DEBRIS GETS OUTSIDE THEIR CONSTRUCTION LIMITS.
- B. CAREFULLY REMOVE ALL EXISTING FLOOR FINISHES WITHIN THE PROJECT LIMIT TO ORIGINAL TERRAZZO OR CONCRETE FLOOR. IT IS THE AIRPORT'S INTENT TO PRESERVE AND REUSE THE ORIGINAL TERRAZZO FLOOR WHEREVER FEASIBLE WITHIN THE PROJECT LIMIT.
- C. THE AIRPORT AUTHORITY WILL REMOVE ALL MOVABLE FURNITURE BEFORE COMMENCEMENT OF DEMOLITION WORK. ANY REMAINING ITEMS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR REMOVAL OR DISPOSAL. RETURN THE LOCK ASSEMBLY TO THE LOCK SHOP.
- D. COORDINATE WITH THE AIRPORT AUTHORITY THE HAUL ROUTE OF DISPOSAL TRAFFIC. IT IS OF UTMOST IMPORTANCE TO ENSURE NO CONSTRUCTION DEBRIS SUCH AS PACKAGING MATERIAL OR SHARP OBJECT BE LEFT AT ANY PUBLIC AREA OR DRIVEWAY. IF HAULING OF CONSTRUCTION DEBRIS MUST COME THRU THE ENTRANCE VESTIBULE, CONTRACTOR MUST PROTECT THE WALK OFF MAT AT THE ROUTE WITH PLYWOOD SO AS NOT TO DAMAGE THE EXISTING WALKOFF MAT.
- E. REMOVE AND DISPOSE ALL EXISTING CEILING GRIDS, GYPSUM BOARD FURR DOWN STRUCTURE CEILING TILES, AND LIGHT FIXTURES WITHIN THE PROJECT LIMIT. CONTRACTOR TO PROTECT THE ELECTRICAL CIRCUITS PRIOR TO DEMOLITION AND COMMENCEMENT OF NEW WORK. DO NOT CUT ANY WIRE UNLESS APPROVED BY THE AIRPORT AUTHORITY. CONTRACTOR SHALL ALSO PRESERVE THE EXISTING HVAC DUCTWORK FOR CAPPING OR REROUTING. DO NOT REMOVE ANY DUCT SYSTEM UNLESS APPROVED BY AIRPORT. BLOCK RETURN AIR TEMPORARILY TO PREVENT CONSTRUCTION DUST AND DEBRIS FROM TRAVELING TO OTHER AIRPORT AREA.
- F. CAREFULLY REMOVE THE EXISTING ADD-ON SECONDARY CEILING FRAMING UNDER AND OUTSIDE THE BALCONY. PROTECT THE ADJACENT CONSTRUCTION WHEREVER POSSIBLE
- G. CAREFULLY REMOVE ALL EXISTING FEATURE GLASS WALL AND CURB.
- H. REMOVE ANY VERTICAL WALL PARTITION AND METAL FRAMING WITHIN THE PROJECT LIMIT.
- I. NOT USED
- J. EXERCISE CAUTION DURING REMOVAL OF VERTICAL PARTITION WHEREVER THERE IS AIRPORT ORIGINAL BRICK WALL BEHIND. IT IS THE OWNER'S INTENT TO PRESERVE THOSE FINISHES WHEREVER POSSIBLE.
- K. REMOVE AND DISPOSE ALL EXISTING RETAIL SHELVING, OWNER MILLWORK, TRANSACTION COUNTER ETC.
- L. PROTECT AND COVER EXISTING RETURN AIR OPENING WITHIN THE PROJECT LIMIT SO THE FINE DUST AND DEBRIS DO NOT TRAVEL THROUGH THE HVAC SYSTEM TO OTHER AREAS.
- M. DOCUMENT ANY EXISTING DAMAGE AT THE AREA WITHIN AND ADJACENT TO THE WORK AREA TO AVOID ANY DISPUTE OF DAMAGES
- N. CAP ANY EXISTING UTILITY AND DRAINLINE WHEREVER POSSIBLE FOR FUTURE REUSE. CAP THE UTILITY BELOW TICKETING LEVEL FLOOR DECK OR BEHIND WALL.
- O. NOT USED.
- P. PROVIDE PLUMBING CUT OFF VALVE BEFORE CUTTING OF ANY UTILITY. COORDINATE WITH AIRPORT MAINTENANCE IF TEMPORARY SHUT OFF OF UTILITIES IS NEEDED IN ORDER TO PERFORM THE WORK
- Q. REMOVE ALL EXISTING TOILET FIXTURES, TOILET PARTITIONS, AND TOILET ACCESSORIES.



1 **TERMINAL A - RETAIL SPACES - DEMOLITION**
3/32" = 1'-0"



NEW WORK AND REPAIR NOTES:

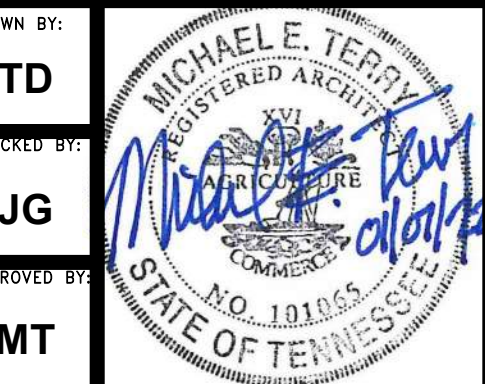
1. SPOT REPAIR EXISTING ORIGINAL BRICK VENEER WALL SURFACE. TOOTH IN MASONRY UNITS PROVIDED BY AIRPORT AUTHORITY. COORDINATE WITH AIRPORT FOR OBTAINING THE BRICK FOR REPAIR. CLEAN BRICK WALL FROM CORNER TO CORNER TO RESTORE THE CLEANLINESS OF THE SURFACE. MATCH MORTAR COLOR AT REPAIR.
2. EXISTING INTERIOR 2'X2' BRICK WRAPPED COLUMN. REPAIR AS NEEDED. CLEAN THE SURFACE THE SAME MANNER AS OTHER BRICK VENEER.
3. IF EXISTING WALL BOARD CANNOT BE RESTORED FOR PAINTING SATISFACTORILY, INSTALL ONE LAYER OF NEW 5/8" GYPSUM WALL BOARD. MUD, SAND AND APPLY 3 COATS OF PAINT COATING. COLOR TO BE SELECTED BY AIRPORT. INSTALL WALL BASE AND BUMPER RAIL. WALL BASE TO BE MATCH EXISTING. BUMPER RAIL TO MATCH EXISTING.
4. NEW WALL PARTITION WITH 3 5/8" 22GA METAL STUD FRAMING AT 16" O.C. INSTALL 1 LAYER OF 5/8" GYPSUM WALL BOARD ON BOTH SIDES. FILL WALL CAVITY WITH BATT INSULATION. INSTALL WALL BASE AND BUMPER RAIL. WALL BASE TO BE MATCH EXISTING. BUMPER RAIL TO MATCH EXISTING.
5. CLEAN, SAND, AND POLISH AND RESTORE THE UNCOVERED ORIGINAL TERRAZZO FLOOR. SPOT REPAIR AS NEEDED WHERE DAMAGE WAS CAUSED BY PREVIOUS CONSTRUCTION OR UTILITY PENETRATIONS. SPOT REPAIR TO MATCH EXISTING ADJACENT FINISH WHEREVER FEASIBLE.
6. INSTALL THE FEATURE PVC COLOR BANDING TO MATCH THE EXISTING ADJACENT TUBELITE 3MM CRANBERRY OR CACTUS GREEN BAND. ADHERE WITH DOUBLE SIDED TAPE.
7. STAINLESS STEEL WALL END CAP TO MATCH THE APPEARANCE OF THE END CAP AT OPPOSITE WALL END.
8. INSTALL NEW 15/16" 2'X4' CEILING GRID SYSTEM WITH NEW CEILING TILES TO MATCH LAYOUT AND ORIENTATION OF ADJACENT LAY-IN CEILING SYSTEM. BLEND TILES AS NEEDED. THE CEILING TILES TO BE ARMSTRONG 2'X4' CORTEGA SECOND LOOK I NO. 2765. TIE IN TO EXISTING SYSTEM WHEREVER FEASIBLE. UNLESS NOTED OTHERWISE, CEILING HEIGHT TO BE MINIMUM 9' AFF. MATCH ADJACENT.
9. PROTECT EXISTING NEARBY LIGHTING FIXTURES AND LIFE SAFETY SYSTEM IMMEDIATELY OUTSIDE PROJECT LIMITS. THESE ITEMS TO REMAIN AS IS. ANY SENSOR, DETECTOR, ALARM, AND ANY CEILING OR WALL MOUNTED DEVICES AT THE WORK AREA MUST BE PROPERLY PROTECTED FROM CONSTRUCTION DAMAGE. FIRE AND LIFE SAFETY MUST NOT BE COMPROMISED AT ALL TIMES.
10. PROTECT AND MAINTAIN ALL EXISTING EGRESS ACCESS UNLESS NOTED OTHERWISE
11. AREAS IMMEDIATELY OUTSIDE PROJECT LIMIT MUST BE PROPERLY PROTECTED FROM CONSTRUCTION TRAFFIC UNTIL THE COMPLETION OF CONSTRUCTION.

2 **TERMINAL A - RETAIL SPACES - NEW WORK & REPAIR**
3/32" = 1'-0"

NOTE:
ALL WORK ON THIS TERMINAL A AREA SHALL BE CONSIDERED AS ALTERNATE 2.



