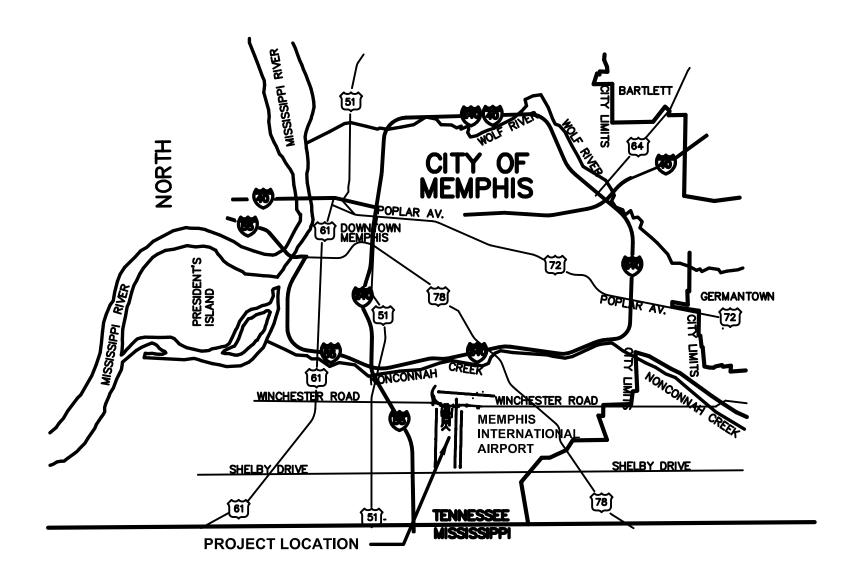
# **MEMPHIS INTERNATIONAL AIRPORT** MEMPHIS, TENNESSEE

## **MEMPHIS-SHELBY COUNTY AIRPORT AUTHORITY**

# **TERMINAL APRON RECONSTRUCTION (INCLUDING CONCOURSE C SOUTH DEMO) - CONSTRUCTION**



**OWNER / SPONSOR** 

**GENERAL DESCRIPTION OF PROJECT:** 

## **MSCAA PROJECT NUMBER 13-1368-02 ENGINEER'S FILE NUMBER 13030 ISSUED FOR BID/CONSTRUCTION** MAY 31, 2022

VICINITY MAP NOT TO SCALE

## **CONSULTANTS**

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## **ABBREVIATIONS**

FHS FIRE HOSE STATION FLEX FLEXIBLE	FF FINISHED FLOOR REQD FOB FACE OF BRICK RCP	FOC FACE OF CONCRETE REF FOF FACE OF FINISH REFR	AL AL ALT ALUM ANOD APPROX ARCH A.D. AUTO B.E.J. BSMT BRG BRG PL BM BD BOC BLDG B.U.R CAB CPT CPB CSMT CTR CLG CLG HT CT CVT CLO COL CONC COL CONC CONC CONC CONC CO	ACOUSTICAL ACOUSTICAL CEILING TILE ADJUSTABLE ACCESS LADDER ALTERNATE ALUMINUM ANODIZED APPROXIMATE ARCHITECT(URAL) AREA DRAIN AUTOMATIC BRICK EXPANSION JOINT BASEMENT BEARING BEARING PLATE BENCH MARK BOARD BOTTOM OR CURB BUILDING BUILT UP ROOF CABINET CARPET BASE CASEMENT CENTERLINE CEILING HEIGHT CERAMIC TILE CERAMIC TILE CERAMIC WALL TILE CLOSET COLUMN CONCRETE MASONRY UNIT CONTRACTOR FURN/INSTALL CONTROL JOINT CUBIC FOOT CUBIC YARD DETAIL DIAMETER DIMENSION DISPENSER DOUBLE DOUBLE HUNG DOWN SPOUT DRAWING DRINKING FOUNTAIN EACH ELEC DRINK COOLER ELEC TRIC ELEC PANELBOARD EMERGENCY ELEVATION ELEVATOR	LI WI LVR MH MFR MO MATL MAX MECH MEMB M MM MIN MIN MIN MTL MFTR MIR MISC MTD MOD MOV MULL NAT
	FEC FIRE EXITING. CAB	FFFINISHED FLOORREQDFOBFACE OF BRICKRCPFEFIRE EXTINGUISHERRESILFECFIRE EXITING. CAB	ELEV ENCL EQ EQUIP ESCAL EXIST EJ EXP EXT FOC FOF FOS FF FOB FE	EQUAL EQUIPMENT ESCALATOR EXISTING EXPANSION JOINT EXPOSED EXTERIOR FACE OF CONCRETE FACE OF FINISH FACE OF STUD FINISHED FLOOR FACE OF BRICK FIRE EXTINGUISHER FIRE EXITING. CAB	NOM PT PREF PL QT R REF REFR REINF REQD RCP
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ACT       ACOUSTICAL CEILING TILE       FTG         ADJ       ADJUSTABLE       FDTN         AL       ACCESS LADDER       FUR         ALT       ALTERNATE       FR         ALUM       ALUMINUM       GA         ANDD       ANODIZED       GALV         APPROX       APPROXIMATE       GC         ARCH       ARCHITECT(URAL)       GL         AD.       AREA DRAIN       GD         AUTO       AUTORATIC       GYP         BSMT       BASEMENT       HDBD         BGG       BEARING       HATE         BM       BENCH MARK       HVAC         BC       BDARD       HT         BCC       BOTTOM OR CURB       HB         BLDG       BUILDING       HORIZ         BLUR       BULT UP ROOF       HC         CAB       CABINET       INV         CLG       CEINTERLINE       INV         CLG       CEINTERLINE       INV         CLG       CLING       INSUL         CMT       CASEMENT       INT         CT       CERAMIC TILE       J-BOZ         CLG       CLING       INSUL         CLG	ACTACOUSTICAL CEILING TILEFTGADJADJUSTABLEFDTNALACCESS LADDERFURALTALTERNATEFRALUMALUMINUMGAANODANODIZEDGALVAPPROXAPPROXIMATEGCARCHARCHITECT(URAL)GLA.D.AREA DRAINGDAUTOAUTOMATICGYPB.E.J.BRICK EXPANSION JOINTGYPB.E.J.BRICK EXPANSION JOINTGYPB.E.J.BRICK EXPANSION JOINTGYPBRGBEARINGHDWRBRGBEARING PLATEHDRBMBENCH MARKHVACBDBOARDHTBOCBOTTOM OR CURBHBBLDGBUILDINGHORIZB.U.RBUILT UP ROOFHCCABCARPETINTCTCARPETINVCLGCEILINGHEIGHTJTCTCARPETINVCLGCEILINGHEIGHTJTCTCERAMIC TILEJ-BOZCLOCOUWNKPLCONTCONTROL JOINTLTCUTCORRIDORLHCFCICONTRACTOR FURN/INSTALLLTCJCONTROL JOINTLTCUTCUBIC YARDMHDILDETAILMFRDIADIAMETERMODIMDUBLEMECHDHDOUBLEMECHDIADIAMETERMODIMDUMETERMO<	ACTACOUSTICAL CEILING TILEFTGADJADJUSTABLEFDTNALACCESS LADDERFURALTALTERNATEFRALUMALUMINUMGAANODANODIZEDGALVAPPROXAPPROXIMATEGCARCHARCHITECT(URAL)GLA.D.AREA DRAINGDAUTOAUTOMATICGYP EBSMTBASEMENTHDBDBRGBEARINGHDWRBRGBEARING PLATEHDRBMBENCH MARKHVACBDBOARDHTBOCBOTTOM OR CURBHBBLDGBUILDINGHORIZBURGBUILT UP ROOFHCCABCABINETINNCCGCENTERLINEINVCLGCENTERLINEINVCLGCENTERLINEINVCLGCONCCONCRETEJSTCWTCERAMIC TILEJSTCWTCERAMIC WALL TILEJ-BOZCLOCONTINUOSLBCORRCONTRACTOR FURN/INSTALLITCJCONTRACTOR FURN/INSTALLITCJCONTRACTOR FURN/INSTALLITCJCONTRACTOR FURN/INSTALLITCJCONTRACTOR FURN/INSTALLITCJCONTRACTOR FURN/INSTALLITCTCERAMIC GOTTLVRCU PDCUBIC YARDMHDLDETAILMFRDIADIAMETERMODDIMDIMENSIONMATLDSD	ACT		

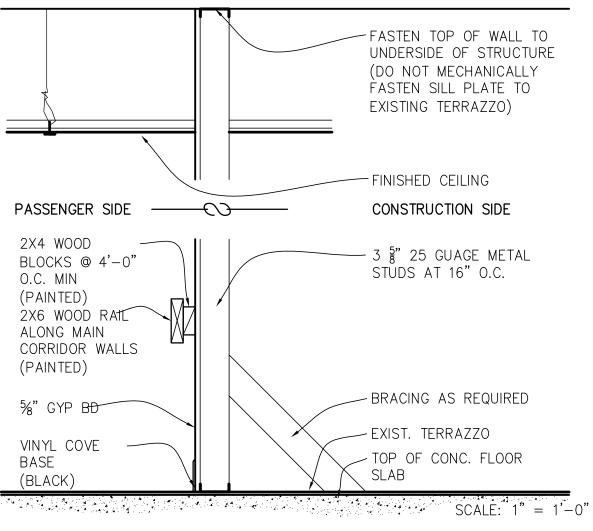
FLOOR FLOOR DRAIN FLOOR CLEANOUT FOOTING FOUNDATION FURRING FRAME GAUGE GALVANIZED GEN CONTRACTOR GLASS GRADE GYPSUM GYPSUM BOARD HARDBOARD HARDWARE HEADER HEATING/VENT/AC HEIGHT HOSE BIB HORIZONTAL HOLLOW CORE HOLLOW METAL INCLUDE(D) INSIDE DIAMETER INTERIOR INVERT INSULATION JOINT JOIST JUNCTION BOX KITCHEN KICKPLATE LAMINATE LAVATORY LAG BOLT LEFT HAND LIGHT LIGHT WEIGHT LOUVER MANHOLE MANUFACTURER MASONRY OPENING MATERIAL MAXIMUM MECHANICAL MEMBRANE METER MILLIMETER MINIMUM METAL MANUFACTURER MIRROR MISCELLANEOUS MOUNTED MODULAR MOVEABLE MULLION NATURAL NOISE REDUCTION NOMINAL NOT IN CONTRACT PLATE PREFINISHED PROPERTY LINE QUARRY TILE RADIUS REFERENCE REFRIGERATOR REINFORCED REQUIRED RCP REFLECTED CLG PLAN RESILIENT

REV RIGHT HAND RH ROW RIGHT OF WAY ROOF DRAIN RD RFG ROOFING RM ROOM ROUGH OPENING RO RUBBER BASE RB SECT SECTION SCHED SCHEDULE SHEET SHT SHT GL SHEET GLASS SHWR SIM SHOWER SIMILAR SS SOLID SURFACE SST STAINLESS STEEL SPKR SPEAKER SPEC SPECIFICATION SQ SQUARE STD ST SD STANDARD STEEL STORM DRAIN STOR STORAGE STRUCT STRUCTURE SUSP SUSPENDED SYM SYMMETRICAL SYNTH SYNTHETIC STC SOUND TRANSMISSION CLASS TEL TELEPHONE ΤV **TELEVISION** TEMP TEMPERATURE TEMPERED TMPD TFCI TENANT FURNISH CONT INSTALL TETI TENANT FURNISH TENANT INSTALL THK THICK THRES THRESHOLD TYP TYPICAL TOL TOLERANCE T&G TONGUE & GROOVE TOS TOP OF SLAB TOST TOP OF STEEL TOW TOP OF WALL TOC TOP OF CURB UNFIN UNFINISHED UNO UNLESS NOTED OTHERWISE VJ VEE JOINT VAR VARNISH VNR VENEER VER VERIFY VEST VESTIBULE VERT VERTICAL VCT VINYL COMPOSITION TILE VINYL WALL COVERING VWC VF VINYL FLOORING VB VINYL BASE WSCT WAINSCOT WT WALL TILE WC WATER CLOSET WWF WELDED WIRE FABRIC WDW WINDOW W∕O WH WITHOUT WATER HEATER WIRED GLASS WGL WITH W/ WOOD WD WOOD BASE WB WEIGHT WT WORK POINT WPT

REVISION

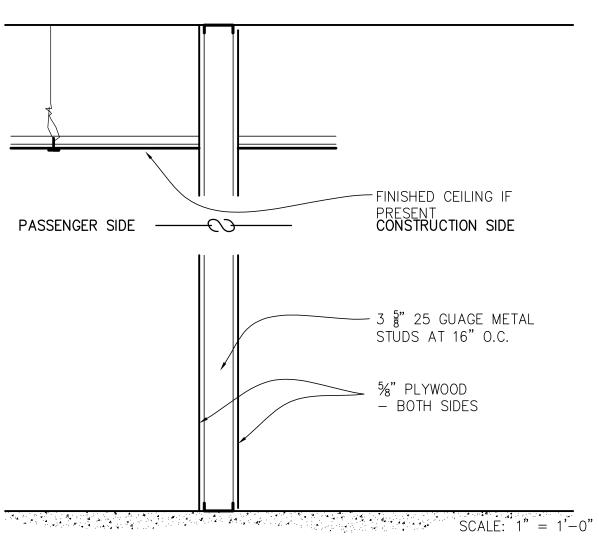
BUILDIN MECHA PLUMB ELECT GAS ENERG ACCES LIFE S/

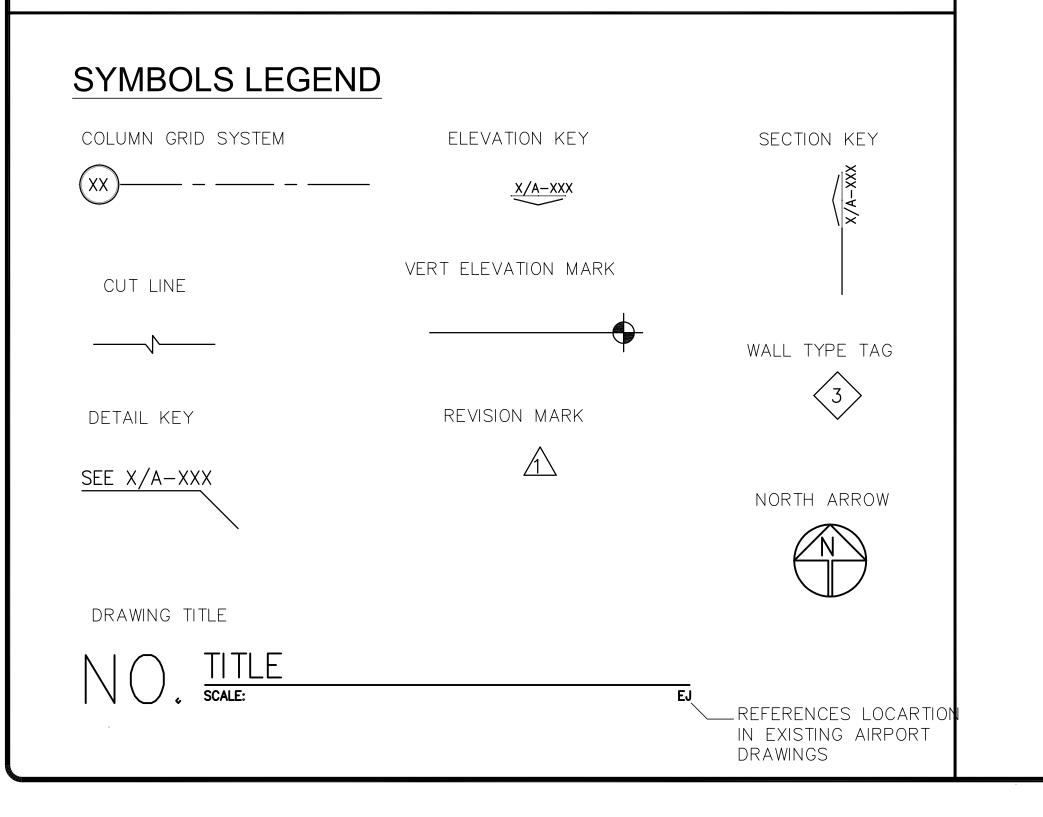
## PROJECT DESCRIPTION











## **PROJECT INFORMATION**

## CODE INFORMATION

ING	2021	INTERNATIONAL BUILDING CODE
ANICAL	2021	INTERNATIONAL MECHANICAL CODE
BING	2021	INTERNATIONAL PLUMBING CODE
FRICAL	2020	NATIONAL ELECTRICAL CODE
	2021	INTERNATIONAL GAS CODE
GY	2021	INTERNATIONAL ENERGY CONSERVATION CODE
SSIBILITY	2013	ANSI A117.1
SAFETY	2021	INTERNATIONAL FIRE CODE

THE PROJECT CONSISTS OF THE DEMOLITION OF APPROXIMATELY 59,565 SF OF EXISTING AIRPORT BUILDINGS WITHIN AN ACTIVE AIRPORT FACILITY. PARTIAL DEMOLITION OF EXISTING AIRFIELD PAVEMENT WILL BE REQUIRED WITH UTILITY REMOVAL. THE EXISTING BUILDING STRUCTURE IS CAST-IN-PLACE CONCRETE WITH NON-LOADBEARING MASONRY, METAL STUD AND GYPSUM BOARD INTERIOR AND EXTERIOR WALLS ON 1 SUBGRADE LEVEL AND 2 ABOVE GRADE LEVELS. PARTIAL SUBGRADE PUT-BACK WILL BE REQUIRED AFTER BUILDING DEMOLITION AND NEW CONSTRUCTION TO ENCLOSE THE PARTIALLY DEMOLISHED ENDS OF THE EXISTING BUILDING TO REMAIN.

