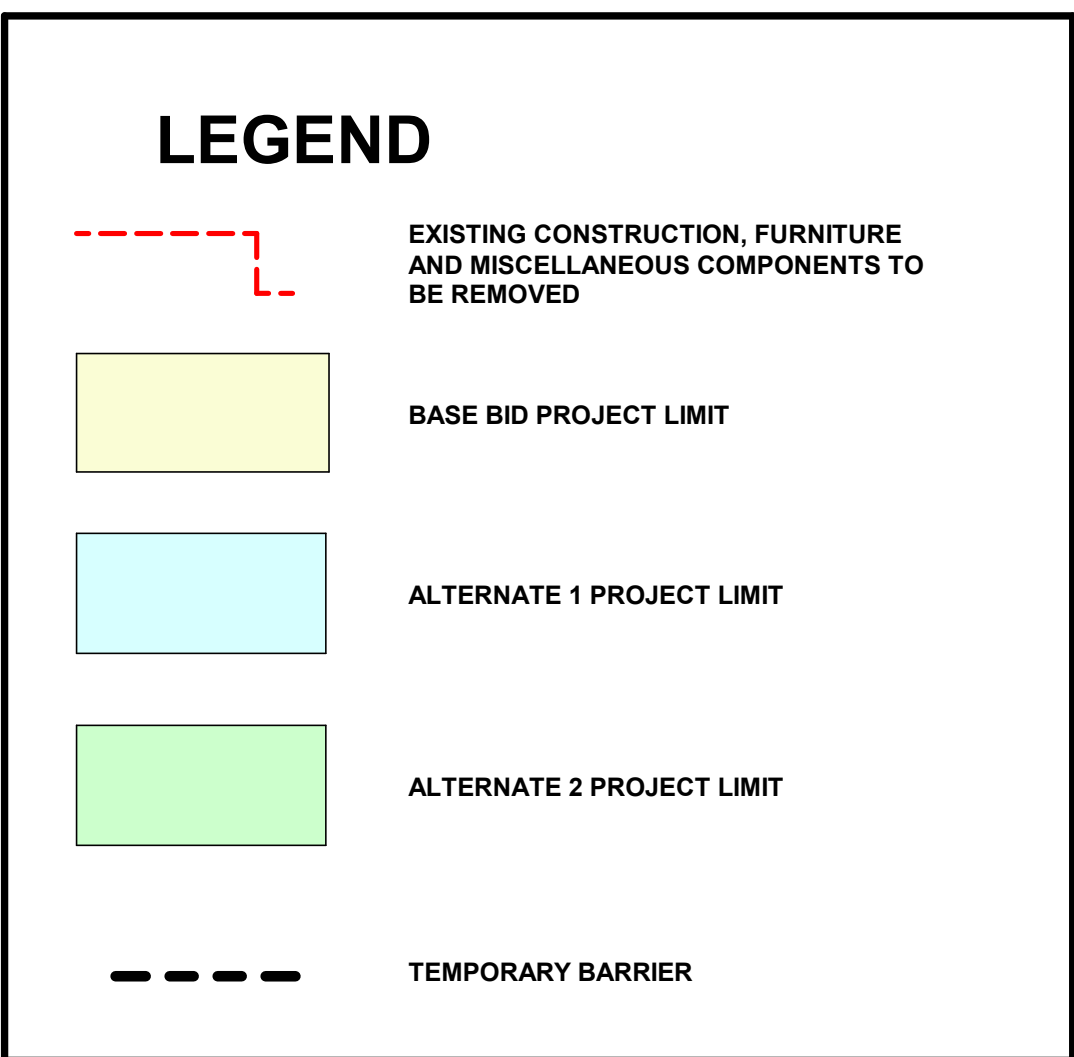