ARCHITECT JOB NO. 20008-T26



CONSULTANT JOB NO. ---

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REVISIONS		
NO.	DESCRIPTION	DATE

MSCAA No. 19-1423-00 (19-1423-26)

PROJECT: **TERMINAL C AND A TICKETING LEVEL RETAIL SPACES DEMOLITION AND REPAIR**

SET:

SHEET TITLE: **TERMINAL "A" RETAIL AREAS DEMOLITION**

REV. FILE NAME:

DATE: 01.07.2022 DWG NO.: A2.1

SCALE: As Indicated

1/7/2022 11:38:32 AM

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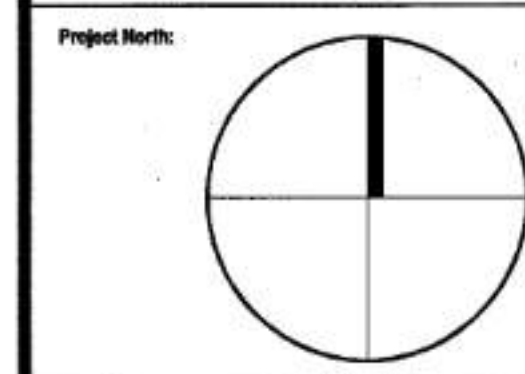
ARCHITECT
TRANSYSTEMS CORPORATION

ENGINEER
JOHN J. GUTH ENGINEERING, P.C.
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 TEL. (913) 967-4300
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CONSULTANT

Stamp:
[Signature]
 04/04/05

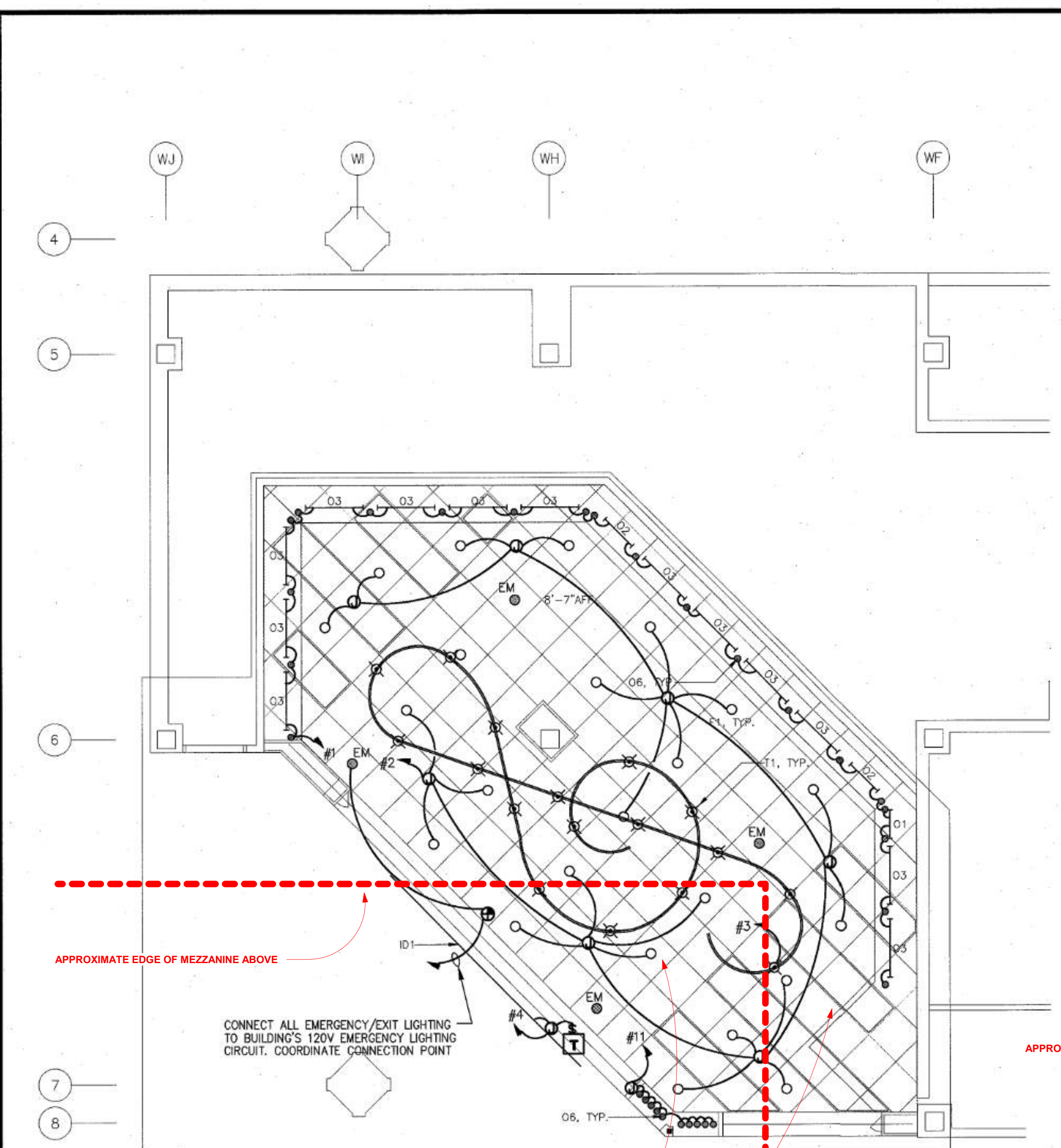
Date	Issue To	Revision / Classification	For
02/25/05	GC		CM_BID
04/01/05	MSCAA/MSCCOE		PERMIT



Project:
HUDSON GROUP
 SPACES 337
 MAIN TERMINAL LAND SIDE
 MEMPHIS INT'L AIRPORT
 MEMPHIS, TN 38116

Sheet Title:
ELECTRICAL
 LIGHTING & POWER PLANS

Date:	02/25/05	Project No.:	24236
Scale:	AS NOTED	Sheet No.:	E
Drawn by:	MS		
Checked by:			
Date Released:			101



PROVIDE 4 POLE MECHANICALLY HELD LIGHTING CONTACTOR. PROVIDE SQUARE D MODEL LXG40. ROUTE CIRCUITS 1-3 THROUGH CONTACTOR. CONTACTOR COIL CIRCUIT PP#8.

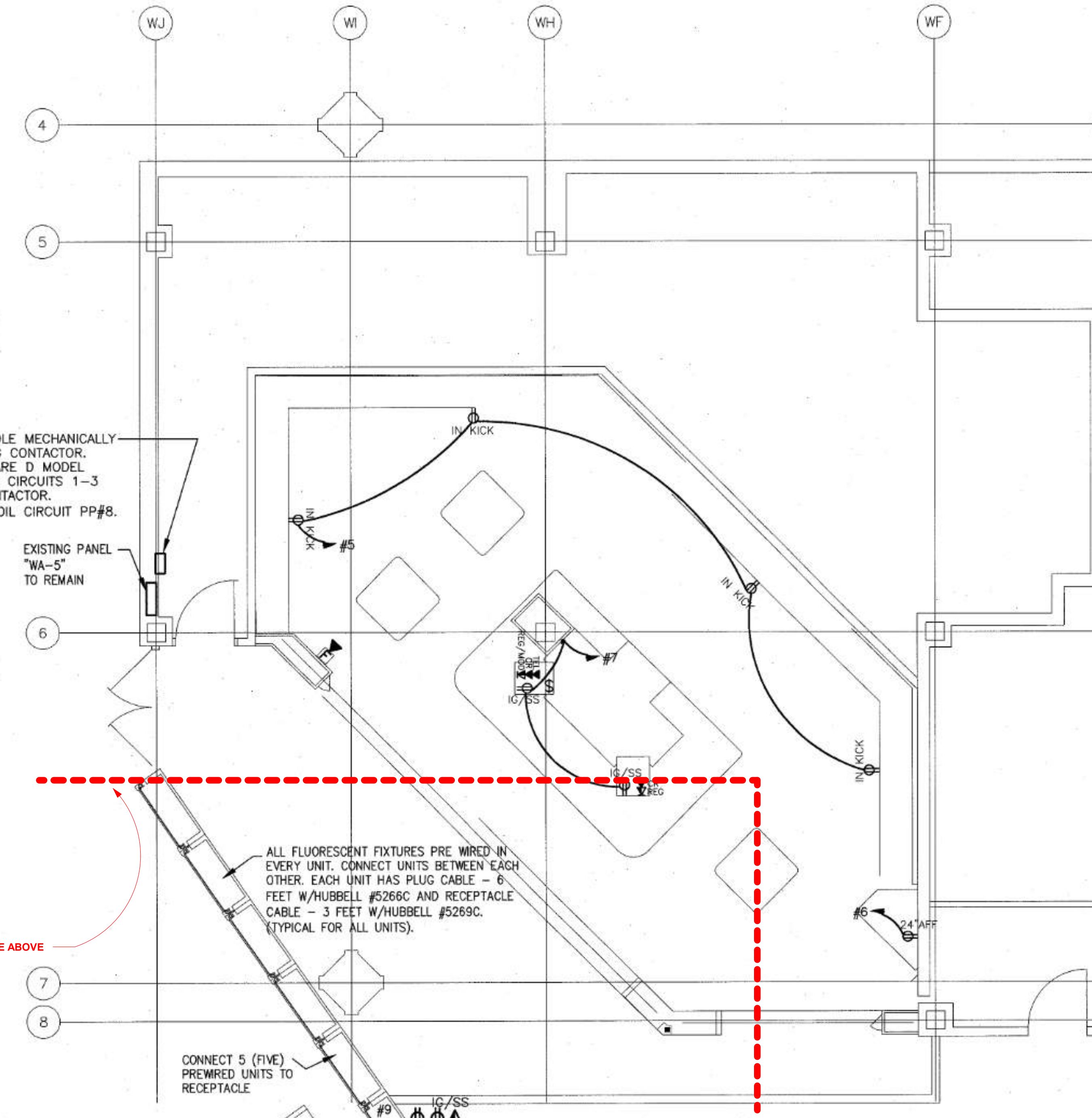
EXISTING PANEL "WA-5" TO REMAIN

APPROXIMATE EDGE OF MEZZANINE ABOVE

APPROXIMATE EDGE OF MEZZANINE ABOVE

CONNECT 5 (FIVE) PREWIRED UNITS TO RECEPTACLE

SCALE: 1/4"=1'-0"



ALL FLUORESCENT FIXTURES PRE WIRED IN EVERY UNIT. CONNECT UNITS BETWEEN EACH OTHER. EACH UNIT HAS PLUG CABLE - 6 FEET W/HUBBELL #5269C AND RECEPTACLE CABLE - 3 FEET W/HUBBELL #5269C. (TYPICAL FOR ALL UNITS).