> TEMPORARY CONSTRUCTION BARRICADE

## TEMPORARY CONSTRUCTION > BARRICADE

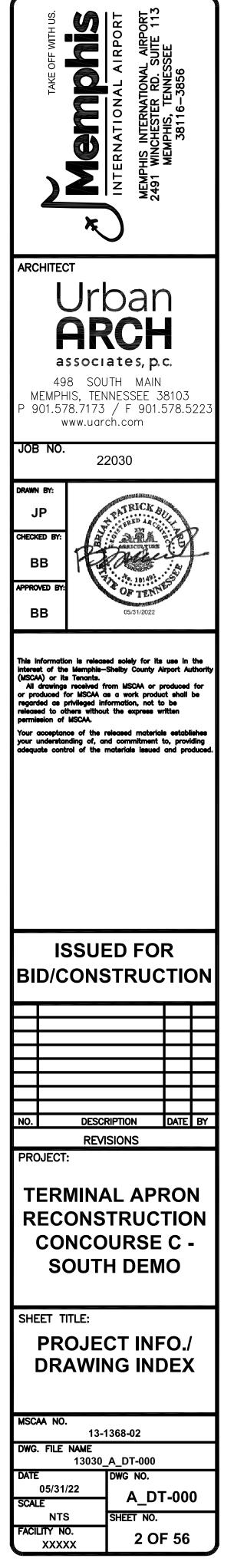
## TYPE

WALL	
1'' = 1' - 0''	

INDEX TO DRA	AWINGS
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<u> </u>		TO DRAVINGS
		– SHEET NUMBER – DISCIPLINE – CONCOURSE DESIGNATION – DRAWING NUMBER
x	x_x-xxx	SHEET TITLE
1	CS-000	COVER SHEET
		TURAL
2	A_DT-000	PROJECT INFO/ DRAWING INDEX
3	A_DT-001	GENERAL NOTES/ KEY PLANS
4	A_DT-002	KEY PLANS
5	A_DT-003	OVERALL SITE DEMOLITION PLAN
6	A_C-001	CONCOURSE C SITE DEMOLITION PLAN
7	A_C-100	CONCOURSE C TUNNEL PLAN
8	A_C-110	CONCOURSE C FIRST FLOOR PLAN
9	A_C-120	CONCOURSE C SECOND FLOOR PLAN
10	A_C-130	CONCOURSE C ENLARGED TUNNEL PLAN
11	A_C-131	CONCOURSE C ENLARGED FIRST FLOOR PLAN
12	A_C-132	CONCOURSE C ENLARGED SECOND FLOOR PLAN
13	A_C-200	CONCOURSE C EXTERIOR DEMO ELEVATIONS
14	A_C-201	CONCOURSE C NEW EXTERIOR ELEVATIONS
15	A_C-300	CONCOURSE C BUILDING SECTIONS
16	A_C-310	CONCOURSE C BUILDING SECTIONS
17	A_C-400	CONCOURSE C WALL SECTIONS
18	A_C-401	CONCOURSE C WALL SECTIONS
	<u>CIVIL</u>	
19	G-ID-01	INDEX OF DRAWINGS
20	G-QT-01	
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## <u>GENERAL NOTES</u>

1. CONTRACT DOCUMENTS INCLUDE ALL DRAWINGS AND THE PROJECT MANUAL.

2. CONTRACTOR SHALL VISIT SITE AND FAMILIARIZE HIMSELF WITH EXISTING CONDITIONS PRIOR TO BID AND CONSTRUCTION.

3. CONTRACTOR TO SUBMIT SITE ACCESS, MOBILIZATION, AND CONSTRUCTION UTILIZATION PLAN TO OWNER FOR APPROVAL PRIOR TO BEGINNING WORK.

4. CONTRACTOR RESPONSIBLE FOR OBTAINING ALL PERMITS.

5. CONTRACTOR TO COORDINATE WITH WORK OF SEPARATE CONTRACTORS, WORKING DIRECTLY FOR THE OWNER (IF ANY).

6. THIS DOCUMENT IS PROVIDED FOR BASIC CONSTRUCTION PURPOSES ONLY. THE ARCHITECT DOES NOT WARRANT ANY MATERIAL EQUIPMENT, HARDWARE, ETC. WHETHER IMPLIED OR EXPLICITLY CALLED OUT ON DRAWINGS.

7. ALL GENERAL NOTES APPLY TO THE SCOPE OF THIS TOTAL PROJECT, REGARDLESS OF WHETHER OR NOT THEY ARE KEYED ON EVERY SHEET TO A SPECIFIC DETAIL.

8. THE GENERAL CONTRACTOR SHALL ENSURE THAT ALL CONSTRUCTION MEETS OR EXCEEDS APPLICABLE CODES AND STANDARD PRACTICES, INCLUDING ALL FEDERAL, STATE AND LOCAL BUILDING AND HANDICAP REQUIREMENTS AND REGULATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY VIOLATION OF THE SAME AND SHALL MAKE ALL WORK ACCEPTABLE TO THE PUBLIC DEPARTMENT INVOLVED WITHOUT EXTRA CHARGE.

9. ALL PERMITS (OCCUPANCY, ELECTRICAL, PLUMBING AND ALL OTHERS) REQUIRED BY STATE AND LOCAL CODES, EXCEPT THOSE ACQUIRED BY SUBCONTRACTORS, ARE TO BE SECURED BY THE GENERAL CONTRACTOR WITH COPIES TO BUILDING OWNER WITHOUT EXTRA CHARGE. ALL PERMITS ACQUIRED BY SUBCONTRACTORS SHALL BE SUBMITTED TO THE GENERAL CONTRACTOR FOR RECORD.

10. ALL EXTERIOR EXPOSED WORK SHALL BE INSTALLED IN SUCH MANNER AS TO ASSURE WEATHER TIGHT CONDITION. CONTRACTOR SHALL PROVIDE ALL CAULKING AND WEATHER BARRIER MATERIALS REQUIRED FOR WEATHER TIGHT CONDITIONS DURING ALL CONSTRUCTION TIMES.

11. EACH TRADE SHALL VERIFY ALL REQUIREMENTS PERTAINING TO WORK PERFORMED IN THE PROJECT AND ANY REQUIRED PERMITS. ALL SUBCONTRACTORS SHALL DIRECT QUESTIONS, CHANGES OR REQUESTS THROUGH THE GENERAL CONTRACTOR. THE GENERAL CONTRACTOR SHALL SUBMIT ALL REQUESTS, CHANGES OR QUESTIONS TO THE OWNER IN WRITING.

12. FOR ANY ARCHITECTURAL WORK THAT IS INDICATED GRAPHICALLY AND NOT NOTED, THE WORK WILL BE EXPECTED TO BE PERFORMED AT NO ADDITIONAL CHARGE.

13. IF UNANTICIPATED MECHANICAL, PLUMBING, ELECTRICAL, STRUCTURAL ELEMENTS, OR ANY OTHER CONDITIONS ARE ENCOUNTERED WHICH MIGHT CONFLICT WITH THE INTENDED FUNCTION OR DESIGN OF THE NEW CONSTRUCTION, CONTACT THE ARCHITECT IMMEDIATELY FOR CLARIFICATION. NOTE: IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO ENSURE THAT ALL TRADES COORDINATE WITH ONE ANOTHER TO IDENTIFY POTENTIAL CONFLICTS BEFORE THE FIRST TRADE BEGINS WORK.

14. REVIEW ALL EXIT SIGNS, EMERGENCY EXITS AND FIRE EXTINGUISHER LOCATIONS WITH LOCAL CODE OFFICIALS PRIOR TO CONSTRUCTION TO VERIFY LIFE SAFETY AND FIRE DEPARTMENT ACCESS AT ALL TIMES DURING CONSTRUCTION. COORDINATE ANY VARIATIONS FROM CONSTRUCTION DOCUMENTS WITH ARCHITECT. EMERGENCY EXITS SHALL BE PROVIDED DURING BUILDING DEMOLITION AT ALL TIMES. IN CASE OF ANY EMERGENCY EVACUATIONS, THE EMERGENCY EXIT WILL SERVE AS A LIFELINE FOR TRANSPORTATION OF INJURED WORKERS.

15. THE CONTRACTOR SHALL PROMPTLY REMEDY ANY DAMAGE AND/OR LOSS TO PROPERTY (ALL MATERIALS AND EQUIPMENT INCORPORATED IN THE WORK DESCRIBED HEREIN) CAUSED IN WHOLE OR IN PART BY THE CONTRACTOR, A SUBCONTRACTOR OR ANYONE DIRECTLY OR INDIRECTLY EMPLOYED BY THE CONTRACTOR.

16. THE CIVIL, STRUCTURAL, MECHANICAL, PLUMBING, AND ELECTRICAL DRAWINGS ARE SUPPLEMENTARY TO THE ARCHITECTURAL DRAWINGS. SHOULD THERE BE A DISCREPANCY BETWEEN THE ARCHITECTURAL DRAWINGS AND THE ENGINEERING DRAWINGS, SUCH DISCREPANCY IS TO BE BROUGHT TO THE ATTENTION OF THE ARCHITECT PRIOR TO INSTALLATION OF SAID WORK. ANY WORK INSTALLED IN CONFLICT WITH ARCHITECTURAL DRAWINGS SHALL BE CORRECTED BY THE CONTRACTOR AT HIS OWN EXPENSE, AND AT NO EXPENSE TO THE OWNER OR THE ARCHITECT.

17. DO NOT SCALE DRAWINGS. ALL DIMENSIONS SHALL BE VERIFIED IN FIELD.

18. THE CONTRACTOR IS RESPONSIBLE FOR ANY DEMO'D OR REMOVED CONSTRUCTION MATERIAL RELOCATION AS DIRECTED BY THE OWNER. ANY DEMO'D OR REMOVED CONSTRUCTION MATERIAL, BUILDING COMPONENTS, FURNITURE, ETC. NOT PREVIOUSLY REMOVED FROM PROJECT LIMITS PRIOR TO CONSTRUCTION, INDICATED IN THE CONTRACT DOCUMENTS OR MARKED BY THE OWNER PRIOR TO CONSTRUCTION BECOMES THE PROPERTY OF THE CONTRACTOR. AT THAT POINT THE CONTRACTOR IS RESPONSIBLE FOR REMOVING SAID MATERIAL AWAY FROM THE PROJECT SITE.

19. COMPLY WITH ALL CITY OF MEMPHIS, SHELBY COUNTY, STATE OF TENNESSEE AND MSCAA REQUIREMENTS FOR WEIGHT LIMITS FOR ALL OPERATIONS ON SURROUNDING PUBLIC STREETS AND MSCAA MAINTAINED STREETS. ANY EXISTING AND/OR NEWLY CONSTRUCTED STREETS OR ROADS DAMAGED BY HAULING OPERATIONS SHALL BE REPAIRED AT NO EXPENSE TO THE OWNER. ALL PROJECT RELATED HAULING AT THE SITE SHALL BE APPROVED BY THE OWNER, ARCHITECT AND/OR ENGINEER.

20. AIRPORT OPERATIONS WILL HAVE PRIORITY AT ALL TIMES OVER VEHICLES, EQUIPMENT, AND PERSONNEL. THE CONTRACTOR SHALL EMPLOY STRICT MEASURES TO PREVENT ANY CONFLICT BETWEEN CONSTRUCTION FORCES AND AIRCRAFT OPERATIONS.

21. ALL EQUIPMENT SUCH AS CRANES, DRILL RIGS AND ANY OTHER CONSTRUCTION EQUIPMENT WITH A VERTICAL COMPONENT ARE SUBJECT TO HEIGHT RESTRICTIONS AS GOVERNED BY THE F.A.R. PART 77 SURFACES PROTECTING NAVIGABLE AIRSPACE.

22. PERFORM CONSTRUCTION OPERATIONS AS NECESSARY TO MINIMIZE ATTRACTION TO BIRDS CAUSED BY PONDING WATER, TRASH AND OTHER CONSTRUCTION FEATURES. PROTECT ANY PORTION OF BUILDING TO REMAIN FROM WEATHER DAMAGE OR WATER INFILTRATION AS A RESULT FROM ANY DEMO OPERATIONS.

23. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO MINIMIZE BLOWING DUST AND/OR DEBRIS. THE OWNER WILL SHUT DOWN CONSTRUCTION OPERATIONS WHEN BLOWING MATERIAL BECOMES HAZARDOUS TO AIRFIELD OPERATIONS. SUCH A SHUTDOWN SHALL NOT BE THE BASIS OF CLAIMS FOR ADDITIONAL COSTS OR CONTRACT TIME.

24. PROTECT PAVEMENTS AND UNDERGROUND UTILITIES FROM DAMAGE CAUSED BY CONSTRUCTION EQUIPMENT. ANY DAMAGE TO PAVEMENTS OR FACILITIES CAUSED BY THE CONTRACTOR'S EQUIPMENT SHALL BE REPAIRED OR REPLACED IN KIND BY THE CONTRACTOR AT NO ADDITIONAL CONTRACT COST.

25. ANY AIRPORT AREA OR FEATURE THAT IS DISTURBED OUTSIDE THE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED OR REPLACED IN KIND BY THE CONTRACTOR AT NO ADDITIONAL CONTRACT COST.

26. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ACQUISITION OF AND THE COST OF WATER AS REQUIRED FOR DUST CONTROL, DEMOLITION, EARTHWORK, BASE CONSTRUCTION AND ALL OTHER CONSTRUCTION OPERATIONS. ALL COSTS OF SAID WATER ACQUISITION INCLUDING PERMITS, MONTHLY BILLING, TEMPORARY PIPING, CONNECTIONS, HYDRANTS, METERS AND DEMOLITION WILL BE CONSIDERED SUBSIDIARY AND WILL NOT BE MEASURED AND PAID FOR SEPARATELY. THE OWNER, ARCHITECT AND/OR ENGINEER SHALL APPROVE ALL UTILITY AGREEMENTS AND ARRANGEMENTS. 27. ALL EARTH, ASPHALT, CONCRETE, CEMENT, TRASH AND OTHER DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS SHALL BE REMOVED CONTINUOUSLY FROM ROADWAYS, STREETS AND PARKING LOTS OPEN TO TRAFFIC DURING CONSTRUCTION. ALL COSTS ASSOCIATED WITH SAID CLEANING TO BE INCLUDED IN THE COSTS OF OTHER ITEMS AND WILL NOT BE PAID FOR SEPARATELY.

28. ANY MARKED AND/OR LOCATED UTILITIES OR UNDERGROUND FEATURES DAMAGED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED IN KIND AT NO ADDITIONAL CONTRACT COST IF THE UTILITY OR FEATURE IS TO REMAIN OPERATIONAL.

## <u>COORDINATION</u>

1. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR BECOMING FAMILIAR WITH THE REQUIREMENTS OF ALL CONSTRUCTION DOCUMENTS WHICH INCLUDES DRAWINGS, SPECIFICATIONS, SOIL, AND ENVIRONMENTAL REPORTS, THE CONTRACT FOR CONSTRUCTION, GENERAL CONDITIONS OF THE CONTRACT AND ALL ADDENDA.

2. LETTER PREFIXES FOR EACH DRAWING SHEET INDICATING THE ENGINEERING DISCIPLINE & BUILDING TYPE ARE FOR CONVENIENCE ONLY. INFORMATION AFFECTING THE SCOPE OF WORK FOR ALL TRADES WILL BE FOUND THROUGHOUT ALL DOCUMENTS AND IS NOT LIMITED TO ONLY THOSE DOCUMENTS WITH THE APPROPRIATE LETTER PREFIX.

3. THE CONTRACTOR SHALL CONSULT AND COORDINATE WITH THE OWNER REGARDING LIMITATIONS FOR SITE ACCESS, MOBILIZATION, CONSTRUCTION WORK HOURS, AND WORK BEING DONE BY THE OWNER OR SEPARATE OWNER CONTRACTOR.

4. THE CONTRACTOR SHALL CONSULT AND COORDINATE WITH THE OWNER FOR ITEMS PURCHASED DIRECTLY BY THE OWNER BUT INSTALLED BY THE CONTRACTOR.

5. THE ARCHITECT/ENGINEER IS THE SOLE INTERPRETER OF THE CONTRACT DOCUMENTS INCLUDING ANY APPARENT CONFLICTS OR OMISSIONS. THE CONTRACTOR SHALL REPORT ANY CONFLICTS, ETC. TO THE ARCHITECT/ENGINEER PRIOR TO BID AND CONSTRUCTION, OTHERWISE IT SHALL BE ASSUMED HE UNDERSTANDS FULLY THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.

6. SEE SPECIFICATIONS AND DRAWINGS FOR ADDITIONAL COORDINATION REQUIREMENTS.

7. ON ELEMENTS LABELED N.I.C. (NOT IN CONTRACT), G.C. TO COORDINATE / ACCOMMODATE FUTURE LOCATIONS WHEN CONSTRUCTING WALLS, FLOORS, CEILINGS, SIDEWALKS, ETC. WHICH WILL PREVENT THESE FUTURE ELEMENTS FROM BEING ACCESSIBLE.

## DEMOLITION NOTES

1. EXISTING CONDITIONS SHOWN ARE FROM AVAILABLE RECORD DRAWINGS AND SHOWN FOR REFERENCE ONLY. VERIFY ACTUAL EXISTING CONDITIONS ON SITE PRIOR TO SUBMITTING BID AND BEGINNING WORK. ALL DEMOLITION, ALTERATION, EXTENSION, RELOCATION, REHABILITATION AND NEW CONSTRUCTION SHALL BE INCLUDED IN CONTRACT.

2. NOTIFY THE CONSTRUCTION MANAGER IMMEDIATELY WHERE EXISTING EQUIPMENT OR UTILITIES ARE ENCOUNTERED WHICH MUST BE RELOCATED DUE TO THE NEW CONSTRUCTION OR NOT INDICATED ON THE "AS BUILT" OR WAS BURIED OR EMBEDDED IN EXISTING STRUCTURE.

3. PROTECT ALL WALLS, TRIM, FLOORS, EQUIPMENT AND MATERIALS WHEN WORKING ON FINISHED SURFACES TO REMAIN. LIMIT DAMAGE TO THE SMALLER AREA IF POSSIBLE AND RESTORE TO THE ORIGINAL CONDITION ALL SURFACES WHICH ARE DAMAGED DUE TO THE INSTALLATION OF THIS WORK.

4. EQUIPMENT, MATERIALS AND SUPPLIES TEMPORARILY REMOVED FOR PROTECTION SHALL BE REPLACED IN ORIGINAL LOCATIONS OR AS SPECIFIED ON DRAWINGS. REPLACE MATERIALS DAMAGED WITH NEW MATERIALS OF LIKE KIND AND QUALITY. AFTER CONCLUSION OF DEMOLITION AND NEW CONSTRUCTION, CONTRACTOR TO CLEAN AND MAKE ALL EXISTING FINISH MATERIALS BOTH INTERIOR AND EXTERIOR LIKE NEW.

5. PROVIDE TEMPORARY SUPPORT OR SHORING TO BUILDING STRUCTURE AND COMPONENTS BEING DEMOLISHED AS REQUIRED INCLUDING BUT NOT LIMITED TO: WHEN THE WHOLE OR ANY PART OF THE STRUCTURE IS SUBJECTED TO EXCESS LOADING DERIVED FROM THE DEMOLITION ACTIVITIES, MOVEMENT OF POWERED MECHANICAL EQUIPMENT, OR DEBRIS ACCUMULATION; WHEN ANY PART OF THE STRUCTURE OR ANY ELEMENT BEING DEMOLISHED IS NOT SELF-SUPPORTING; OR WHEN THE TEMPORARY STABILITY OF THE STRUCTURE OR ITS ELEMENTS COULD BE IMPAIRED AS A RESULT OF THE DEMOLITION ACTIVITIES.

6. WHEN THE DEMOLITION ACTIVITIES ARE SHUT DOWN FOR A PROLONGED PERIOD OF TIME BEFORE ITS COMPLETION FOR ANY REASON, THE REMAINING STRUCTURE IF ANY, SHALL BE STABILIZED BY TEMPORARY SUPPORT AND/OR BRACING SYSTEMS.