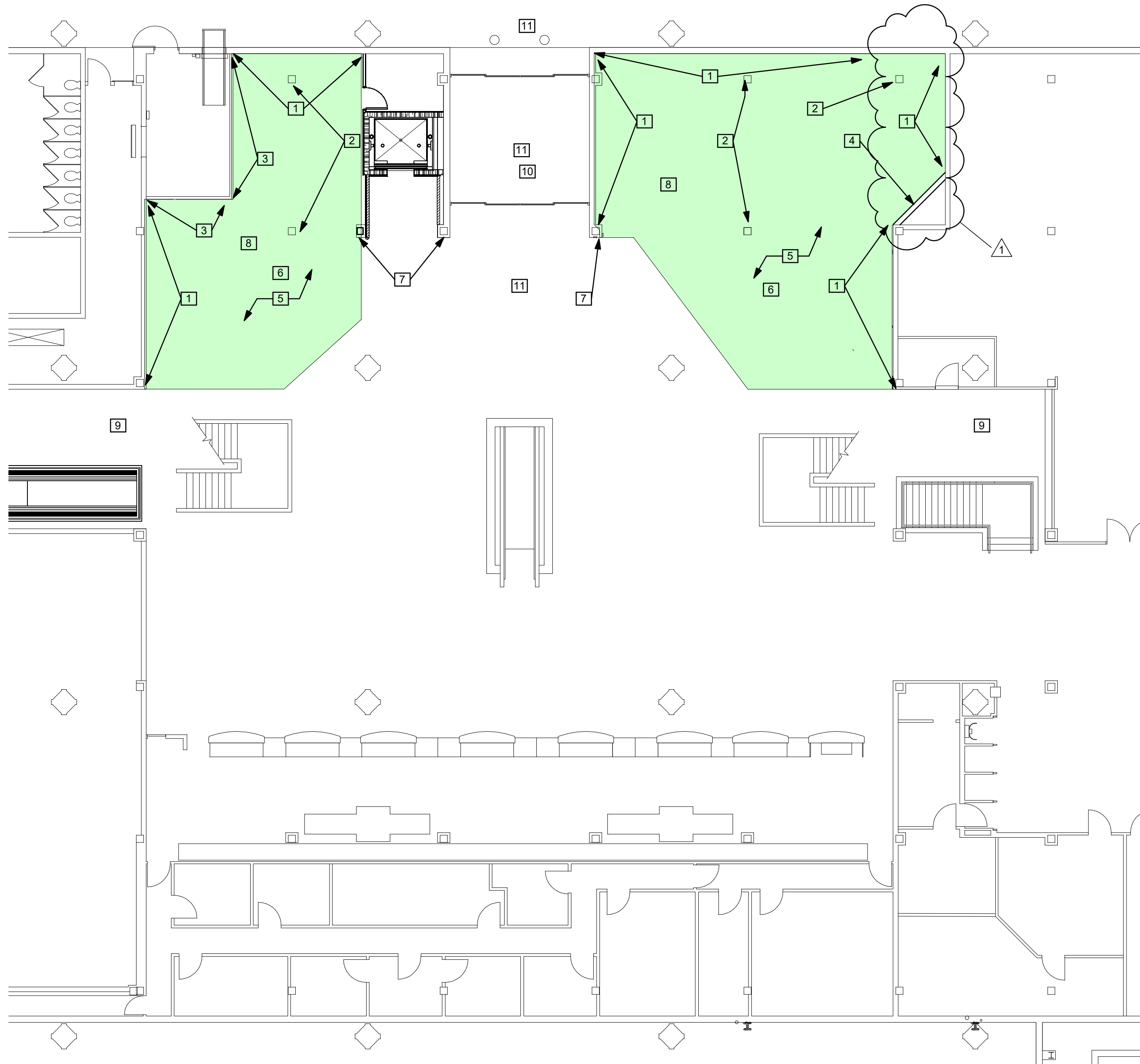
DEMOLITION NOTES:

- A. CONTRACTOR SHALL ERECT TEMPORARY BARRIER TO SEPARATE THE CONSTRUCTION TRAFFIC AND PASSENGER TRAFFIC AS SHOWN IN HEAVY DASHED LINES. ALL TEMPORARY WALLS SHALL BE CONSTRUCTED OF METAL STUDS; ANCHORED ON BOTTOM WITH DOUBLE STICK TAPE AND BRACED TO SOUND STRUCTURAL SUPPORT NEARBY ON TOP (ATTACHMENT TO CEILING TILES OR GRID IS PROHIBITED) THE OUTSIDE (PUBLIC SIDE) OF THE WALL SHALL BE FINISHED FLOOR TO CEILING (OR A MINIMUM OF 10' A.F.F. WHERE MARTINI GLASS HIGH CEILING IS DIRECTLY ABOVE) WITH 5/8" DRYWALL WITH 2 COATS OF FLAT OFF WHITE LATEX PAINT. THE BOTTOM OF THE WALL SHALL BE TRIMMED WITH 1/4" BLACK COVE BASE. A 2"x6" CHAIR RAIL BLOCKED 2" OFF THE WALL. SHALL BE INSTALLED 36" TO CENTER ABOVE FINISHED FLOOR. THE CHAIR RAIL SHALL BE SANDED AND PAINTED WITH TWO COATS OF BRONZE PAINT (PPG PITT-TECH ACRYLIC HIGH GLOSS DTM INDUSTRIAL ENAMEL - BRONZE TONE). SEE ATTACHED PICTURE "A" FOR EXAMPLE. THE CONTRACTOR SHALL RETAIN RESPONSIBILITY DURING CONSTRUCTION TO MAINTAIN THE WALL FOR AESTHETIC AND SECURITY ISSUES. MSCAA DEVELOPMENT DIVISION SHALL APPROVE ITS LOCATION, ANY ATTACHMENTS TO TERRAZZO FLOORS AND ALL SIGNAGE AND/OR GRAPHICS. WALK-OFF MATS SHALL BE USED AT ALL ACCESS POINTS TO THE CONSTRUCTION AREA TO PREVENT TRACKING OF DUST AND DEBRIS AND IS RESPONSIBLE FOR CLEANUP IF ANY DIRT, DUST AND/OR DEBRIS GETS OUTSIDE THEIR CONSTRUCTION LIMITS.
 MAINTAIN VERTICAL AND HORIZONTAL DUST CONTROL BARRIERS AT ALL TIMES DURING CONSTRUCTION. VISQUEEN OR APPROVED HORIZONTAL BARRIER SHALL BE AT LEAST 10 MIL THICK FOR DURABILITY TO WITHSTAND THE WEAR AND TEAR FROM CONSTRUCTION ACTIVITY. PROVIDE SUFFICIENT TEMPORARY OVERHEAD FRAMING AS NEEDED FOR SPANNING THE BARRIER MATERIAL HORIZONTALLY AT AREA AS SHOWN ON PICTURE C ON DRAWING A1.1.
- B. CAREFULLY REMOVE ALL EXISTING FLOOR FINISHES WITHIN THE PROJECT LIMIT TO ORIGINAL TERRAZZO OR CONCRETE FLOOR. IT IS THE AIRPORT'S INTENT TO PRESERVE AND REUSE THE ORIGINAL TERRAZZO FLOOR WHEREVER FEASIBLE WITHIN THE PROJECT LIMIT.
- C. THE AIRPORT AUTHORITY WILL REMOVE ALL MOVABLE FURNITURE BEFORE COMMENCEMENT OF DEMOLITION WORK. ANY REMAINING ITEMS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR FOR REMOVAL OR DISPOSAL. RETURN THE LOCK ASSEMBLY TO THE LOCK SHOP.