CONNECT 5 (FIVE) PREWIRED UNITS TO RECEPTACLE

SCALE: 1/4"=1'-0"

LIGHTING PLAN
 SCALE: 1/4"=1'-0"

POWER PLAN
 SCALE: 1/4"=1'-0"

ALL EXISTING SECONDARY CEILING FRAMING, EXISTING CEILING GRIDS, CEILING TILES, AND LIGHT FIXTURES TO BE REMOVED. PROTECT ANY LIFE SAFETY DEVICES AND OBTAIN APPROVAL FROM AIRPORT BEFORE REMOVAL.

THE DRAWING ABOVE IS PROVIDED FROM THE AIRPORT'S ARCHIVED RECORDS. IT IS INCLUDED IN THE SET FOR CONTRACTOR GENERAL REFERENCE OF EXISTING CONSTRUCTION. CONTRACTOR SHALL FIELD VERIFY THE ACCURACY OF EXISTING CONDITIONS.



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ARCHITECT JOB NO.
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NO.	DESCRIPTION	DATE

MSCAA No. 19-1423-00 (19-1423-26)

PROJECT:
TERMINAL C AND A TICKETING LEVEL RETAIL SPACES DEMOLITION AND REPAIR

SET:

SHEET TITLE:
TERMINAL "A" RETAIL AREAS (EAST) INFO FROM AIRPORT

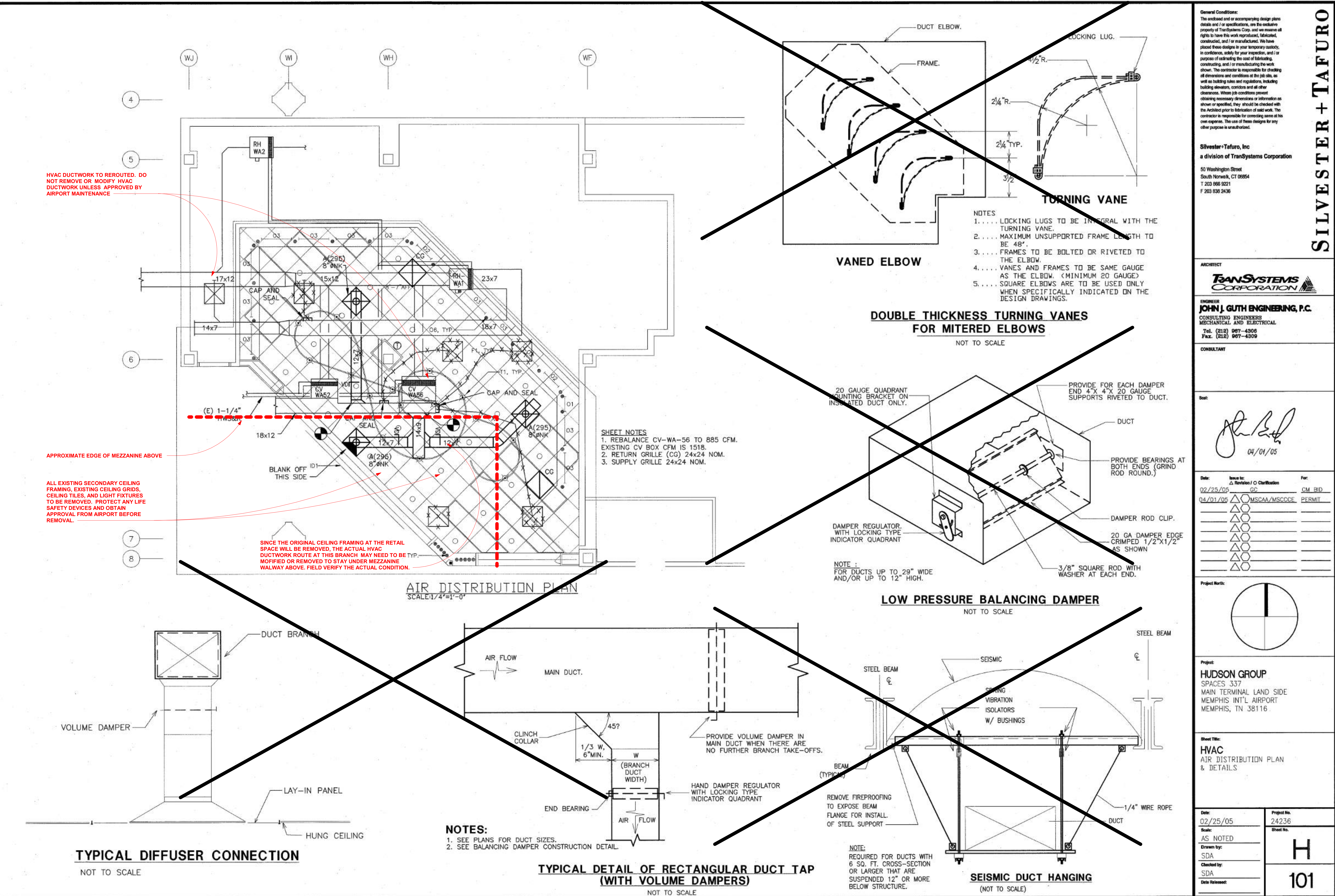
REV. FILE NAME
 DATE: 01.07.2022 DWG NO. A2.2
 SCALE

HVAC DUCTWORK TO REROUTED. DO NOT REMOVE OR MODIFY HVAC DUCTWORK UNLESS APPROVED BY AIRPORT MAINTENANCE

APPROXIMATE EDGE OF MEZZANINE ABOVE

ALL EXISTING SECONDARY CEILING FRAMING, EXISTING CEILING GRIDS, CEILING TILES, AND LIGHT FIXTURES TO BE REMOVED. PROTECT ANY LIFE SAFETY DEVICES AND OBTAIN APPROVAL FROM AIRPORT BEFORE REMOVAL.

SINCE THE ORIGINAL CEILING FRAMING AT THE RETAIL SPACE WILL BE REMOVED, THE ACTUAL HVAC DUCTWORK ROUTE AT THIS BRANCH MAY NEED TO BE MODIFIED OR REMOVED TO STAY UNDER MEZZANINE WALKWAY ABOVE. FIELD VERIFY THE ACTUAL CONDITION.



THE DRAWING ABOVE IS PROVIDED FROM THE AIRPORT'S ARCHIVED RECORDS. IT IS INCLUDED IN THE SET FOR CONTRACTOR GENERAL REFERENCE OF EXISTING CONSTRUCTION. CONTRACTOR SHALL FIELD VERIFY THE ACCURACY OF EXISTING CONDITIONS.

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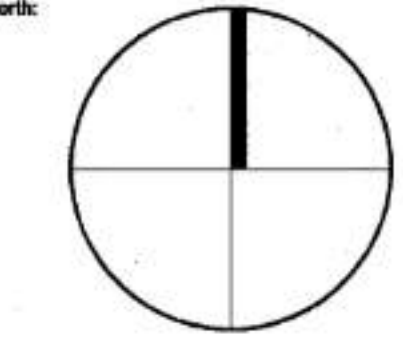
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CONSULTANT

04/01/05
John Guth

Date:	Issue No.:	Revision / C	Classification	For:
02/25/05			GC	CM BID
04/01/05			MSCAA/MSCOCC	PERMIT



Project Name:
HUDSON GROUP
SPACES 337
MAIN TERMINAL LAND SIDE
MEMPHIS INT'L AIRPORT
MEMPHIS, TN 38116

Sheet Title:
HVAC
AIR DISTRIBUTION PLAN
& DETAILS

Date: 02/25/05
Scale: AS NOTED
Drawn by: SDA
Checked by: SDA
Date Released:

Project No. 24236
Sheet No. H 101



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ARCHITECT JOB NO. 20008-T26

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REVISIONS

NO.	DESCRIPTION	DATE

MSCAA No. 19-1423-00 (19-1423-26)

PROJECT:

TERMINAL C AND A TICKETING LEVEL RETAIL SPACES DEMOLITION AND REPAIR

SHEET TITLE:

TERMINAL "A" RETAIL AREA (EAST) INFO FROM AIRPORT

DATE: 01.07.2022 DWG NO. A2.3



EXISTING GLASS PARTITION WALL AND BOTTOM CURBS TO BE REMOVED. REMOVE FLOOR TILES AND RESTORE THE ORIGINAL TERRAZZO FLOOR SYSTEM. SPOT REPAIR AS NEEDED.