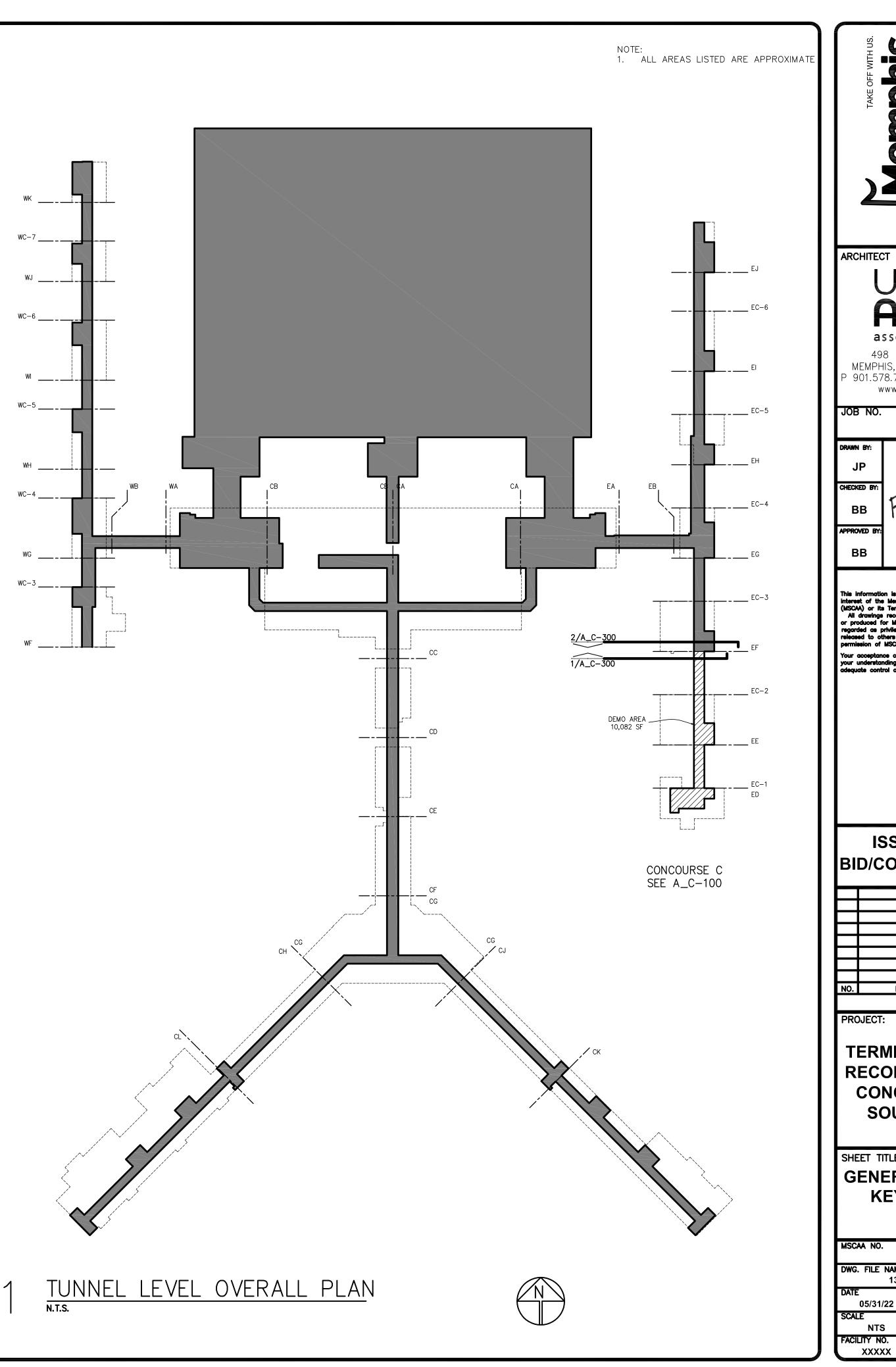
7. PROTECT EXISTING BUILDING TO REMAIN AS REQUIRED TO PREVENT DAMAGE DURING DEMOLITION. STABILITY TREATMENT SHALL BE PROVIDED TO PROTECT BUILDING ELEMENTS THAT MAY BE AFFECTED BY THE DEMOLITION PROJECT. THE DESIGN OF THE BRACING OR PROTECTION SYSTEM SHALL BE BASED ON A STRUCTURAL ASSESSMENT AND ENGINEERED BY THE CONTRACTOR PENDING THE APPROVAL OF THE OWNER, ARCHITECT, AND/OR ENGINEER.

8. IF ANY UNSAFE CONDITIONS ARE PRESENT AS RECOGNIZED BY THE OWNER, ARCHITECT, ENGINEER, CONTRACTOR OR ANY OTHER AUTHORIZED PERSONELL, ALL DEMOLITION ACTIVITIES SHALL BE IMMEDIATELY HALTED UNTIL THE UNSAFE CONDITIONS ARE RECTIFIED. ALL UNSAFE CONDITIONS SHALL BE REPORTED TO THE MSCAA PROJECT MANAGER FOR FURTHER INSTRUCTIONS.

9. CONTRACTOR TO SUBMIT FOR APPROVAL A DEMOLITION PLAN REPORT INCLUDING BUT NOT LIMITED TO: DEMOLITION SEQUENCE, DEMOLITION METHODS, POWERED MECHANICAL DEMOLITION EQUIPMENT LIST, EXPECTED EXISTING BUILDING PROTECTION MEASURES, PROPOSED DUST-CONTROL MEASURES, PROPOSED NOISE-CONTROL MEASURES, CONTRACTOR'S WASTE MANAGEMENT AND RECYCLING PLAN, ETC. SEE DEMOLITION SPEC FOR MORE INFO.

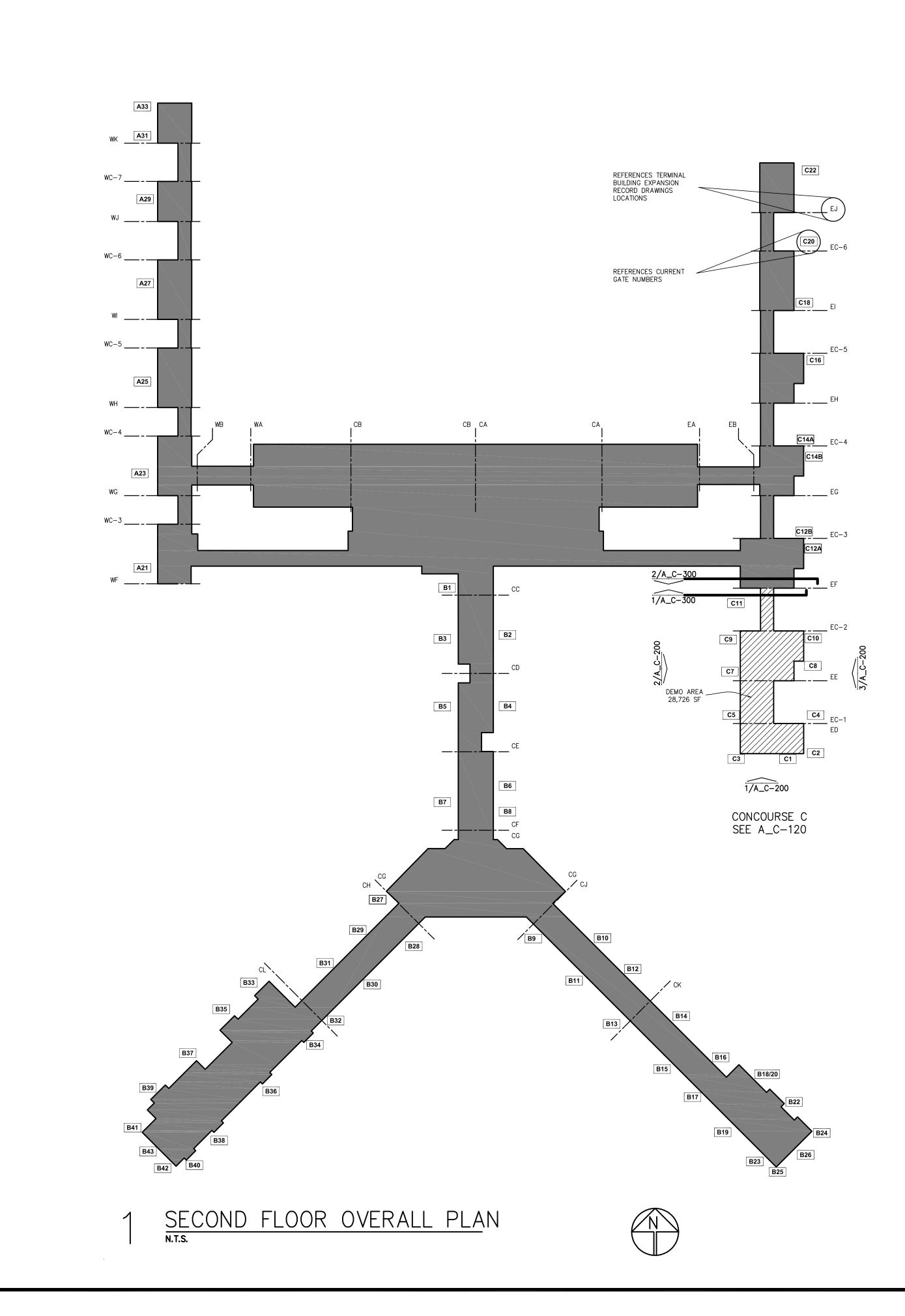
10. TEMPORARY EXCAVATION SHORING SHALL BE BASED ON A STRUCTURAL ASSESSMENT AND ENGINEERED BY THE CONTRACTOR PENDING THE APPROVAL OF THE OWNER, ARCHITECT, AND/OR ENGINEER. PARAMETERS AND LIMITS FOR TEMPORARY EXCAVATION SHORING ARE SET FORTH IN THE SPECIFICATIONS AND GEOTECHNICAL INVESTIGATION REPORT. CONTRACTOR TO LOCATE SHORING IN THE FIELD TO INCORPORATE AND ACCOUNT FOR NEARBY LOCATIONS OF BUILDING FOOTINGS, UTILITY LINES, POSSIBLE CEMENT TREATED BASE BACKFILL AGAINST TUNNEL WALLS, ETC.

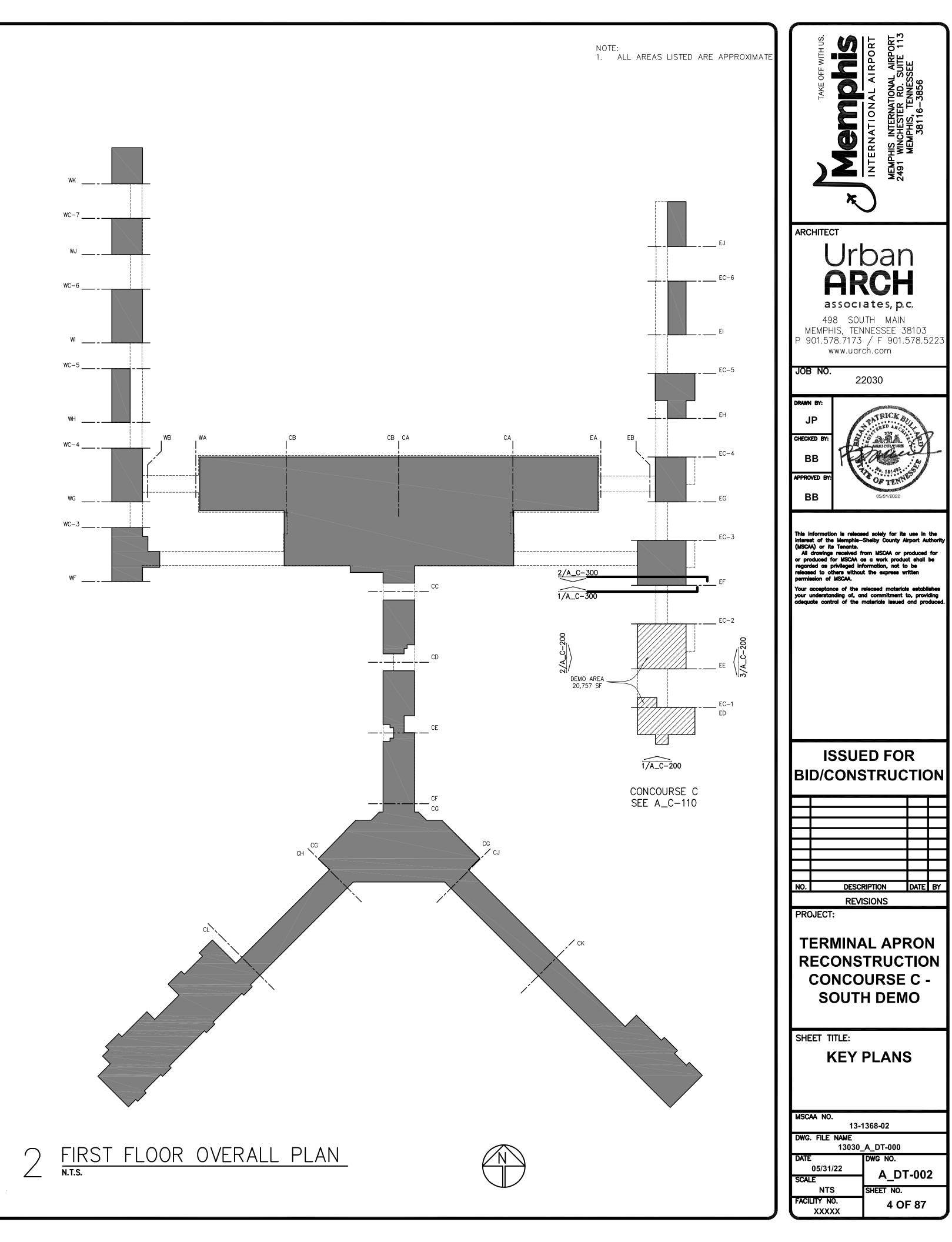
11. SEE CIVIL SHEET C-PB-01 FOR RESPONSIBILITY MATRIX OF PBB (PASSENGER BOARDING BRIDGE) REMOVAL.

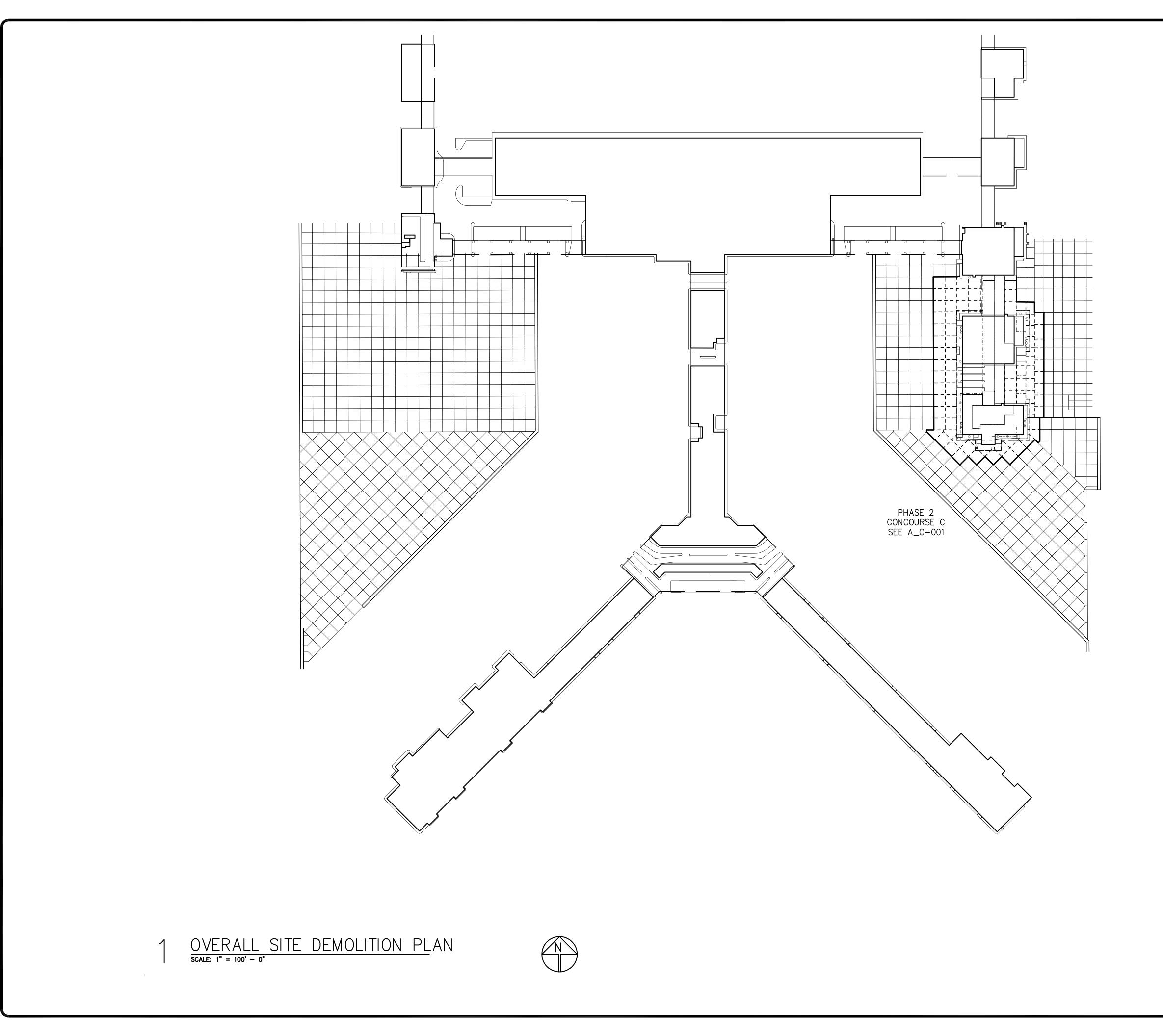


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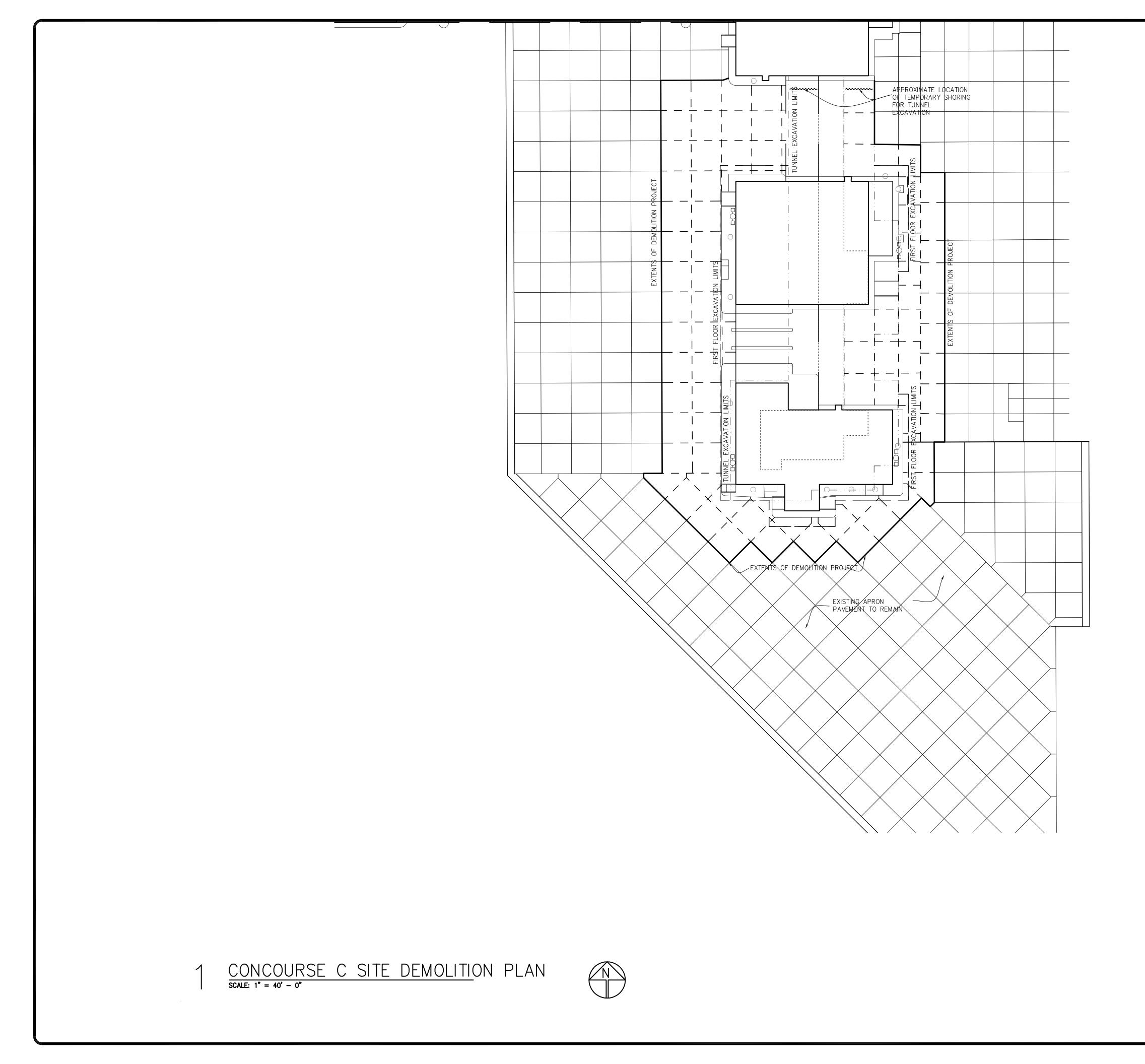
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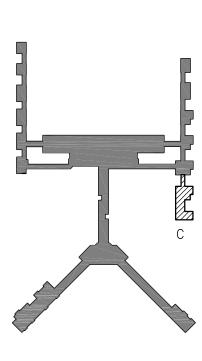