- D. COORDINATE WITH THE AIRPORT AUTHORITY THE HAUL ROUTE OF DISPOSAL TRAFFIC. IT IS OF UTMOST IMPORTANCE TO ENSURE NO CONSTRUCTION DEBRIS SUCH AS PACKAGING MATERIAL OR SHARP OBJECT BE LEFT AT ANY PUBLIC AREA OR DRIVEWAY. IF HAULING OF CONSTRUCTION DEBRIS MUST COME THRU THE ENTRANCE VESTIBULE, CONTRACTOR MUST PROTECT THE WALK OFF MAT AT THE ROUTE WITH PLYWOOD SO AS NOT TO DAMAGE THE EXISTING WALKOFF MAT.
- E. REMOVE AND DISPOSE ALL EXISTING CEILING GRIDS, GYPSUM BOARD FURR DOWN STRUCTURE CEILING TILES, AND LIGHT FIXTURES WITHIN THE PROJECT LIMIT. CONTRACTOR TO PROTECT THE ELECTRICAL CIRCUITS PRIOR TO DEMOLITION AND COMMENCEMENT OF NEW WORK. DO NOT CUT ANY WIRE UNLESS APPROVED BY THE AIRPORT AUTHORITY. CONTRACTOR SHALL ALSO PRESERVE THE EXISTING HVAC DUCTWORK FOR CAPPING OR REROUTING. DO NOT REMOVE ANY DUCT SYSTEM UNLESS APPROVED BY AIRPORT. BLOCK RETURN AIR TEMPORARILY TO PREVENT CONSTRUCTION DUST AND DEBRIS FROM TRAVELING TO OTHER AIRPORT AREA.
- F. CAREFULLY REMOVE THE EXISTING ADD-ON SECONDARY CEILING FRAMING UNDER AND OUTSIDE THE BALCONY. PROTECT THE ADJACENT CONSTRUCTION WHEREVER POSSIBLE
- G. CAREFULLY REMOVE ALL EXISTING FEATURE GLASS WALL AND CURB.
- H. REMOVE ANY VERTICAL WALL PARTITION AND METAL FRAMING WITHIN THE PROJECT LIMIT.
- I. NOT USED
- J. EXERCISE CAUTION DURING REMOVAL OF VERTICAL PARTITION WHEREVER THERE IS AIRPORT ORIGINAL BRICK WALL BEHIND. IT IS THE OWNER'S INTENT TO PRESERVE THOSE FINISHES WHEREVER POSSIBLE.
- K. REMOVE AND DISPOSE ALL EXISTING RETAIL SHELVING, OWNER MILLWORK, TRANSACTION COUNTER ETC.
- L. PROTECT AND COVER EXISTING RETURN AIR OPENING WITHIN THE PROJECT LIMIT SO THE FINE DUST AND DEBRIS DO NOT TRAVEL THROUGH THE HVAC SYSTEM TO OTHER AREAS.
- M. DOCUMENT ANY EXISTING DAMAGE AT THE AREA WITHIN AND ADJACENT TO THE WORK AREA TO AVOID ANY DISPUTE OF DAMAGES
- N. CAP ANY EXISTING UTILITY AND DRAINLINE WHEREVER POSSIBLE FOR FUTURE REUSE. CAP THE UTILITY BELOW TICKETING LEVEL FLOOR DECK OR BEHIND WALL.
- O. NOT USED.
- P. PROVIDE PLUMBING CUT OFF VALVE BEFORE CUTTING OF ANY UTILITY. COORDINATE WITH AIRPORT MAINTENANCE IF TEMPORARY SHUT OFF OF UTILITIES IS NEEDED IN ORDER TO PERFORM THE WORK
- Q. REMOVE ALL EXISTING TOILET FIXTURES, TOILET PARTITIONS, AND TOILET ACCESSORIES.
- R. PROTECT THE HORIZONTAL VENTILATION GRILLE AT THE BOTTOM OF THE MEZZANINE AS SHOWN IN PICTURE C AND D ON DRAWING A1.1