EXISTING MILLWORK TO BE REMOVED. REMOVE FLOOR FINISHES AND RESTORE THE ORIGINAL TERRAZZO FLOOR SYSTEM. SPOT REPAIR AS NEEDED.



COORDINATE WITH AIRPORT AUTHORITY REGARDING MOVING OF EXISTING FURNITURE. THE AIRPORT AUTHORITY SHALL BE RESPONSIBLE TO REMOVE ANY REQUIRED FFE PRIOR TO DEMOLITION.



COORDINATE WITH AIRPORT AUTHORITY AND OBTAIN APPROVAL BEFORE REMOVING ANY UTILITY PANEL. OBSERVE OSHA LOCK OUT TAG OUT REQUIREMENTS AT ALL TIME.



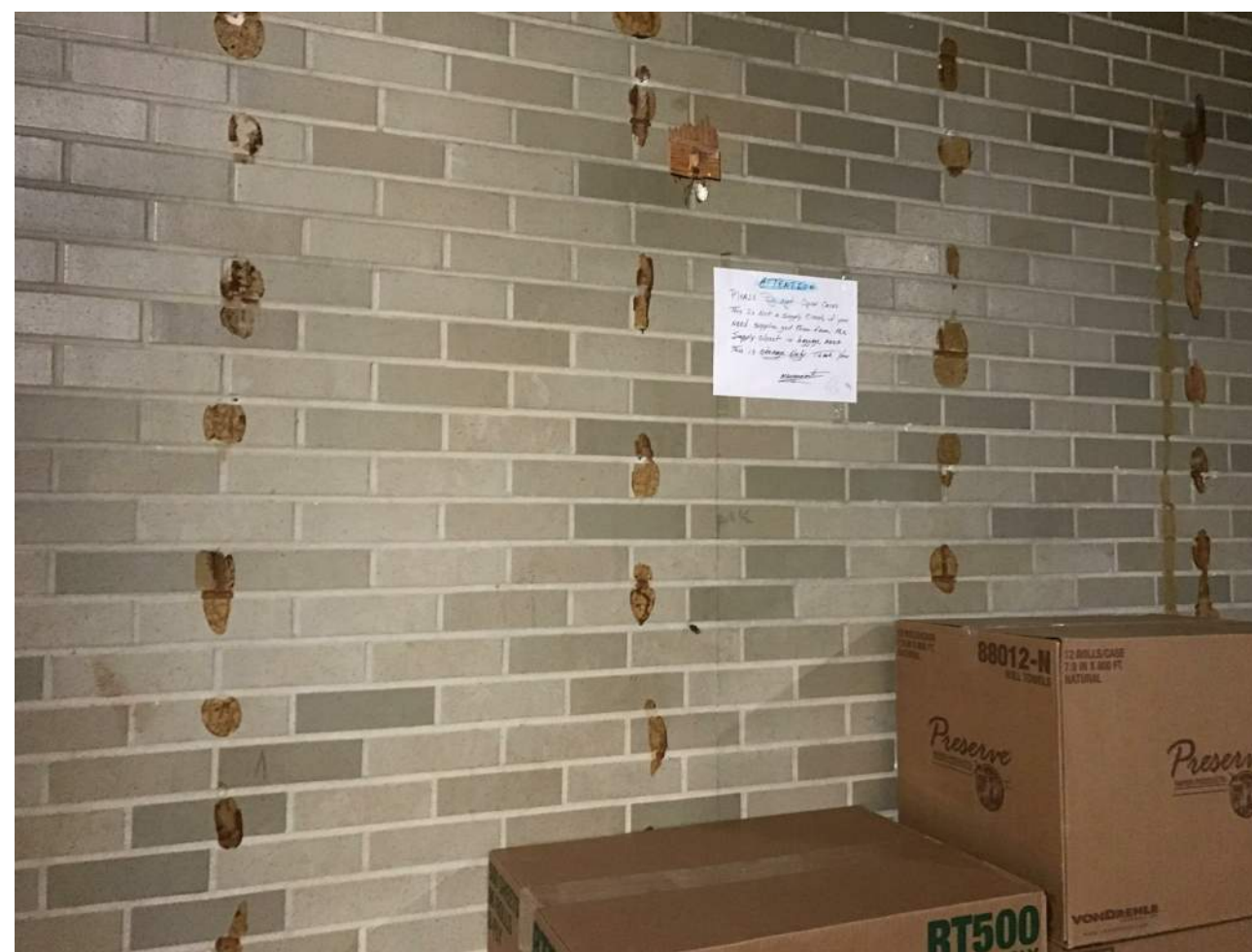
REMOVE ALL EXISTING SHELVING AND DISPLAY MILLWORK. REMOVE FLOOR AND CEILING TILE SYSTEM.



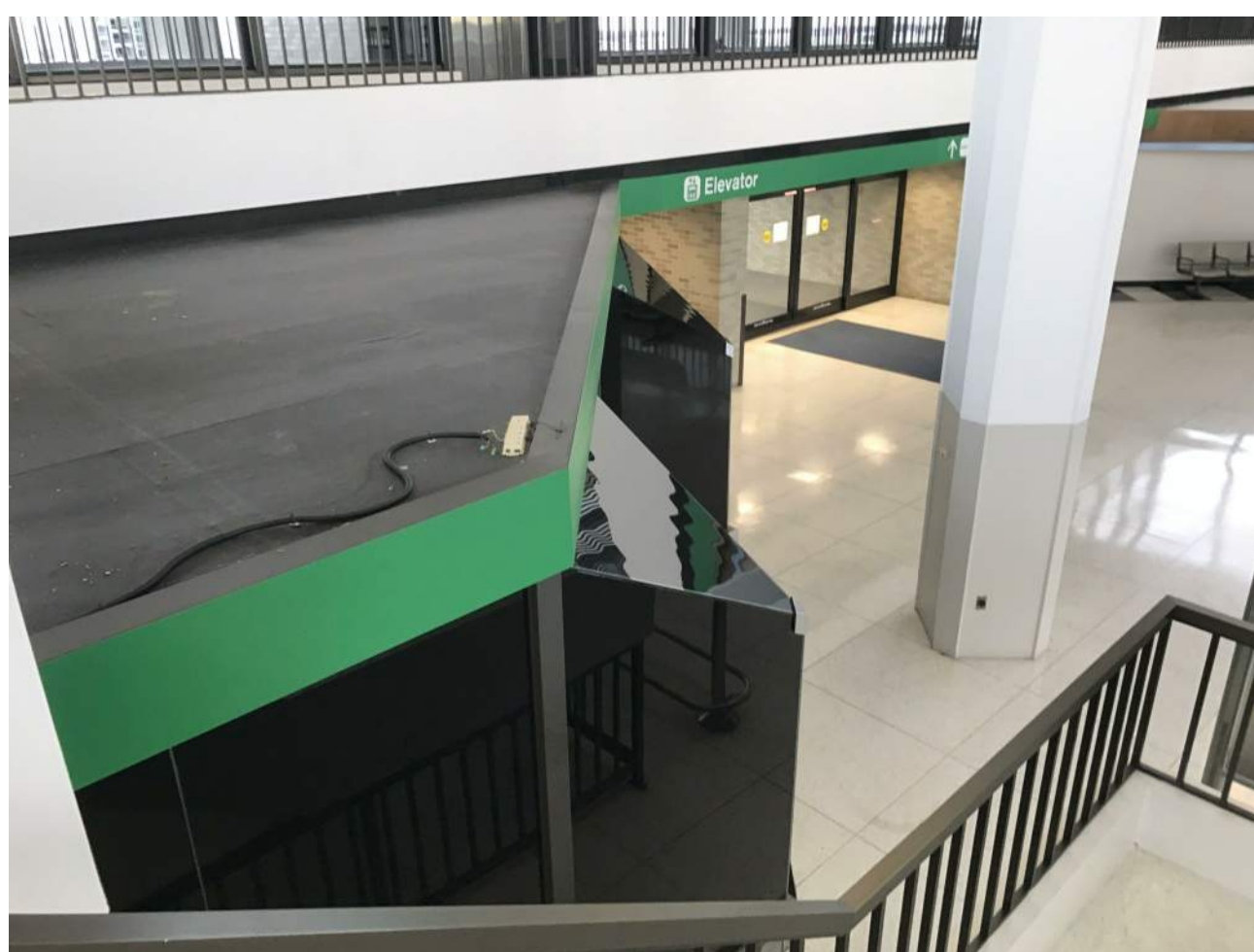
COORDINATE WITH AIRPORT AUTHORITY REGARDING MOVING OF EXISTING FURNITURE. THE AIRPORT AUTHORITY SHALL BE RESPONSIBLE TO REMOVE ANY REQUIRED FFE PRIOR TO DEMOLITION.