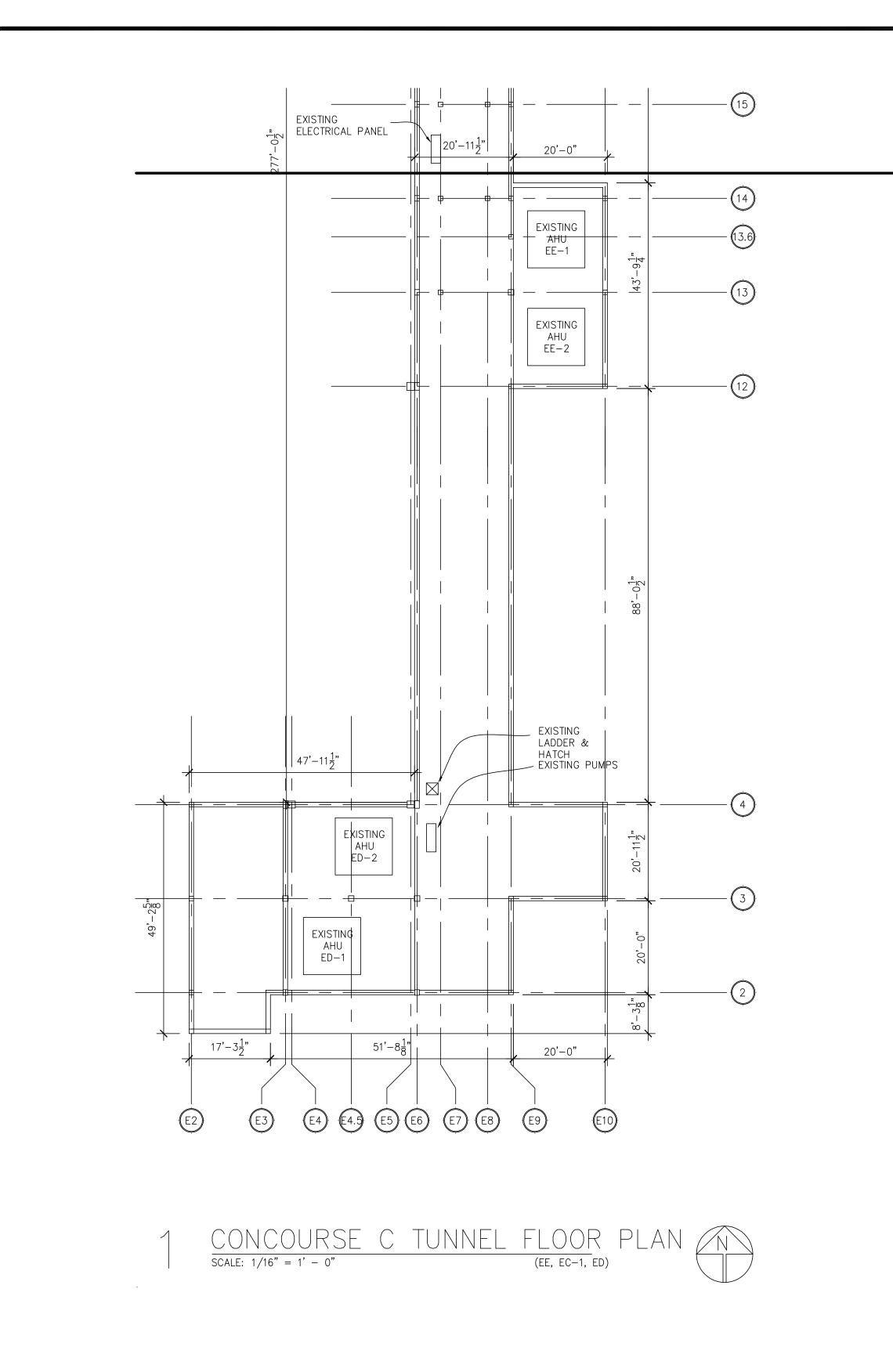


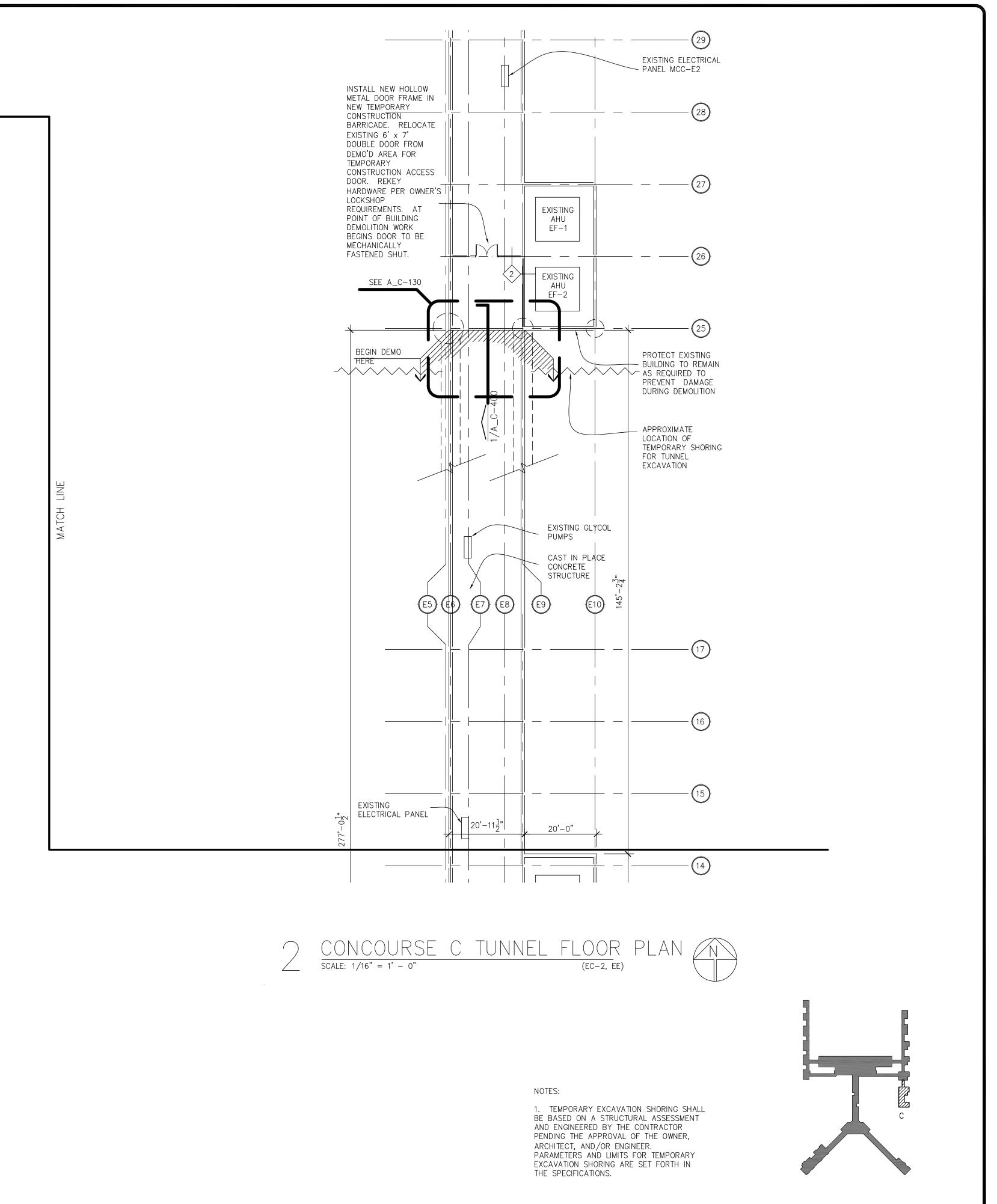
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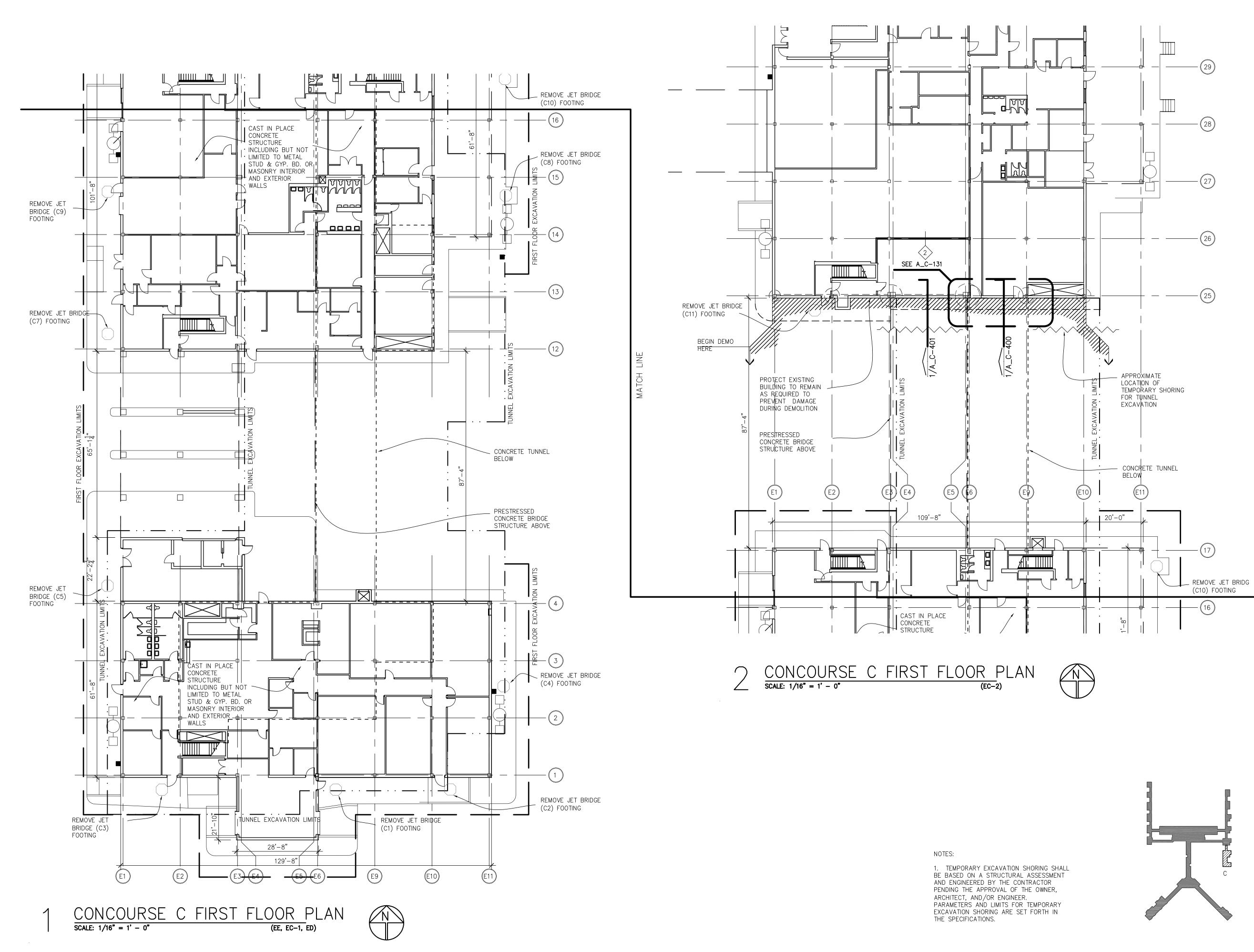