1 TERMINAL A - RETAIL SPACES - DEMOLITION

3/32" = 1'-0"



NEW WORK AND REPAIR NOTES:

- 1. SPOT REPAIR EXISTING ORIGINAL BRICK VENEER WALL SURFACE. TOOTH IN MASONRY UNITS PROVIDED BY AIRPORT AUTHORITY. COORDINATE WITH AIRPORT FOR OBTAINING THE BRICK FOR REPAIR. CLEAN BRICK WALL FROM CORNER TO CORNER TO RESTORE THE CLEANLINESS OF THE SURFACE. MATCH MORTAR COLOR AT REPAIR.
- 2. EXISTING INTERIOR 2'X2' BRICK WRAPPED COLUMN. REPAIR AS NEEDED. CLEAN THE SURFACE THE SAME MANNER AS OTHER BRICK VENEER.
- 3. IF EXISTING WALL BOARD CANNOT BE RESTORED FOR PAINTING SATISFACTORILY, INSTALL ONE LAYER OF NEW 5/8" GYPSUM WALL BOARD. MUD, SAND AND APPLY 3 COATS OF PAINT COATING. COLOR TO BE SELECTED BY AIRPORT. INSTALL WALL BASE AND BUMPER RAIL. WALL BASE TO BE MATCH EXISTING. BUMPER RAIL TO MATCH EXISTING.
- 4. NEW WALL PARTITION WITH 3 5/8" 22GA METAL STUD FRAMING AT 16" O.C. INSTALL 1 LAYER OF 5/8" GYPSUM WALL BOARD ON BOTH SIDES. FILL WALL CAVITY WITH BATT INSULATION. INSTALL WALL BASE AND BUMPER RAIL. WALL BASE TO BE MATCH EXISTING. BUMPER RAIL TO MATCH EXISTING.
- 5. CLEAN, SAND, AND POLISH AND RESTORE THE UNCOVERED ORIGINAL TERRAZZO FLOOR. SPOT REPAIR AS NEEDED WHERE DAMAGE WAS CAUSED BY PREVIOUS CONSTRUCTION OR UTILITY PENETRATIONS. SPOT REPAIR TO MATCH EXISTING ADJACENT FINISH WHEREVER FEASIBLE.
- 6. INSTALL THE FEATURE PVC COLOR BANDING TO MATCH THE EXISTING ADJACENT TUBELITE 3MM CRANBERRY OR CACTUS GREEN BAND. ADHERE WITH DOUBLE SIDED TAPE.
- 7. STAINLESS STEEL WALL END CAP TO MATCH THE APPEARANCE OF THE END CAP AT OPPOSITE WALL END.
- 8. INSTALL NEW 15/16" 2'X4' CEILING GRID SYSTEM WITH NEW CEILING TILES TO MATCH LAYOUT AND ORIENTATION OF ADJACENT LAY-IN CEILING SYSTEM. BLEND TILES AS NEEDED. THE CEILING TILES TO BE ARMSTRONG 2'X4' CORTEGA SECOND LOOK I NO. 2765. TIE IN TO EXISTING SYSTEM WHEREVER FEASIBLE. UNLESS NOTED OTHERWISE, CEILING HEIGHT TO BE MINIMUM 9' AFF. MATCH ADJACENT.
- 9. PROTECT EXISTING NEARBY LIGHTING FIXTURES AND LIFE SAFETY SYSTEM IMMEDIATELY OUTSIDE PROJECT LIMITS. THESE ITEMS TO REMAIN AS IS. ANY SENSOR, DETECTOR, ALARM, AND ANY CEILING OR WALL MOUNTED DEVICES AT THE WORK AREA MUST BE PROPERLY PROTECTED FROM CONSTRUCTION DAMAGE. FIRE AND LIFE SAFETY MUST NOT BE COMPROMISED AT ALL TIMES.
- 10. PROTECT AND MAINTAIN ALL EXISTING EGRESS ACCESS UNLESS NOTED OTHERWISE
- 11. AREAS IMMEDIATELY OUTSIDE PROJECT LIMIT MUST BE PROPERLY PROTECTED FROM CONSTRUCTION TRAFFIC UNTIL THE COMPLETION OF CONSTRUCTION.

2 TERMINAL A - RETAIL SPACES - NEW WORK & REPAIR

3/32" = 1'-0"

NOTE:
ALL WORK ON THIS TERMINAL A AREA SHALL BE CONSIDERED AS ALTERNATE 2.

REVISIONS		
NO.	MISC. CLARIFICATION DESCRIPTION	DATE
1	MISC. CLARIFICATION	01.25.22