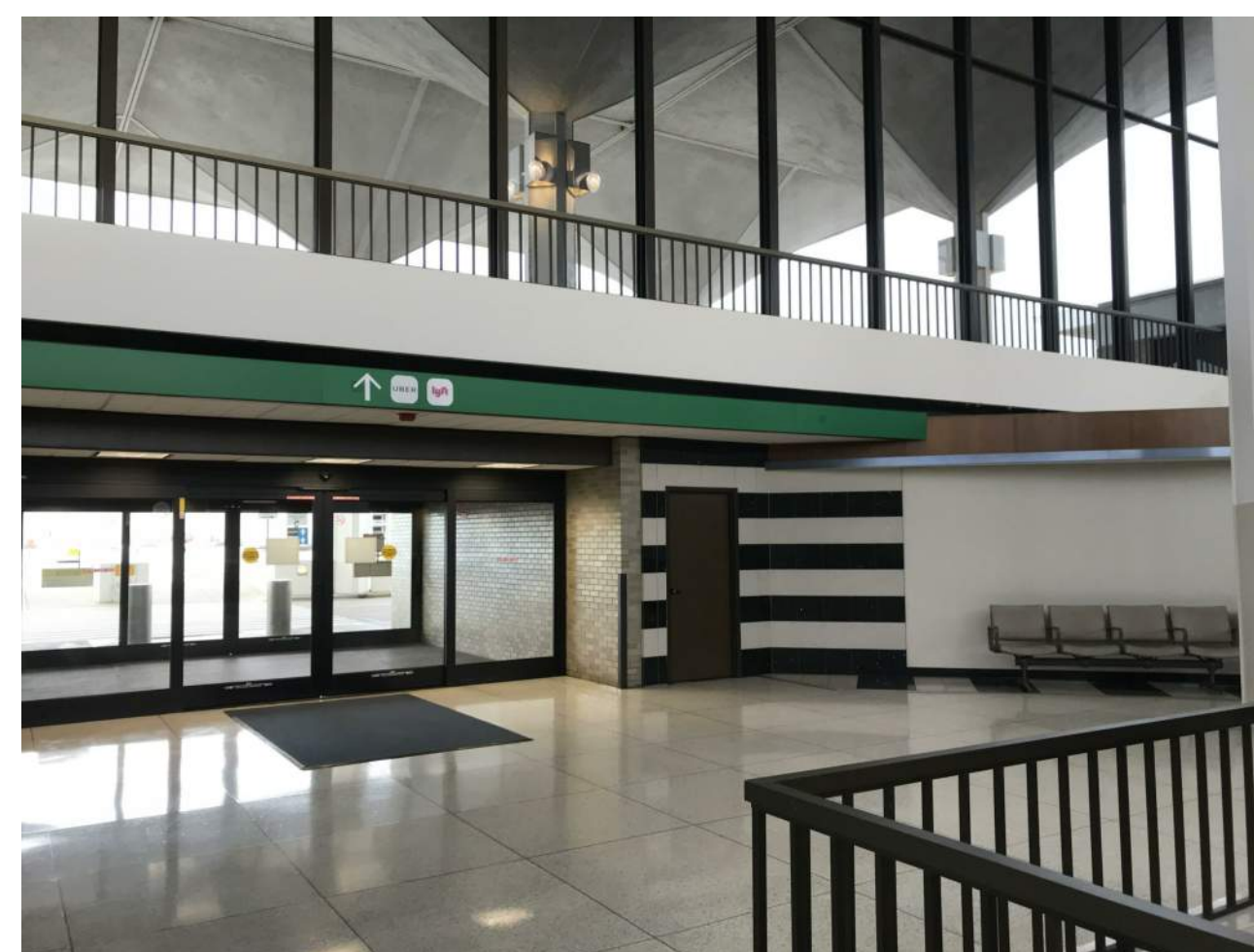
COORDINATE WITH AIRPORT AUTHORITY REGARDING MOVING OF EXISTING FURNITURE. THE AIRPORT AUTHORITY SHALL BE RESPONSIBLE TO REMOVE ANY REQUIRED FFE PRIOR TO DEMOLITION.



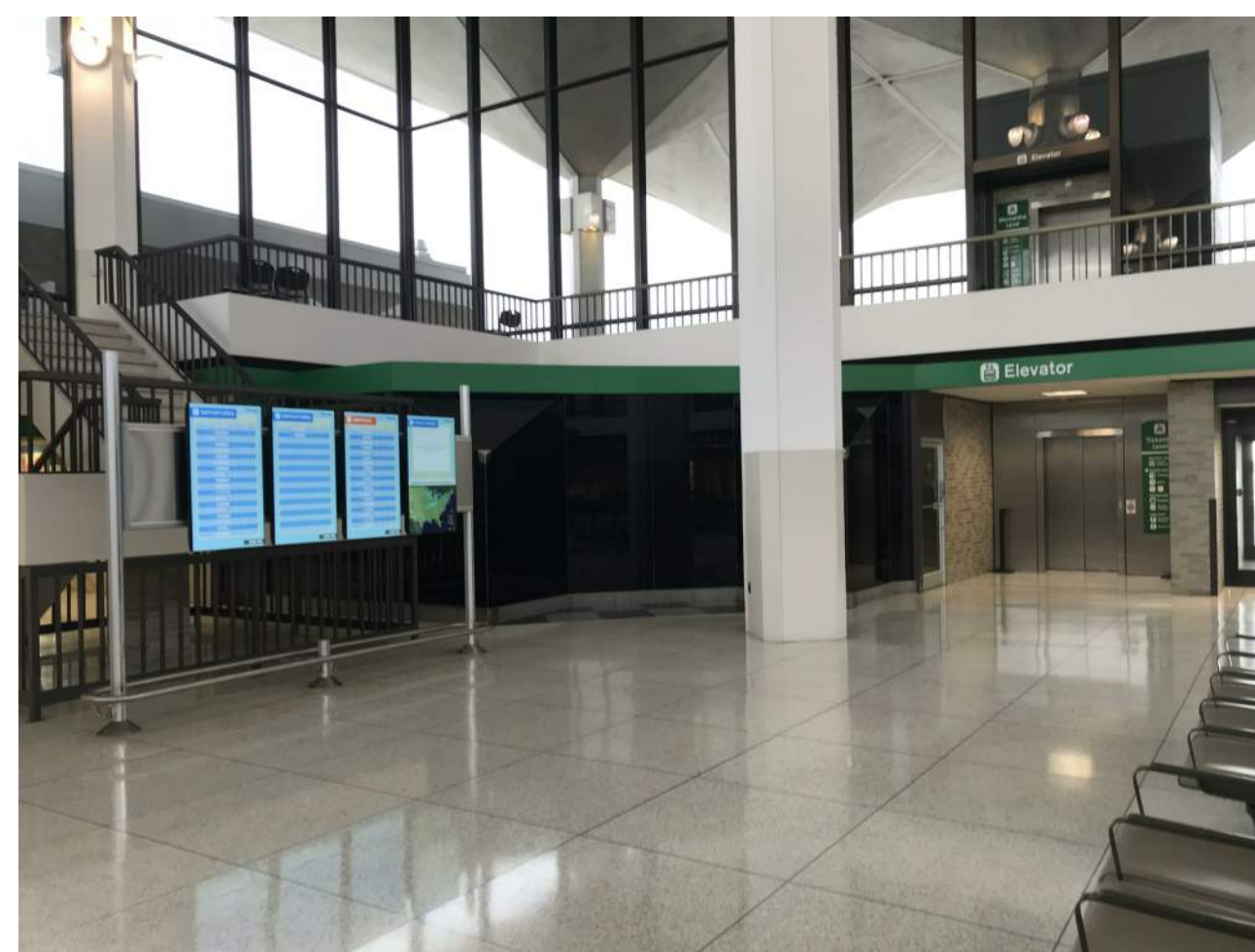
SPOT REPAIR EXISTING BRICK VENEER FINISH AFTER REMOVAL OF INTERIOR FINISHES. THE AIRPORT AUTHORITY SHALL PROVIDE THE BRICK MASONRY UNITS NEEDED FOR THE REPAIR.



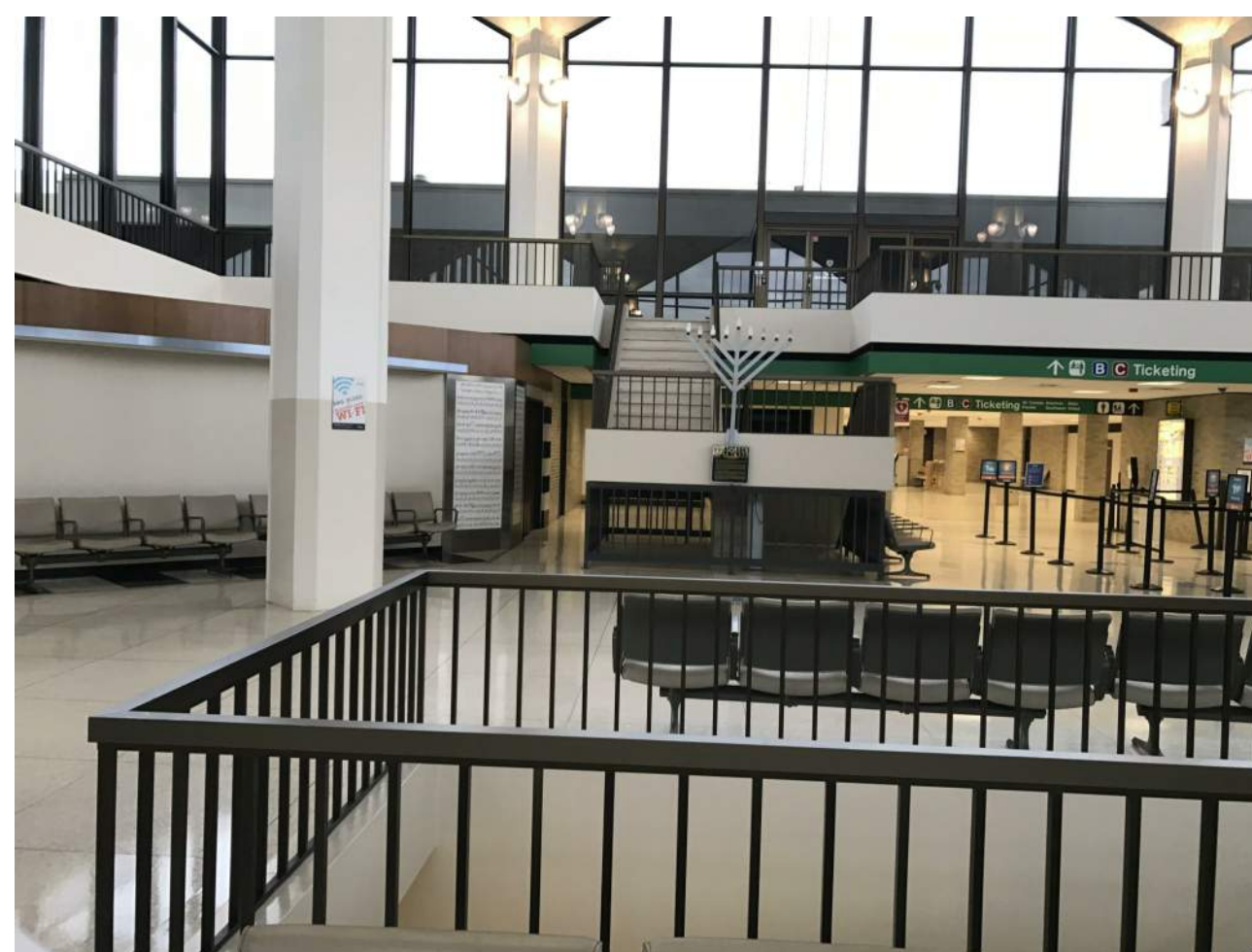
REMOVE SECONDARY CEILING FRAMING AT THE VACANT SPACE. INSTALL NEW COLOR CODED BANDING AT THE BOTTOM OF THE MEZZANINE TO MATCH EXISTING ADJACENT.