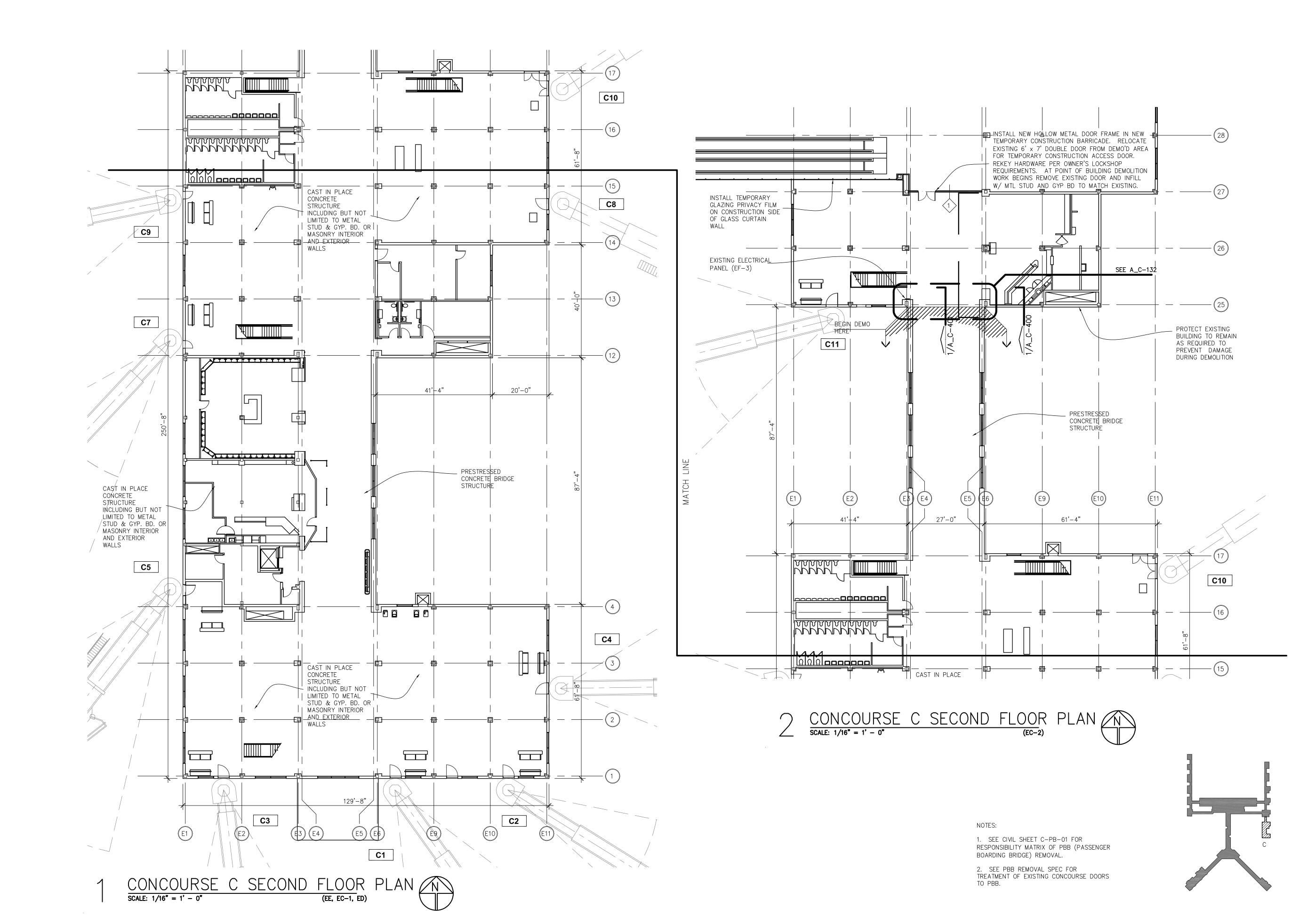


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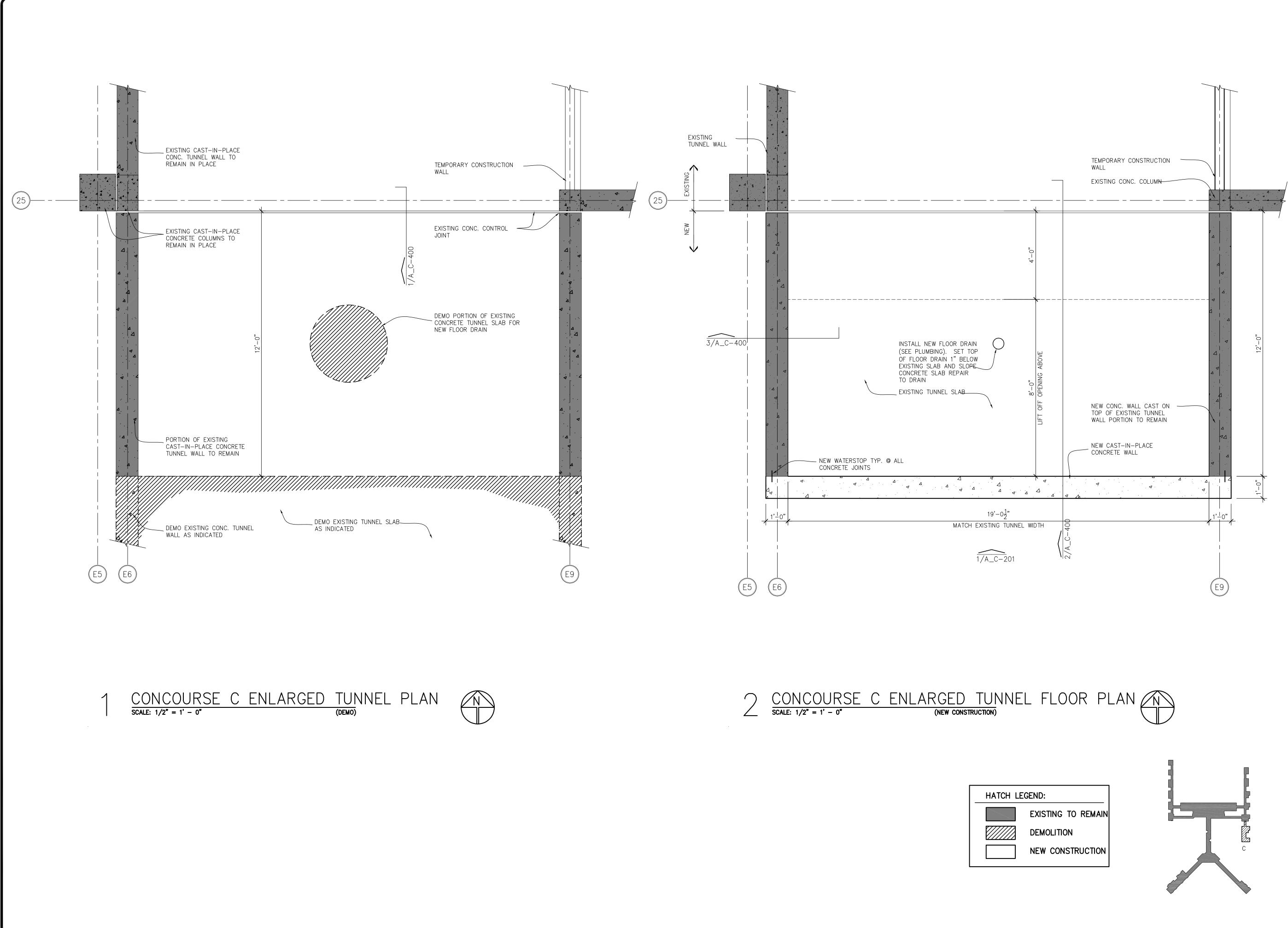
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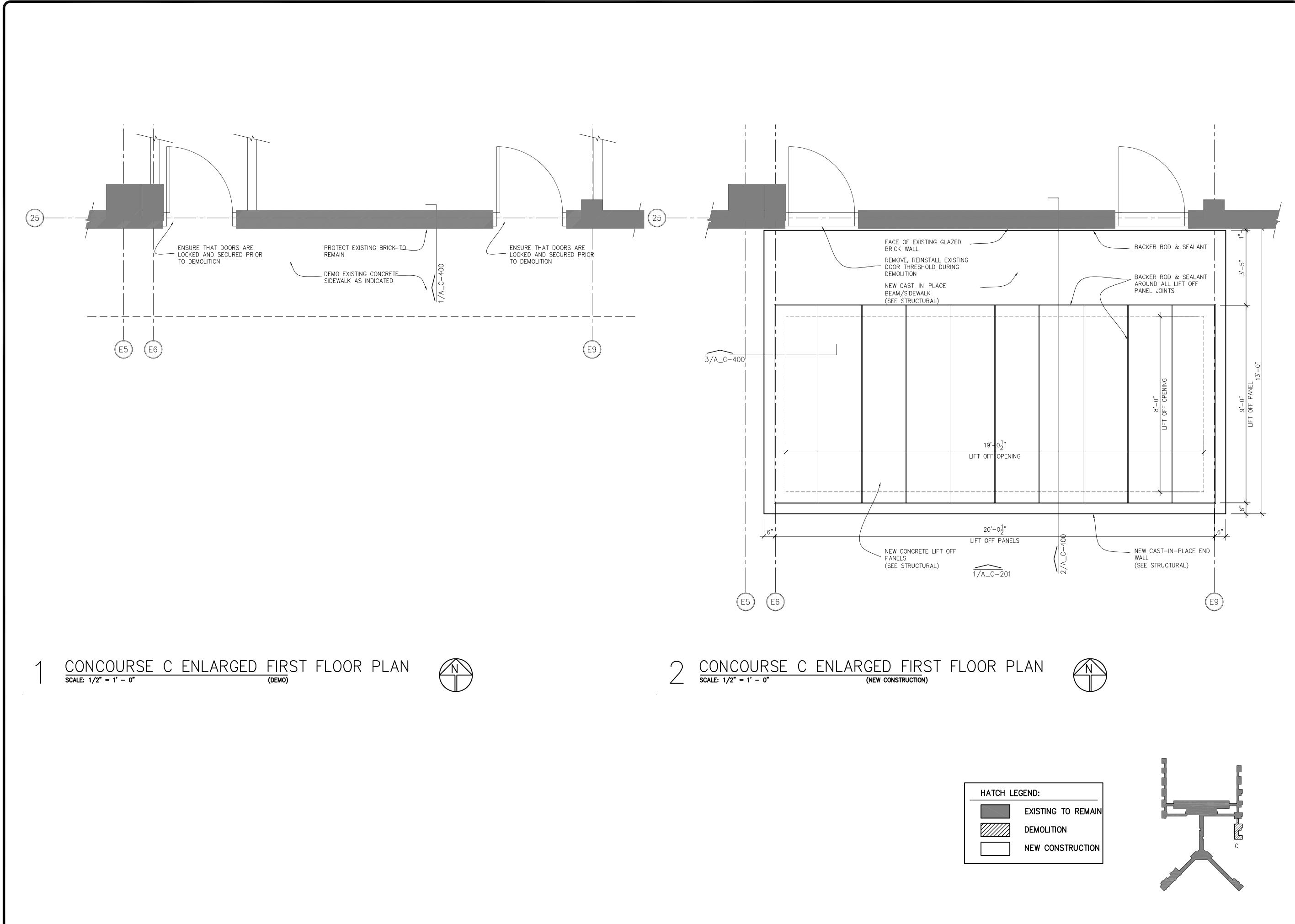


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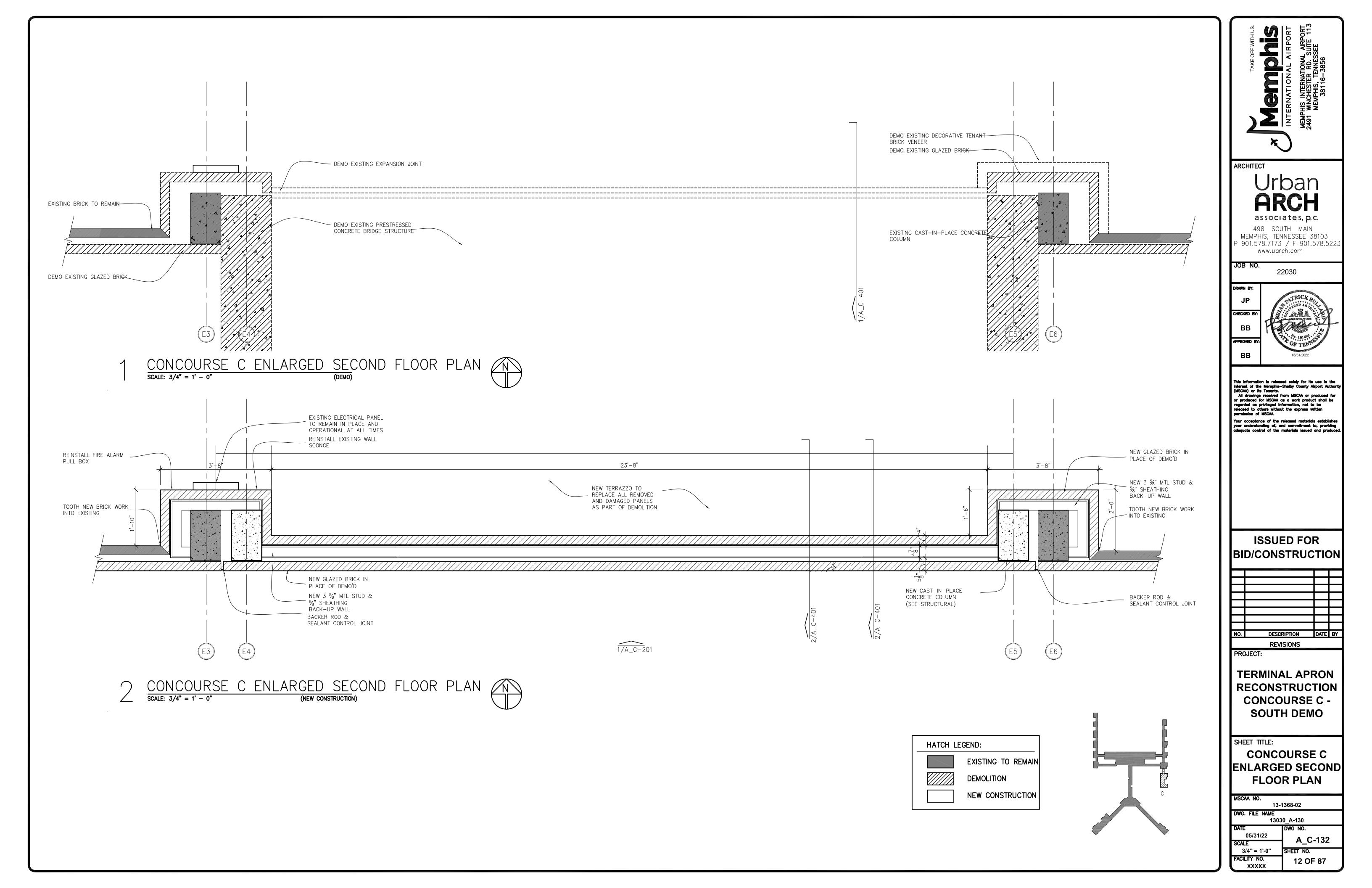
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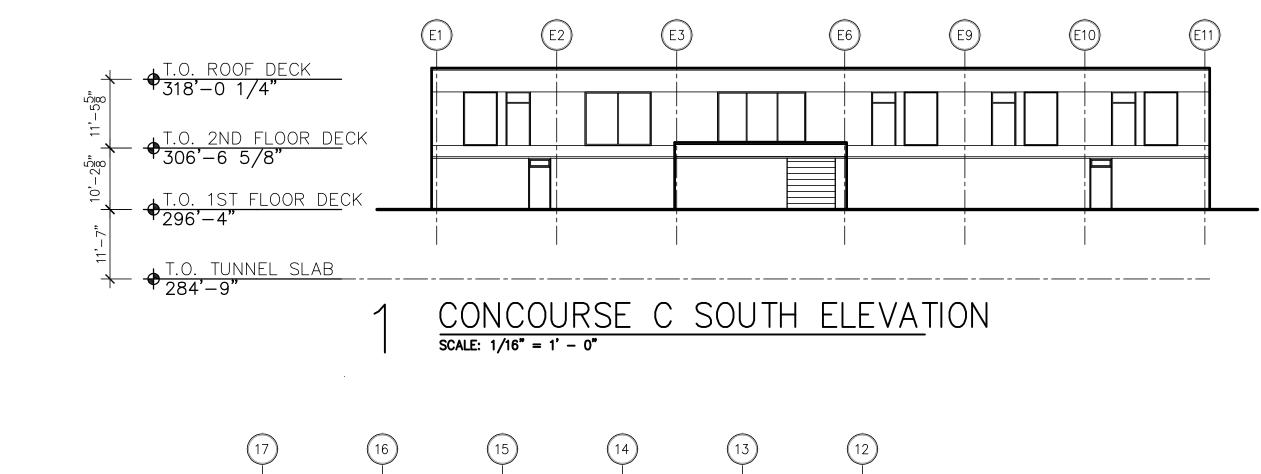
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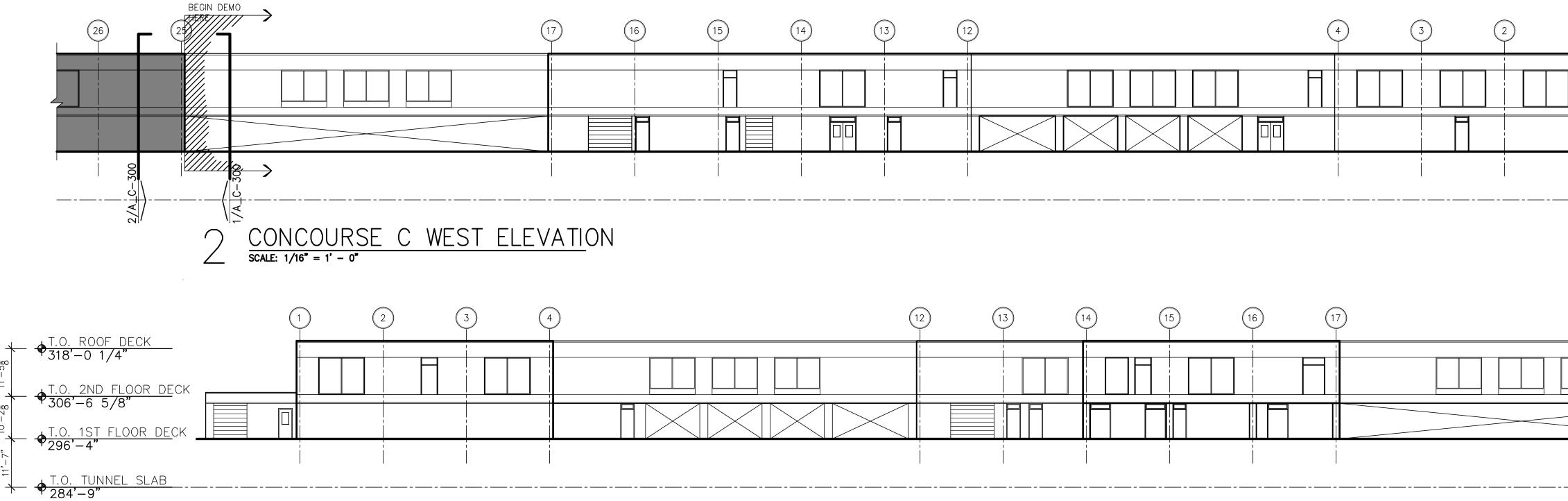