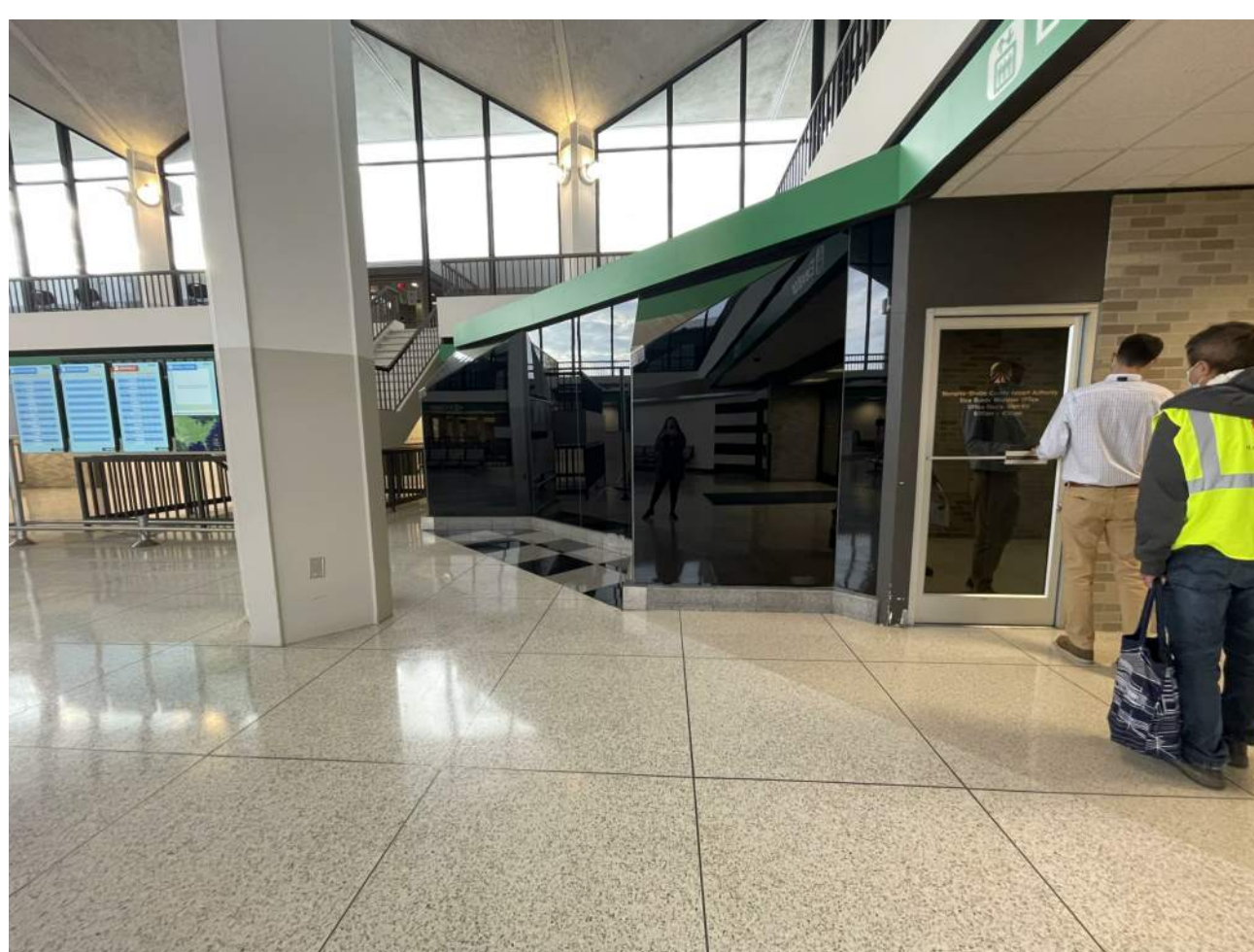
INSTALL WING WALL STAINLESS STEEL END CAP TO MATCH OPPOSITE WALL. PROTECT AREA OUTSIDE PROJECT LIMIT FROM DAMAGE DUE TO CONSTRUCTION ACTIVITIES. LIFE SAFETY AND EGRESS SHALL BE MAINTAINED AND SHALL NOT BE COMPROMISED AT ALL TIME.



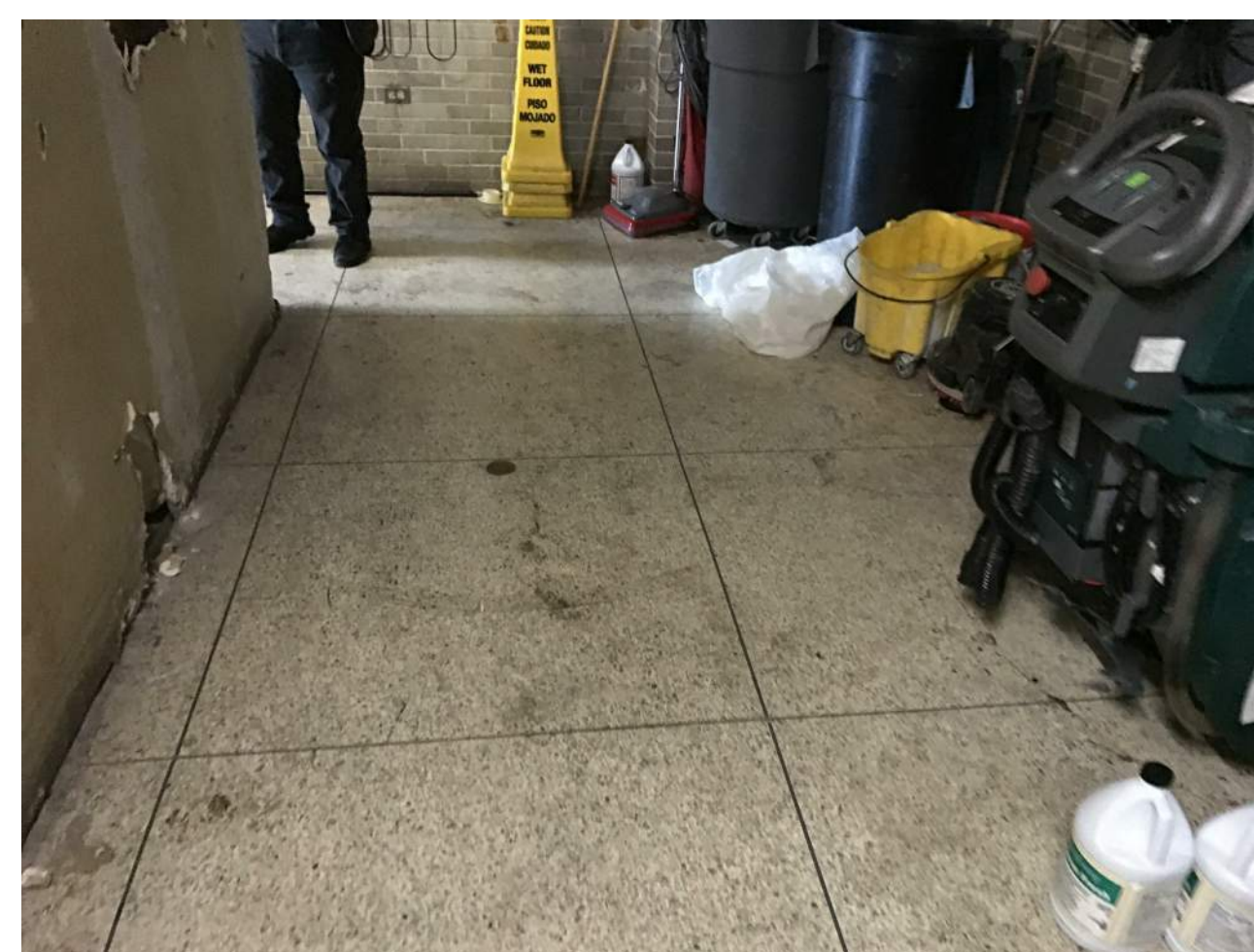
REMOVE SECONDARY CEILING FRAMING AT THE VACANT SPACE. INSTALL NEW COLOR CODED BANDING AT THE BOTTOM OF THE MEZZANINE TO MATCH EXISTING ADJACENT.



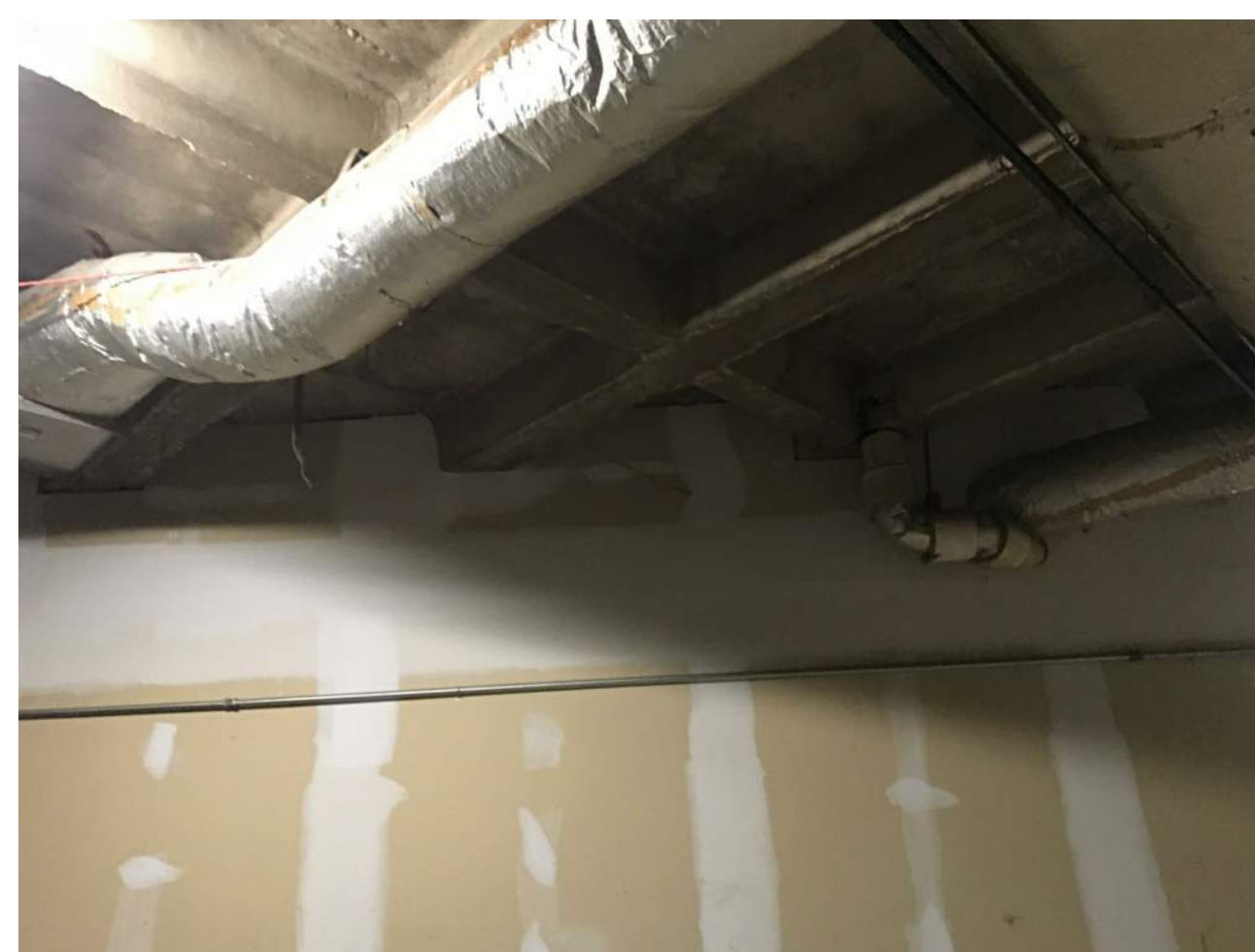
REMOVE SECONDARY CEILING FRAMING STRUCTURE AT THE VACANT SPACE. INSTALL NEW COLOR CODED BANDING AT THE BOTTOM OF THE MEZZANINE TO MATCH EXISTING ADJACENT.



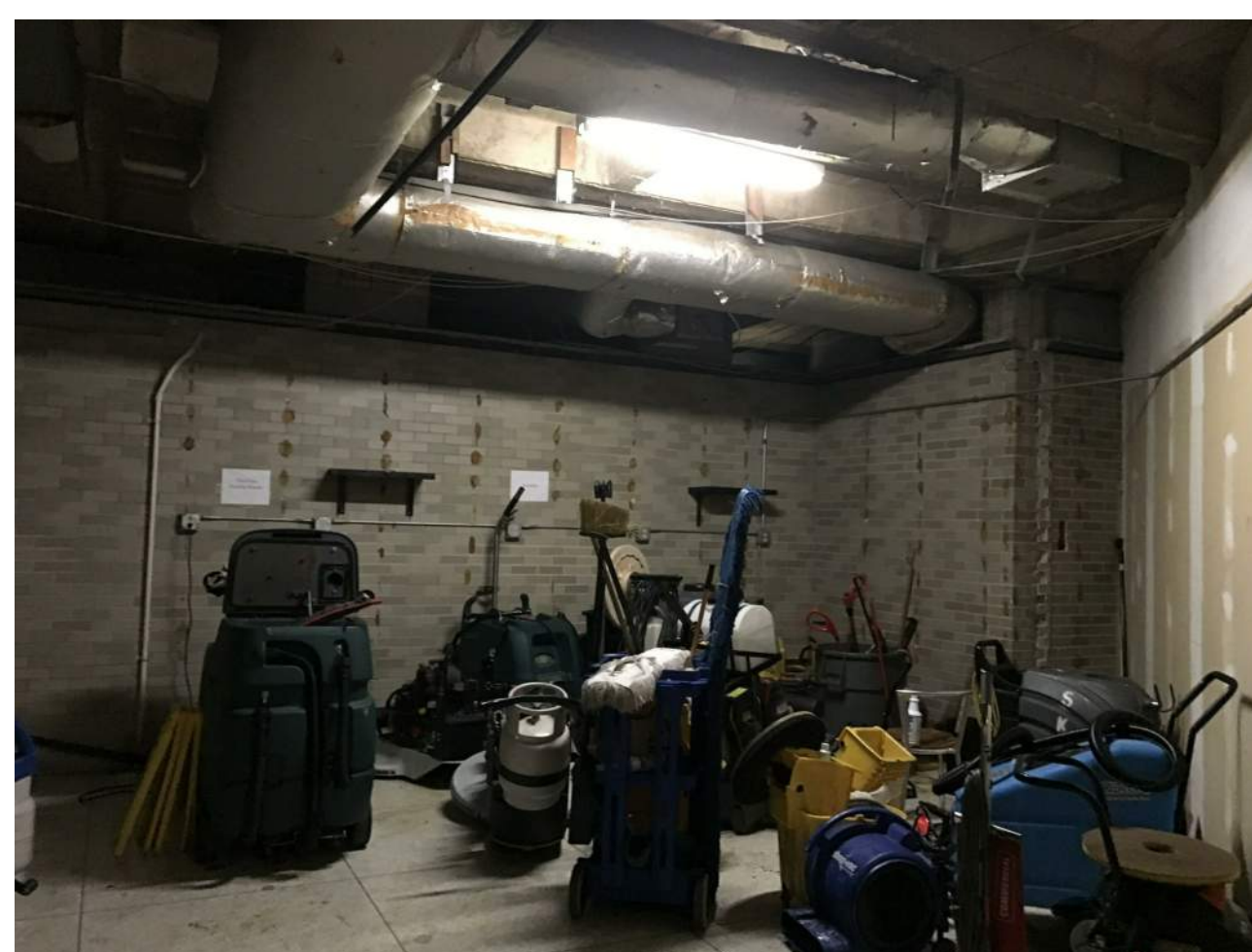
TIE IN NEW CEILING GRID SYSTEM IN THE NEWLY RESTORED SPACE TO MATCH EXISTING 2X4 GRID SYSTEM. TILE TYPE AND ORIENTATION. THE NEW LIGHT FIXTURE SHALL MATCH THE EXISTING 2X2 FIXTURE.