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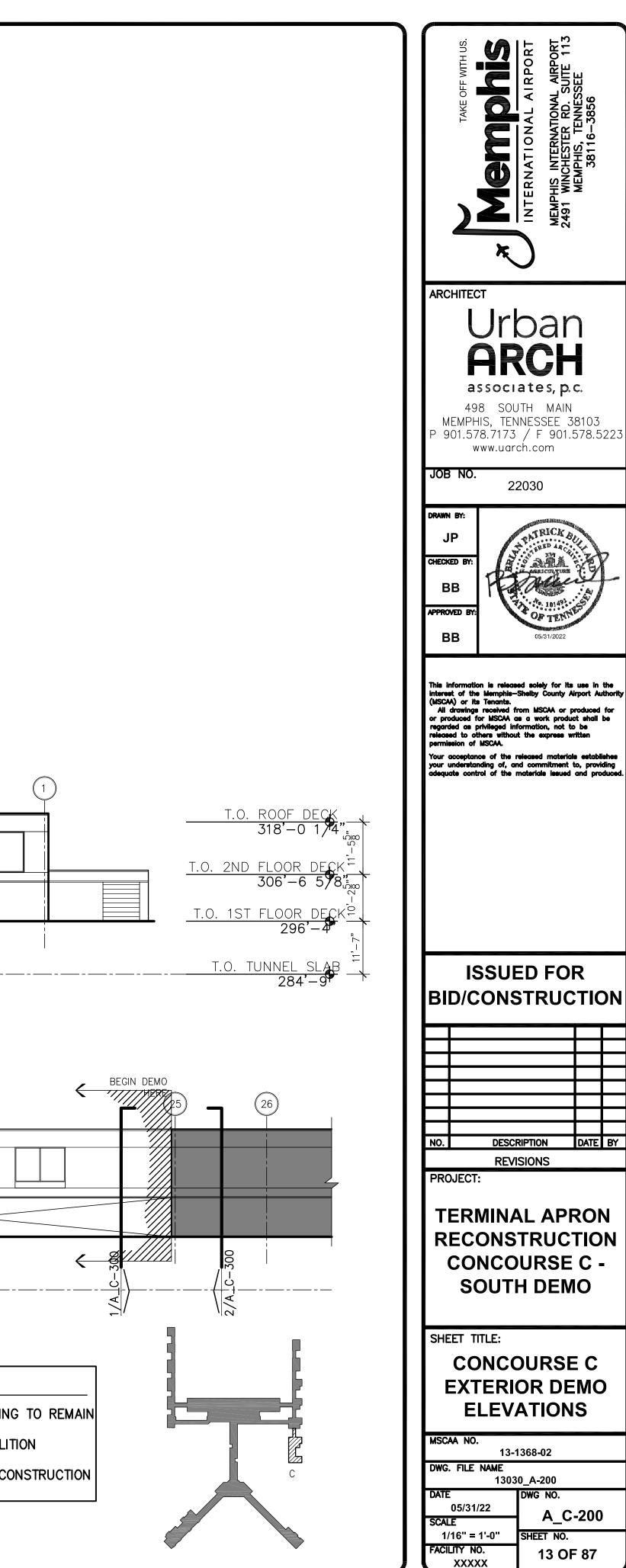
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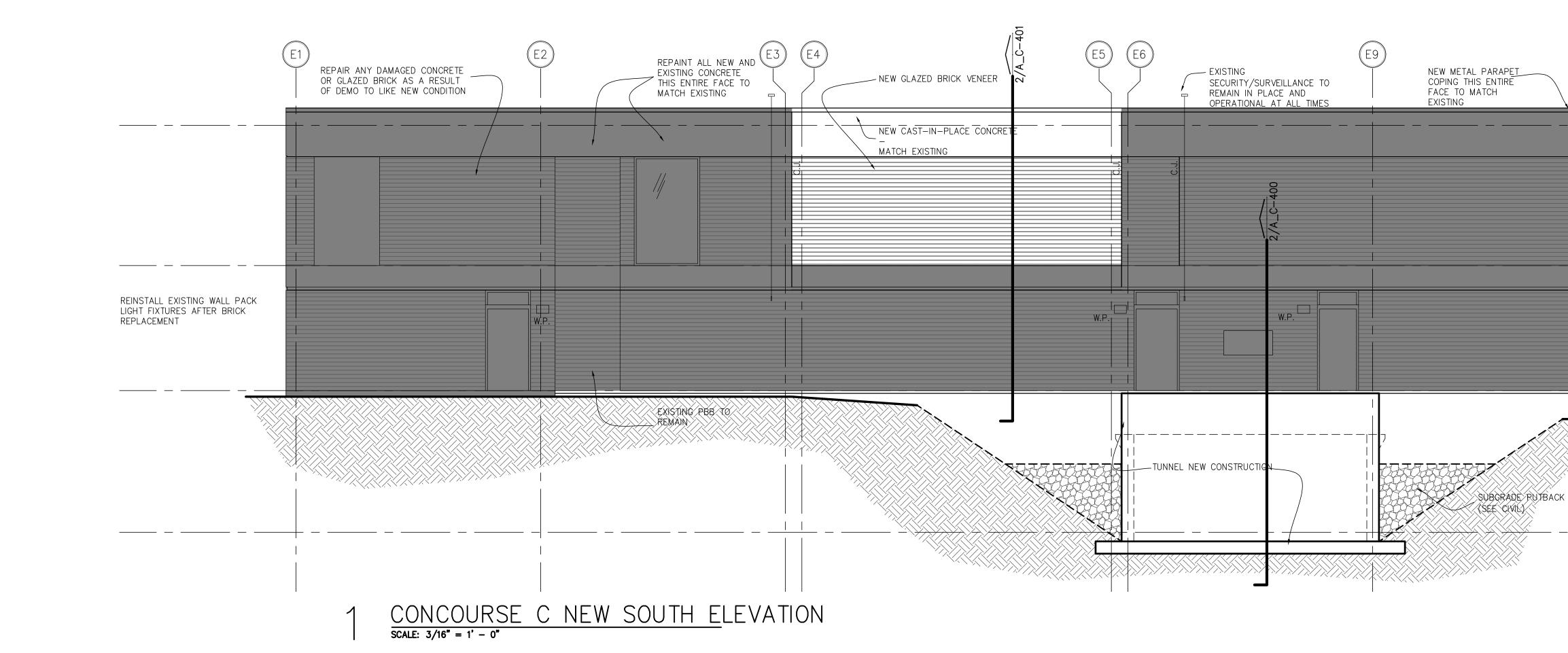
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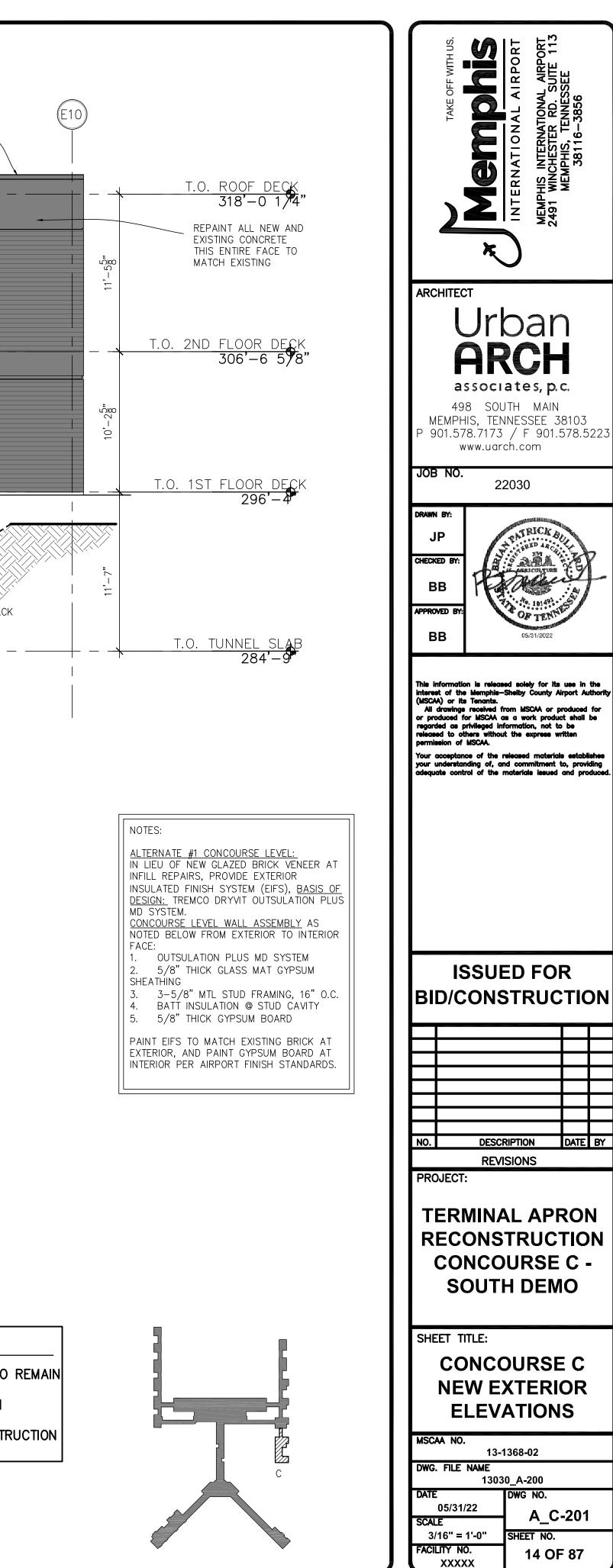
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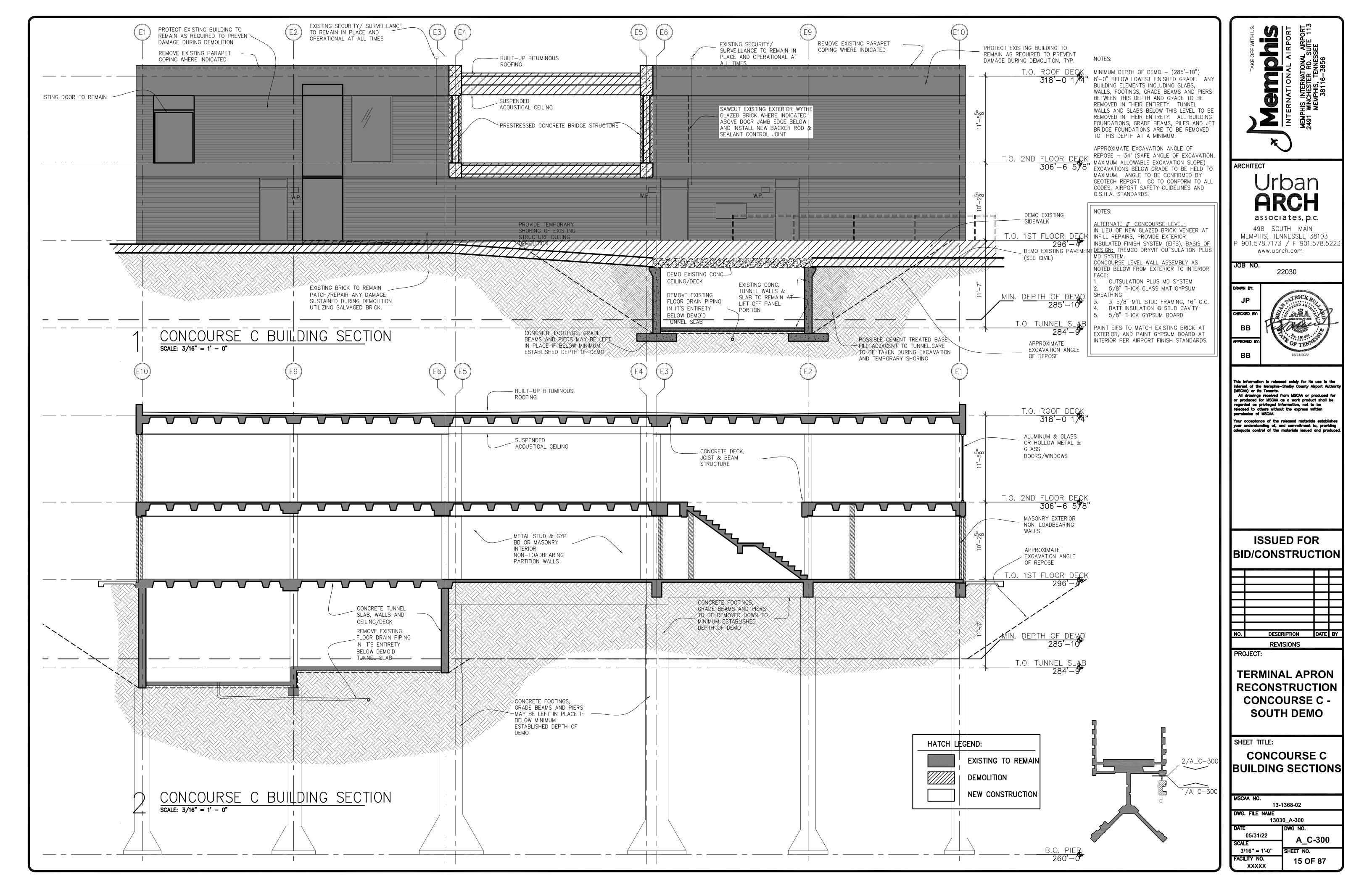
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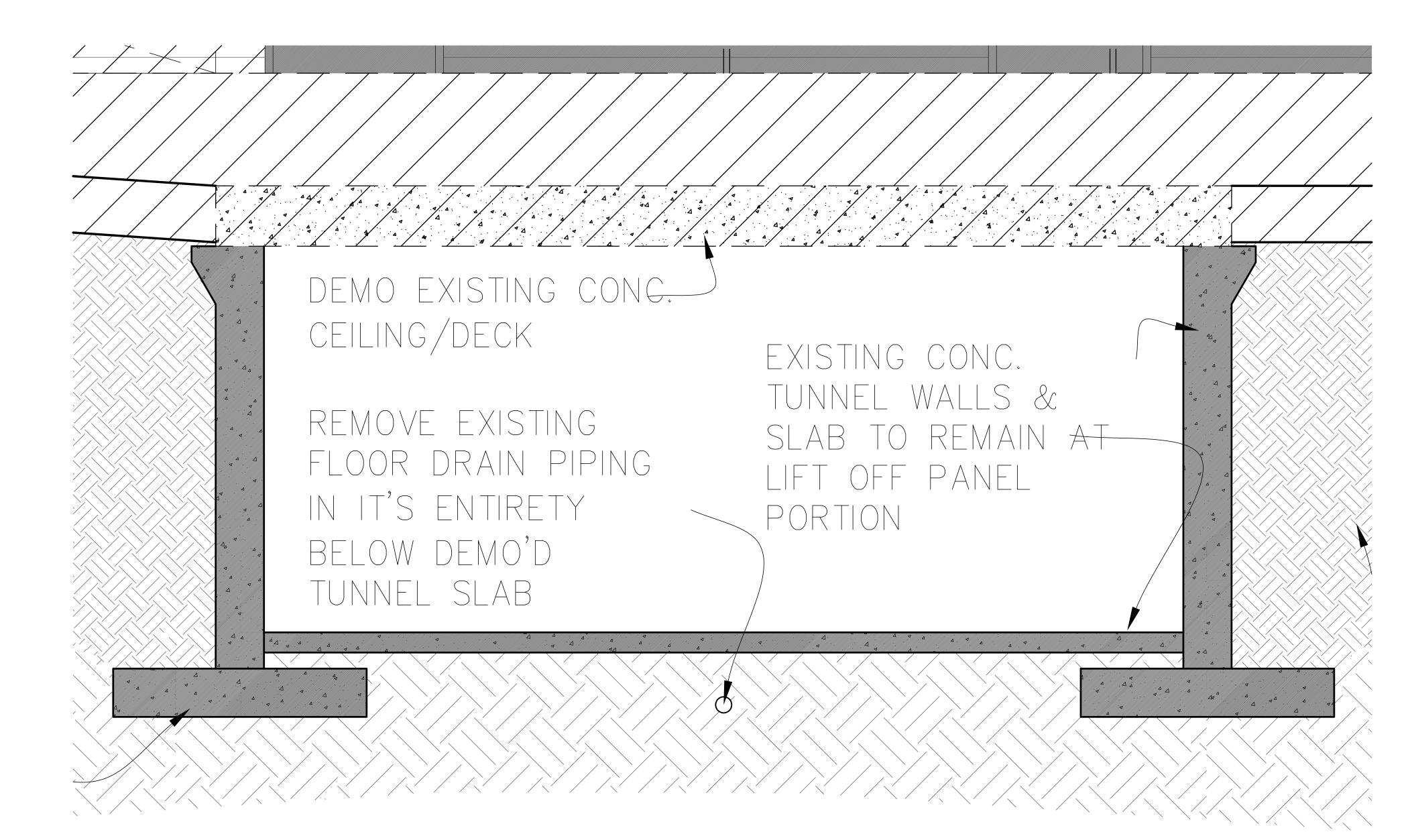




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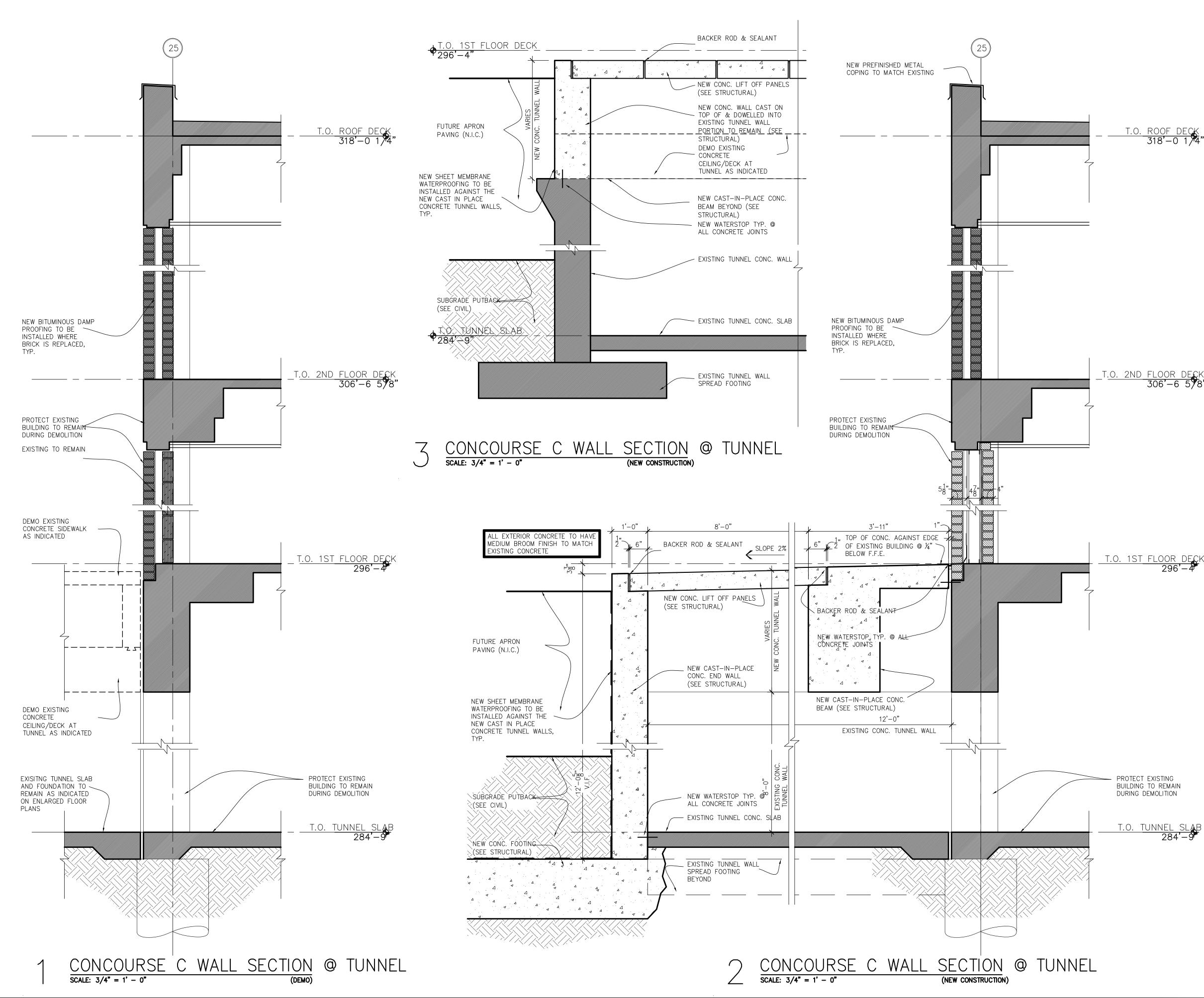








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## NOTES:

ALTERNATE #1 CONCOURSE LEVEL: IN LIEU OF NEW GLAZED BRICK VENEER AT INFILL REPAIRS, PROVIDE EXTERIOR INSULATED FINISH SYSTEM (EIFS), <u>BASIS OF</u> <u>DESIGN:</u> TREMCO DRYVIT OUTSULATION PLUS MD SYSTEM. CONCOURSE LEVEL WALL ASSEMBLY AS NOTED BELOW FROM EXTERIOR TO INTERIOR FACE: 1. OUTSULATION PLUS MD SYSTEM 2. 5/8" THICK GLASS MAT GYPSUM SHEATHING

3. 3-5/8" MTL STUD FRAMING, 16" O.C. 4. BATT INSULATION @ STUD CAVITY 5. 5/8" THICK GYPSUM BOARD

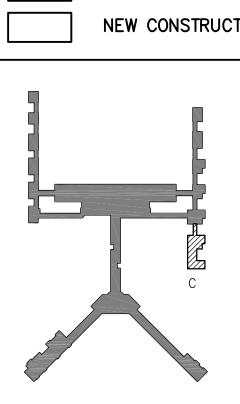
PAINT EIFS TO MATCH EXISTING BRICK AT EXTERIOR, AND PAINT GYPSUM BOARD AT INTERIOR PER AIRPORT FINISH STANDARDS.