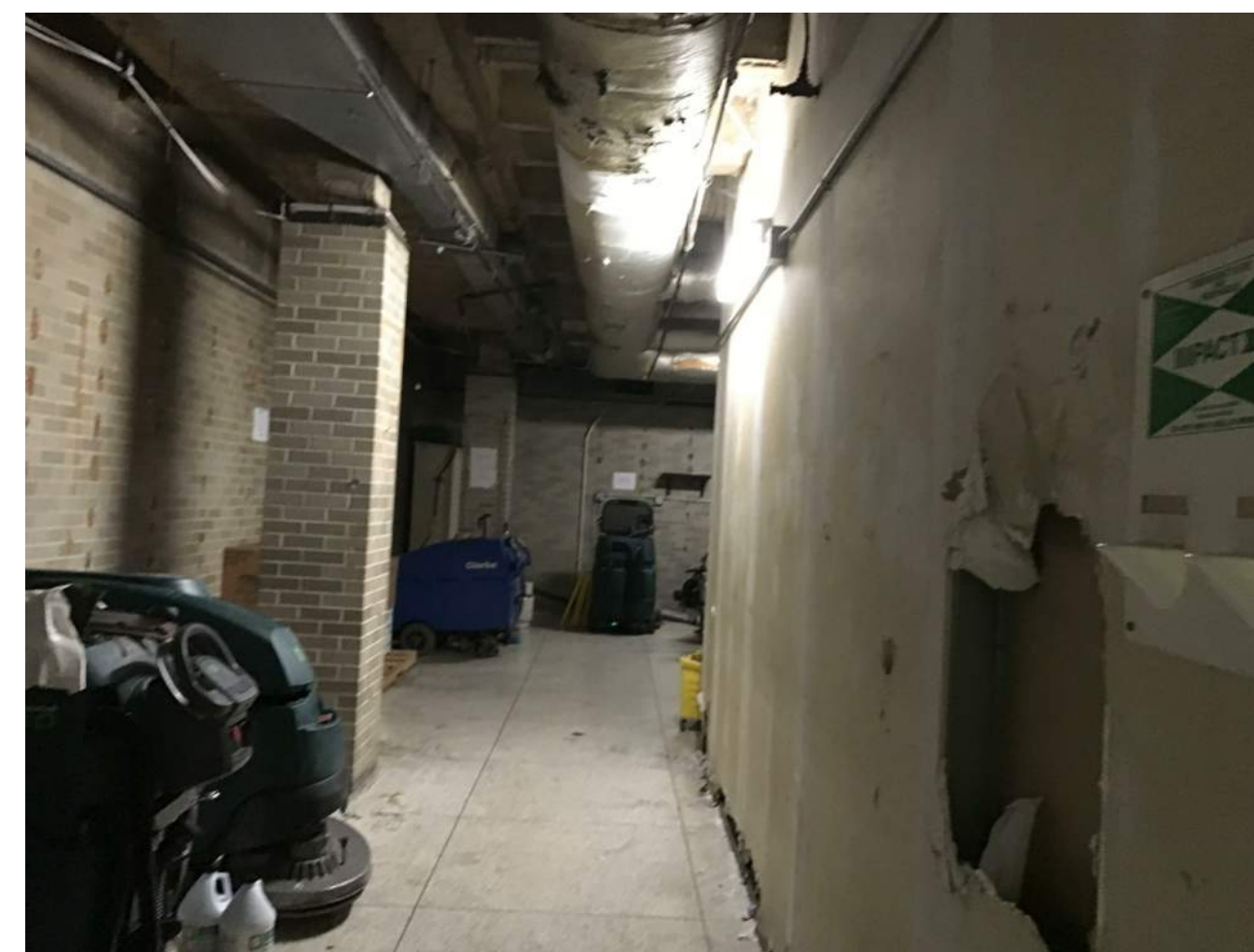
CLEAN AND RESTORE THE ORIGINAL TERRAZZO FLOOR SYSTEM. SPOT REPAIR AS NEEDED.



REROUTE THE EXISTING DUCT WORK AS NEEDED IN THE STORAGE ROOM BEHIND TO PROVIDE VERTICAL CLEARANCE FOR THE APPROXIMATE 9'-0" LAY IN CEILING HEIGHT TO MATCH ALL EXISTING ADJACENT CEILING.

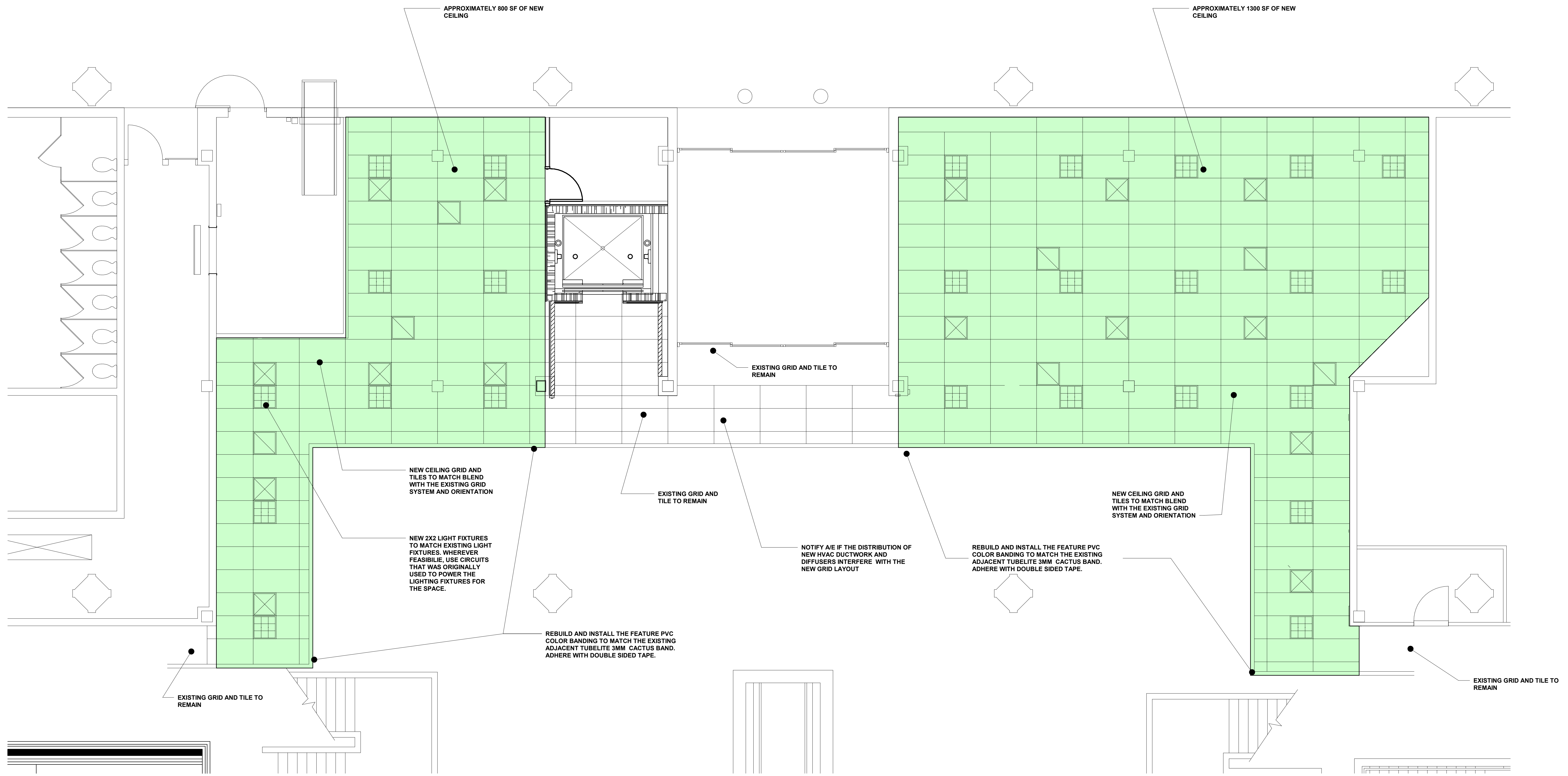


SPOT REPAIR EXISTING BRICK VENEER FINISH AFTER REMOVAL OF INTERIOR FINISHES. THE AIRPORT AUTHORITY SHALL PROVIDE THE BRICK MASONRY UNITS NEEDED FOR THE REPAIR.



SPOT REPAIR EXISTING BRICK VENEER FINISH AFTER REMOVAL OF INTERIOR FINISHES. THE AIRPORT AUTHORITY SHALL PROVIDE THE BRICK MASONRY UNITS NEEDED FOR THE REPAIR.

Table with 3 columns: NO., DESCRIPTION, DATE. It is currently empty.



1 **TERMINAL A - RETAIL SPACES - RCP**
 1/4" = 1'-0"

NOTE:

HVAC SUPPLY & RETURN GRILL AND LIGHT FIXTURE LOCATIONS AND QUANTITY SHOWN ARE APPROXIMATE ONLY AND MAY NEED TO BE FIELD ADJUSTED BASED ON THE EXSITING RESTRICTIONS ABOVE CEILING

CONTRACTOR TO FIELD VERIFY THE EXISTING CONDITIONS. NOTIFY A/E IF ADJUSTMENT IS NEEDED TO ACHIEVE SATISFACTORY INSTALLATION. IT IS THE INTENT OF THE AIRPORT TO PRESERVE AND REUSE AS MUCH OF THE EXSITING CIRCUIT AND DUCTWORK WHEREVER POSSIBLE.

1/7/2022 11:38:44 AM

ARCHITECT JOB NO:
 20008-T26

DRAWN BY: TD
 CHECKED BY: JG
 APPROVED BY: MT

CONSULTANT JOB NO:

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REVISIONS		
NO.	DESCRIPTION	DATE

MSCAA NO:
 19-1423-00 (19-1423-26)

PROJECT:
TERMINAL C AND A TICKETING LEVEL RETAIL SPACES DEMOLITION AND REPAIR

SET:
 -

SHEET TITLE:
TERMINAL "A" CEILING PLAN

REV. FILE NAME:
 -
 DATE: 01.07.2022 DWG NO: A2.6
 SCALE: 1/4" = 1'-0"