T.O. 2ND FLOOR DECK 306'-6 578"

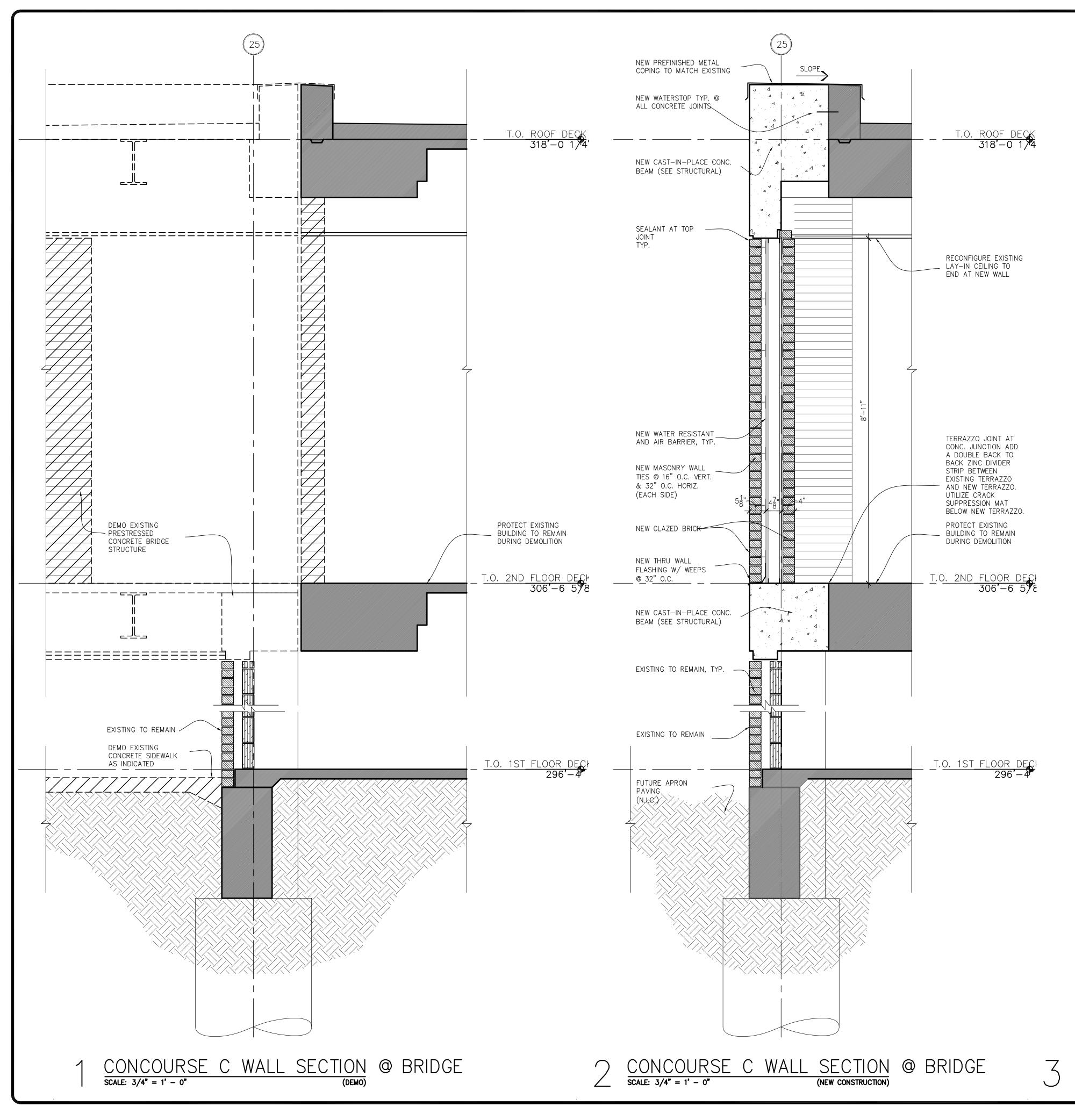
T.O. 1ST FLOOR DECK 296'—4"

> HATCH LEGEND: EXISTING TO REMAIN DEMOLITION NEW CONSTRUCTION

T.O. TUNNEL SLAB 284'-9



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drawn by: JP Checked by: BB Approved by: BB			RICK BURNERS
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CONCOURSE C WALL SECTION @ BRIDGE SCALE: 3/4" = 1' - 0" (NEW CONSTRUCTION)

## NOTES:

ALTERNATE #1 CONCOURSE LEVEL: IN LIEU OF NEW GLAZED BRICK VENEER AT INFILL REPAIRS, PROVIDE EXTERIOR INSULATED FINISH SYSTEM (EIFS), <u>BASIS OF</u> <u>DESIGN:</u> TREMCO DRYVIT OUTSULATION PLUS MD SYSTEM. <u>CONCOURSE LEVEL WALL ASSEMBLY</u> AS NOTED BELOW FROM EXTERIOR TO INTERIOR FACE: 1. OUTSULATION PLUS MD SYSTEM 2. 5/8" THICK GLASS MAT GYPSUM SHEATHING 3. 3–5/8" MTL STUD FRAMING, 16" O.C. 4. BATT INSULATION @ STUD CAVITY 5. 5/8" THICK GYPSUM BOARD PAINT EIFS TO MATCH EXISTING BRICK AT EXTERIOR, AND PAINT GYPSUM BOARD AT INTERIOR PER AIRPORT FINISH STANDARDS.

HATCH LE	GEND:
	EXISTING TO REMAIN
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	NEW CONSTRUCTION

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36	C-HG-02	CIVIL DETAILS BOLLARDS GUARDRAIL UTILITY MARKERS	
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38	C-HG-04	CIVIL FLATWORK DETAILS TYPE 2 RAMP	
39	C-HG-05	CIVIL FLATWORK DETAILS TYPE 3 RAMP	
40	C-HG-06	CIVIL FLATWORK DETAILS STEPS HANDRAILS	
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46	C-EG-01 C-GD-01	GRADING AND DRAINAGE PLAN	
46 47	C-EG-01 C-GD-01 C-GD-02	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION	
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46 47 48 49	C-EG-01 C-GD-01 C-GD-02 C-GD-03 C-GD-04	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES	
46 47 48 49 50	C-EG-01 C-GD-01 C-GD-02 C-GD-03 C-GD-04 C-GD-05	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN	
46 47 48 49 50 51	C-EG-01 C-GD-01 C-GD-02 C-GD-03 C-GD-04 C-GD-05 C-GD-06	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN	
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46 47 48 49 50 51 52	C-EG-01 C-GD-01 C-GD-02 C-GD-03 C-GD-04 C-GD-05 C-GD-06 C-GD-07 C-GD-08	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS PRECAST MANHOLE STRUCTURES STORM DRAINAGE DETAILS CAST IN PLACE INLETS AT	
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46 47 48 49 50 51 52 53 54	C-EG-01 C-GD-01 C-GD-02 C-GD-03 C-GD-04 C-GD-04 C-GD-05 C-GD-06 C-GD-07 C-GD-07 C-GD-08	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS PRECAST MANHOLE STRUCTURES STORM DRAINAGE DETAILS CAST IN PLACE INLETS AT TRENCH DRAINS STORM DRAINAGE DETAILS PIPE BEDDING, PLUGS, COLLARS, AND ENCASEMENT	
46 47 48 49 50 51 52 53 54 55	C-EG-01 C-GD-01 C-GD-02 C-GD-03 C-GD-04 C-GD-04 C-GD-05 C-GD-06 C-GD-07 C-GD-07 C-GD-08 C-GD-09	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS PRECAST MANHOLE STRUCTURES STORM DRAINAGE DETAILS CAST IN PLACE INLETS AT TRENCH DRAINS STORM DRAINAGE DETAILS PIPE BEDDING, PLUGS, COLLARS, AND ENCASEMENT EROSION CONTROL NOTES AND DETAILS	
46 47 48 49 50 51 52 53 54 55 56	C-EG-01 C-GD-01 C-GD-02 C-GD-03 C-GD-04 C-GD-05 C-GD-06 C-GD-07 C-GD-07 C-GD-08 C-GD-09 C-GD-10 C-GD-11	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS PRECAST MANHOLE STRUCTURES STORM DRAINAGE DETAILS PRECAST IN PLACE INLETS AT TRENCH DRAINS STORM DRAINAGE DETAILS PIPE BEDDING, PLUGS, COLLARS, AND ENCASEMENT EROSION CONTROL NOTES AND DETAILS EROSION CONTROL DETAILS	
46 47 48 49 50 51 52 53 54 55 56 57	C-EG-01 C-GD-02 C-GD-03 C-GD-04 C-GD-05 C-GD-06 C-GD-07 C-GD-07 C-GD-09 C-GD-10 C-GD-11 C-GD-12	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS PRECAST MANHOLE STRUCTURES STORM DRAINAGE DETAILS CAST IN PLACE INLETS AT TRENCH DRAINS STORM DRAINAGE DETAILS PIPE BEDDING, PLUGS, COLLARS, AND ENCASEMENT EROSION CONTROL NOTES AND DETAILS EROSION CONTROL DETAILS	
46 47 48 49 50 51 52 53 54 55 56 57 58	C-EG-01 C-GD-02 C-GD-03 C-GD-04 C-GD-05 C-GD-06 C-GD-07 C-GD-07 C-GD-09 C-GD-10 C-GD-11 C-GD-12 C-UD-01	GRADING AND DRAINAGE PLAN GRADING AND DRAINAGE TABULATION STORM DRAINAGE PROFILES TRENCH DRAIN PROFILES STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS CAST IN PLACE TRENCH DRAIN STORM DRAINAGE DETAILS PRECAST MANHOLE STRUCTURES STORM DRAINAGE DETAILS PRECAST MANHOLE STRUCTURES STORM DRAINAGE DETAILS CAST IN PLACE INLETS AT TRENCH DRAINS STORM DRAINAGE DETAILS PIPE BEDDING, PLUGS, COLLARS, AND ENCASEMENT EROSION CONTROL NOTES AND DETAILS EROSION CONTROL DETAILS EROSION CONTROL PLAN	

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	C-CP-04	CONCRETE PAVEMENT JOINTING LAYOUT PLAN CONCRETE PAVEMENT DETAILS CONCRETE PAVEMENT DETAILS CONCRETE PAVEMENT DETAILS		
	C-PE-01	APRON PAVEMENT FINISHED GRADE ELEVATIONS		
	WATER LI	NE SHEETS (1)		
	C-WT-01	WATER LINE PLAN		

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TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO					
SHEET TITLE: INDEX OF DRAWINGS					
MSCAA NO. 13-1368-02 DWG. FILE NAME 13136802_G-ID-01 DATE 05/31/22 SCALE SHEET NO. FACILITY NO. 19 OF 87					

	ITEM	FAA SPEC		0		NTITY	
N	No.	No.	DESCRIPTION	UNIT	ORIGINAL		
			MISCELLANEOUS ITEMS				
	1	C-100-14.1	CONTRACTOR QUALITY CONTROL PROGRAM (CQCP)	LS	1		
	2	C-105-6.1	MOBILIZATION	LS	1		
_							
_	3	C-102-5.1			300		
_	4 5	C-102-5.2 C-102-5.3	EROSION CONTROL BARRIER SILT FENCE	LF LF	200 1,000		
┥	6	C-102-5.3 C-102-5.4	MANHOLE SEDIMENT TRAP		3		
	7	C-102-5.5	SEDIMENT BASIN	EA	1		
	8	C-102-5.6	TEMPORARY CONSTRUCTION EXIT	EA	2		
	_						
	9	S-100-6.1	GUARD HOUSE	EA	2		
	10	S-100-6.2	FIELD OFFICE TRAILER AND CURRING FACILITIES	LS	1		
	11	S-100-6.3	CROSSING GATE ARM	EA	4		
_	12	S-100-6.4	PROJECT SAFETY & SECURITY	LS	1		
	4.0						
┥	13	S-101-5.1	UTILITY MARKERS	EA	25		
┥	11	S 103 5 1		SF	200		
┥	14	S-103-5.1	EXTERIOR BUILDING REPAIRS		200		
		I	NATER LINE ITEMS		1		
T	15	S-105-5.1	12" DUCTILE IRON PIPE WITH RESTRAINED JOINTS	LF	300		
┨	16	S-105-5.2	6" DUCTILE IRON PIPE WITH RESTRAINED JOINTS		30		
	17	S-105-5.3	FLUSH MOUNTED FIRE HYDRANT	EA			
1	18	SC-30-4	MLGW WATER LINE FEES	ALLOW			
1							
		1	DEMOLITION ITEMS				
ļ		P-101-5.1	PAVEMENT DEMOLITION, CONCRETE (ALL THICKNESS AND TYPES)	SY	14,500		
		P-101-5.2	PAVEMENT DEMOLITION, ASPHALT (ALL THICKNESS AND TYPES)	SY	14,500		
	21	P-101-5.3	STABILIZED BASE DEMOLITION, CONCRETE (ALL THICKNESS AND TYPES)	SY	14,500		
_		D 450 4 4			500		
		P-150-4.1	DEMOLITION OF EXISTING STORM DRAIN PIPE (ALL SIZES)		500		
+		P-150-4.2	DEMOLITION OF EXISTING STORM DRAIN MANHOLE	EA	2		
	24	P-150-4.3 P-150-4.4	DEMOLITION OF EXISTING SANITARY SEWER PIPE (ALL SIZES)		400		
	25 26	P-150-4.4 P-150-4.5	DEMOLITION OF EXISTING SANITARY SEWER MANHOLE DEMOLITION OF EXISTING WATER PIPE (ALL SIZES)	EA LF	2 1,200		
+	20	P-150-4.5 P-150-4.6	REMOVE EXISTING FLUSH MOUNTED FIRE HYDRANT		3		
┥		P-150-4.6 P-150-4.7	DEMOLITION OF EXISTING METAL GUARDRAIL		150		
-	20	P-150-4.8	DEMOLITION OF EXISTING ABANDONED PASSENGER BOARDING BRIDGE FOUNDATIONS	EA	10		
		P-150-4.9	REMOVE EXISTING WATER CABINET	EA	5		
+			REMOVE EXISTING PASSENGER BOARDING BRIDGE CARGO LIFT	EA	3		
		P-150-4.10	REMOVE EXISTING LIGHT POLE AND FOUNDATION	EA	4		
	33	P-152-4.1	UNCLASSIFIED EXCAVATION	CY	2,500		
	34	P-152-4.2	UNDERCUT EXCAVATION	CY	200		
1		P-152-4.3	UNSUITABLE MATERIAL EXCAVATION AND DISPOSAL	CY	100		
ļ		P-152-4.4	BORROW EXCAVATION	CY	1,500		
1		P-152-4.5	SUBGRADE PREPARATION	SY	16,625		
╡		P-152-4.6		CY	100		
╡	39	P-152-4.7	LIME	TON	200		
┦	40	P-209-5.1	CRUSHED AGGREGATE BASE COURSE	CY	300		
┥	40	r -∠∪ฮ-ฮ.1			300		
┥	41	P-219-5.1	RECYCLED CONCRETE AGGREGATE FOR GENERAL USE	CY	100		
┥							
╏	42	P-220-6.1	CEMENT TREATED SOIL BASE COURSE	SY	16,625		
1	43	P-220-6.2	CEMENT	TON	850		
4							
	44	P-304-8.1	CEMENT-TREATED AGGREGATE BASE COURSE (8-INCH THICK)	SY	14,315		
	<u>۸ ۲</u>				0.040		
┥	45	P-306-8.1	LEAN CONCRETE BASE COURSE	SY	2,310		
+	46	S-102-6.1	POROUS BITUMINOUS BASE COURSE (4" THICK)	SY	14,315		
┨	40	0-102-0.1			17,010		
┨	47	P-501-8.1	PORTLAND CEMENT CONCRETE PAVEMENT (18 INCH THICK)	SY	16,625		
┫							
1	48	P-610-6.1	STEEL PIPE BOLLARDS	EA	10		
1		P-610-6.2	PROTECTIVE CONCRETE APRON	SF	1,120		
1		P-610-6.3	TYPE 2 CONCRETE LANDING WITH RAMPS	SF	200		
Ţ		P-610-6.4	TYPE 3 CONCRETE LANDING WITH RAMPS	SF	210		
1	52	P-610-6.5	PEDESTRIAN HANDRAIL	LF	50		
]	53	P-610-6.6	WALL-MOUNTED PEDESTRIAN HANDRAIL	LF	25		
1	54	P-610-6.7	ELEVATED PROTECTIVE CONCRETE APRON	SF	120		
~							
	55 56	P-620-5.1 P-620-5.2	PAVEMENT MARKINGS WITHOUT REFLECTIVE BEADS EXISTING PAINT MARKING REMOVAL	SF SF	1,500 4,000		

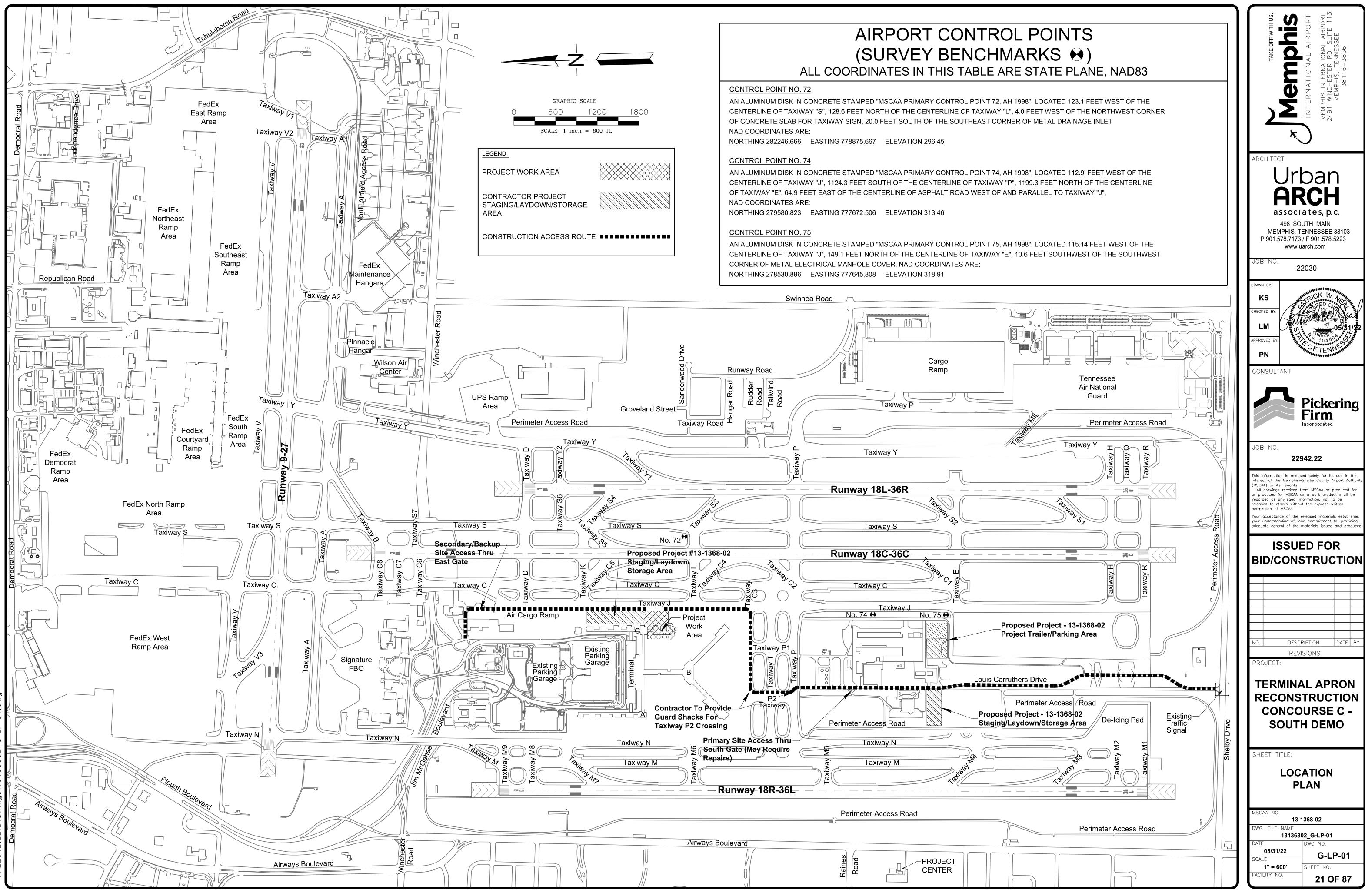
# SCHEDULE OF ESTIMATED QUANTITIES ITEM FAA SPEC DESCRIPTION ITEM FAA SPEC DESCRIPTION DRAINAGE ITEMS 57 D-701-5.1 18 INCH REINFORCED CONCRETE PIPE (CLASS V) 58 D-701-5.2 24 INCH REINFORCED CONCRETE PIPE (CLASS V) 59 D-701-5.3 CONCRETE FOR PIPE PLUGS AND COLLARS 60 D-705-5.1 4 INCH PERFORATED UNDERDRAIN PIPE 61 D-705-5.2 UNDERDRAIN CLEANOUT - CONCRETE 62 D-751-5.1 STORM DRAIN MANHOLE (5' DIAMETER, > 4' - 8' DEPTH) 63 D-751-5.3 STORM DRAIN MANHOLE (6' DIAMETER, > 4' - 8' DEPTH) 64 D-751-5.3 STORM DRAIN INLET (4'x4', > 4' - 8' DEPTH) 65 D-751-5.4 TRENCH DRAIN (ALL DEPTHS)

## FOOTNOTES (FN):

- 1 CONTINGENCY ITEM TO BE USED AS DIRECTED BY THE PROGRAM MANAGER.
- 2 OWNERSHIP OF ITEM AFTER REMOVAL TO REMAIN WITH MSCAA.
- 3 OWNERSHIP OF ITEM AFTER REMOVAL TO REMAIN WITH CONTRACTOR.
- 4 OWNERSHIP OF ITEM AFTER REMOVAL TO REMAIN WITH MLG&W.

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		ORIGINAL	FINAL	
	LF	152		
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	CY	10		
	LF	750		
	EA	6		
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	EA	1		
	EA	2		
	LF	340		

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## <u>GENERAL NOTES</u>

- MAY REQUIRE COORDINATION:

MSCAA PROJECT NO. 08-1260-01 14-1374-03

- AGREEMENTS AND ARRANGEMENTS.
- REGULATIONS, AND REQUIREMENTS.

- LIME, AND OTHER DEBRIS

IF EXCESSIVE DUST ACCUMULATES ON EXTERIOR BUILDING SURFACES, THE PROGRAM MANAGER MAY REQUIRE WASHING OF THE SURFACE. THE PROGRAM MANAGER RESERVES THE RIGHT TO HALT CONSTRUCTION OPERATIONS WHEN FUGITIVE DUST, CEMENT, LIME, OR OTHER AIRBORNE DEBRIS BECOME HAZARDOUS TO APRON AND/OR AIRFIELD OPERATIONS, CAUSE INDOOR AIR PROBLEMS, OR ACCUMULATE EXCESSIVELY ON COOLING TOWERS, AIR INTAKES, OR OTHER MECHANICAL SYSTEMS. ALL COSTS ASSOCIATED WITH CONTROL OF FUGITIVE DUST, CEMENT, LIME, OR OTHER AIRBORNE DEBRIS WILL BE THE CONTRACTOR'S RESPONSIBILITY.

CONTRACTOR'S EXPENSE.

1. COMPLY WITH ALL CITY OF MEMPHIS REQUIREMENTS FOR WEIGHT LIMITS, CLEANING, AND MAINTENANCE OF STREETS FOR ALL OPERATIONS ON SHELBY DRIVE, SWINNEA ROAD, WINCHESTER ROAD, LOUIS CARRUTHERS DRIVE, ACCESS DRIVES AND ROADS, AND OTHER CITY STREETS AND CONNECTIONS.

2. OTHER CONTRACTORS WILL BE WORKING WITHIN THE AIRPORT PROPERTY DURING THIS PROJECT. THE CONTRACTOR WILL COOPERATE WITH THE OWNER AND THE PROGRAM MANAGER IN COORDINATING SCHEDULES IN ORDER TO MINIMIZE CONFLICTS AND COMPLETE THE PROJECTS IN A TIMELY MANNER.

3. THE FOLLOWING IS A LISTING OF SOME, BUT NOT NECESSARILY ALL, MAJOR PROJECTS IN THE AREA THAT

## DESCRIPTION

GLYCOL MANAGEMENT PROGRAM CONCOURSE MODERNIZATION - PASSENGER BOARDING BRIDGES

4. ONLY THE SURVEY CONTROL POINTS SHOWN HEREIN MAY BE USED FOR CONSTRUCTION CONTROL. NOTIFY THE PROGRAM MANAGER IMMEDIATELY IF ANY CONTROL POINTS ARE DISTURBED OR MISSING. ALL CONTROL POINTS ARE TO BE PRESERVED THROUGHOUT THE PROJECT.

5. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE ACQUISITION OF WATER AS REQUIRED FOR DUST CONTROL, DEMOLITION, EARTHWORK, AND OTHER CONSTRUCTION OPERATIONS. ALL COSTS OF SAID ACQUISITION, INCLUDING PERMITS, MONTHLY BILLING, TEMPORARY PIPING, CONNECTIONS, HYDRANTS, METERS, AND DEMOLITION WILL BE THE CONTRACTOR'S RESPONSIBILITY. THE PROGRAM MANAGER WILL APPROVE ALL UTILITY

6. THE CONTRACTOR IS RESPONSIBLE FOR REMOVAL AND DISPOSAL OF ALL DEBRIS AND SPOILS FROM THE PROJECT WORK AREA TO A SITE OR SITES OFF OF AIRPORT PROPERTY UNLESS DIRECTED BY THE PROGRAM MANAGER TO A SITE ON AIRPORT PROPERTY. OFF AIRPORT SITES WILL COMPLY WITH ALL APPLICABLE CODES,

7. PROTECT TAXIWAYS, APRON PAVEMENTS, AND UNDERGROUND FACILITIES FROM DAMAGE CAUSED BY CONSTRUCTION EQUIPMENT. ANY DAMAGE TO PAVEMENTS OR FACILITIES CAUSED BY THE CONTRACTOR'S EQUIPMENT WILL BE REPAIRED OR REPLACED IN KIND BY THE CONTRACTOR AT NO COST TO THE OWNER.

8. SIGNIFICANT PONDING OF STORMWATER WILL NOT BE ALLOWED DURING CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES, INCLUDING ANY NECESSARY STORMWATER PUMPING FACILITIES. ALL COSTS ASSOCIATED WITH PROVISION AND MAINTENANCE OF POSITIVE DRAINAGE FACILITIES WILL BE THE CONTRATOR'S RESPONSIBILITY.

9. DUE TO THE NATURE AND SCOPE OF THE PROJECT, HEAVY CIVIL CONSTRUCTION WILL BE OCCURRING IN CLOSE PROXIMITY TO EXISTING AIRPORT CONCOURSE BUILDING FACILITIES. THE CONTRACTOR WILL TAKE NECESSARY PRECAUTIONS AND PROVIDE THE NECESSARY PROTECTIVE MEASURES TO PREVENT DAMAGE TO THE BUILDINGS, ANY DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED, AT NO ADDITIONAL CONTRACT COST, TO THE SATISFACTION OF THE PROGRAM MANAGER.

10. CONTROL OF FUGITIVE DUST, CEMENT, LIME, AND OTHER AIRBORNE DEBRIS FROM CONSTRUCTION OPERATIONS IS OF THE UTMOST IMPORTANCE IN THIS PROJECT. IT SHOULD BE NOTED THAT AREAS WITH SENSITIVITY TO FUGITIVE DUST EXIST ADJACENT TO THE PROJECT AREA, INCLUDING AIR INTAKES ALONG THE CONCOURSE BUILDINGS AS WELL AS COOLING TOWERS NORTH OF THE COURTYARDS BETWEEN CONCOURSES A AND B AS WELL AS B AND C. THE CONTRACTOR SHALL:

a) UTILIZE CONSTRUCTION EQUIPMENT, MEANS, AND METHODS TO MINIMIZE FUGITIVE DUST, CEMENT,

b) APPLY WATER AND/OR DUST PALLIATIVE PRODUCTS DURING EARTHWORK OPERATIONS

c) CONSIDER CEMENT APPLICATION VIA SLURRY OR OFF-SITE MIXING

d) CONSIDER LIME APPLICATION VIA SLURRY

e) MONITOR SENSITIVE AREAS FOR EXCESSIVE DEBRIS ACCUMULATION

11. THE REQUIRED SHELBY COUNTY CODE ENFORCEMENT PERMITS, INSPECTIONS, AND FEES ARE THE CONTRACTOR'S RESPONSIBILITY. THE COST OF ALL PERMITS, INSPECTIONS, AND FEES ARE AT THE

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## HAUL ROAD NOTES

- 1. HAUL ROADS TO BE USED UNDER THIS PROJECT SHALL BE THOSE INDICATED ON THE DRAWINGS OR OTHERWISE SPECIFICALLY AUTHORIZED BY THE PROGRAM MANAGER. IN GENERAL, THE CONTRACTOR SHALL CONFINE EQUIPMENT AND HAULING TO THE AREAS UNDER CONSTRUCTION.
- 2. THE ACCESS POINTS TO THE PROJECT SITE ARE SHOWN ON THE PLANS. THE SPECIFIED GATES SHALL BE MONITORED BY A MSCAA GUARD DURING ALL CONTRACTOR OPERATIONS WHILE THE GATE IS OPEN OR UNLOCKED. GATE GUARDS ARE PROVIDED AT NO COST TO THE CONTRACTOR.
- 3. HAULING OPERATIONS FOR THIS PROJECT WILL OCCUR ACROSS ACTIVE TAXIWAYS. AIRPORT OPERATIONS AND SAFETY WILL TAKE PRECEDENCE OVER HAULING OPERATIONS AT ALL TIMES AND THEREFORE SOME HAULING OPERATIONS MAY BE DELAYED. SAID DELAYS WILL NOT BE THE BASIS OF CLAIMS FOR ADDITIONAL COSTS OR CONTRACT TIME. THE NORMAL TIMES OF HEAVY AIR CARRIER AIRCRAFT MOVEMENTS ARE ESTIMATED FROM 7:00 AM TO 9:30 AM, 12:30 PM TO 2:30 PM, 6:30 PM TO 8:30 PM, AND 10:30 PM TO 5:00 AM. THESE MOVEMENT TIMES ARE SUBJECT TO CHANGE DUE TO LOCAL OR NATIONAL WEATHER CONDITIONS, HOLIDAYS, AND OTHER EVENTS THAT IMPACT AIR OPERATIONS AND ARE ONLY PROVIDED AS A CONVENIENCE TO THE CONTRACTOR.
- 4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. OFF-SITE HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE.
- 5. WHILE ON THE AIRPORT. ALL CONTRACTOR VEHICLES AND TRAFFIC SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION AREAS. STAGING AREAS OR HAUL ROUTES.
- 6. THE CONTRACTOR'S ACCESS ROUTE TO THE PROJECT SITE SHALL BE AS DIRECTED BY AIRPORT OPERATIONS. VISITORS OR ANY INDIVIDUALS WHO ARE NOT BADGED SHALL BE ESCORTED IN ACCORDANCE WITH AIRPORT SECURITY REQUIREMENTS AT ALL TIMES WHILE IN THE SIDA.
- 7. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE VARIOUS CONSTRUCTION AREAS ON THE SITE. THE CONTRACTOR IS RESPONSIBLE FOR THE IMMEDIATE CLEANUP OF ANY DEBRIS DEPOSITED AT THE PROJECT SITE AND ALONG ANY ROAD AS A RESULT OF HIS/HER CONSTRUCTION TRAFFIC. DIRECTIONAL SIGNAGE AT THE ACCESS GATE AND ALONG THE DELIVERY ROUTE TO THE STORAGE AREA OR WORK SITE SHALL BE APPROVED BY THE PROGRAM MANAGER.
- 8. THE CONTRACTOR, THROUGH THE CONSTRUCTION SECURITY OFFICER, SHALL ESTABLISH AND MAINTAIN A LIST OF CONTRACTOR AND SUB-CONTRACTOR VEHICLES AUTHORIZED TO OPERATE ON THE PROJECT SITE. VEHICLE USE PERMITS SHALL BE ASSIGNED IN ACCORDANCE WITH AIRPORT SECURITY PROCEDURES.
- 9. ALL VEHICLES USING HAUL ROUTES INCLUDING OFF-SITE ROUTES, SHALL BE COVERED TO PREVENT BLOWING AWAY OR SPILLAGE OF LOOSE MATERIAL, ALL SPILLAGES ON PUBLIC ROADWAYS AND ON-SITE ROADS SHALL BE PROMPTLY CLEANED UP AND LEGALLY DISPOSED OF AT NO ADDITIONAL COST TO THE OWNER.
- 10. NO CONTRACTOR VEHICLES ARE TO CROSS ACTIVE RUNWAYS, TAXIWAYS, OR NAVAID CRITICAL AREAS UNLESS ACCOMPANIED BY AN ESCORT UNDER THE DIRECT CONTROL OF THE AIRPORT GROUND CONTROLLER. IT SHALL BE UNDERSTOOD BY THE CONTRACTOR THAT AIRPORT TRAFFIC ON RUNWAYS, TAXIWAYS, AND APRONS SHALL HAVE PRIORITY OVER ALL CONSTRUCTION TRAFFIC.
- 11. THE CONTRACTOR IS ADVISED THAT OTHER CONTRACTORS MAY BE UTILIZING THE SAME HAUL ROAD AND PERFORMING WORK IN THE VICINITY OF THIS PROJECT.
- 12. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DESIGNATE AN EMPLOYEE WITH A PHOTO AIRPORT ID TO BE RESPONSIBLE FOR THE ESCORTING OF ALL VENDOR/SUPPLIERS REQUIRING ACCESS TO THE CONSTRUCTION SITE. THIS INDIVIDUAL WILL BE KNOWLEDGEABLE IN ALL APPLICABLE AIRPORT SECURITY, SAFETY, AOA DRIVING RULES AND REGULATIONS. THE NAME OF THIS INDIVIDUAL WILL BE PROVIDED TO THE AIRPORT SECURITY MANAGER, AIRSIDE MANAGER OF AIRPORT OPERATIONS AND PROGRAM MANAGER.
- 13. ALL CONTRACTOR VEHICLES SHALL DISPLAY IN FULL VIEW, COMPANY LOGOS, AFFIXED TO EACH SIDE OF THE VEHICLE. LOGOS SHALL BE NO LESS THAN 12" X 12" AND CAN BE MAGNETIC. PRINTED OR PAINTED ON. BUT MUST BE COMMERCIALLY MADE.
- 14. ALL CONTRACTOR VEHICLES OPERATING WITHIN THE ACTIVE AOA. RELATED SAFETY AREAS AND CONSTRUCTION LIMITS SHALL BE EQUIPPED WITH ELECTRONICALLY POWERED, AMBER COLOR, 360-DEGREE OMNI-DIRECTION LIGHT, MOUNTED ON THE VEHICLE SUCH THAT IT IS CONSPICUOUS FROM ANY DIRECTION.
- 15. ALL ACTIVE TAXIWAY CROSSINGS WILL BE APPROXIMATELY PERPENDICULAR TO THE TAXIWAY, AND WILL BE APPROVED IN ADVANCE BY THE PROGRAM MANAGER AND MSCAA OPERATIONS. ESTABLISH ALL ACTIVE AND INACTIVE TAXIWAY CROSSINGS TO MISS EXISTING EDGE LIGHTS.
- 16. ALL ACTIVE AND IN-ACTIVE TAXIWAY CROSSINGS AND WORK AREAS ADJACENT TO TAXIWAYS WILL BE KEPT BROOM CLEAN. A MINIMUM OF TWO (2) VACUUM SWEEPERS DEDICATED EXCLUSIVELY TO THIS PROJECT, WILL BE MAINTAINED ON CALL AT THE PROJECT SITE DURING ANY TYPE OR FORM OF HAULING OR CONSTRUCTION OPERATIONS. ALL ACTIVE TAXIWAY CROSSINGS WILL HAVE A DEDICATED VACUUM SWEEPER AND OPERATOR ON DUTY AT ALL TIMES DURING ANY CONSTRUCTION ACTIVITY.

## TEMPORARY GUARD HOUSE NOTES

- 1. CONTRACTOR SHALL PROVIDE A TEMPORARY CONTRACTOR-FURNISHED GUARD HOUSE AT ALL LOCATIONS WITHIN THE AOA SECURE AREA AS IDENTIFIED IN THE PLANS. THE GUARD HOUSE(S) SHALL BE EQUIPPED WITH HEATING, AIR CONDITIONING, ELECTRICITY AND INSIDE AND OUTSIDE LIGHTING, CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE GUARD HOUSE(S) FOR THE DURATION OF THE PROJECT. THE CONTRACTOR WILL PROVIDE A PORTABLE TOILET FACILITY AT EACH LOCATION FOR THE EXCLUSIVE USE OF THE GATE GUARD AT NO ADDITIONAL COST TO THE OWNER. TRASH SHALL BE REMOVED DAILY AND GUARD HOUSE CLEANED WEEKLY WITH NO DIRECT PAYMENT.
- 2. ALL EQUIPMENT SHALL BE SUBMITTED TO THE PROGRAM MANAGER FOR APPROVAL BY AIRPORT SECURITY PRIOR TO INSTALLATION AND OPERATION. CONTACT AIRPORT SECURITY (901) 922-8021 FOR DETAILED SPECIFICATIONS OF THE ABOVE EQUIPMENT.
- 3. THE CONTRACTOR SHALL INSTALL THE FOLLOWING SIGNS:
  - A. "STOP-WAIT UNTIL TRAFFIC IS CLEAR" SIGN MOUNTED AT THE INBOUND LANE, TWENTY-FIVE FEET (25') PRIOR TO THE GUARD HOUSE
  - B. "STOP" SIGNS ADJACENT TO THE GUARD HOUSE ON THE INBOUND AND OUTBOUND LANES. "STOP" SIGN MAY BE MOUNTED ON THE SAME POST AS THE "WAIT UNTIL TRAFFIC IS CLEAR" SIGN.
  - C. "WAIT UNTIL CLEARANCE IS GIVEN BEFORE PROCEEDING NO TAILGATING" SIGN ON BOTH THE INBOUND AND OUTBOUND LANES
  - D. "RESTRICTED AREA NO PUBLIC ACCESS" SIGN SHALL BE MOUNTED ON THE TOP FRONT OF THE GUARD HOUSE FACING THE PUBLIC SIDE. THE SIGN SHALL BE THE SAME SIZE AND DESIGN AS OTHER GUARD HOUSE SIGNAGE.
- 4. ALL PERSONNEL ALLOWED PAST THE GUARD HOUSE SHALL BE PROPERLY BADGED BY THE OWNER'S SECURITY DEPARTMENT FOR ACCESS ONTO THE AOA AREA UNLESS ESCORTED PER MSCAA REQUIREMENTS. ACCESS WILL BE DENIED IF NOT PROPERLY BADGED.

## CONTRACTOR STAGING AREA NOTES

- 1. THE APPROXIMATE AREA FOR STAGING/LAYDOWN/STORAGE REQUIRED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION ARE SHOWN ON THE PLANS. THE EXACT LIMITS WILL BE ESTABLISHED BY THE PROGRAM MANAGER. THIS AREA SHALL INCLUDE A JOB TRAILER, PROJECT RELATED STORAGE, STOCKPILES, AND EMPLOYEE AND EQUIPMENT PARKING. THE CONTRACTOR IS RESPONSIBLE FOR OVERALL SECURITY OF THE STAGING AREA, BUT MUST ALLOW CONTRACTORS ON OTHER PROJECTS TO PASS THROUGH ON THE EXISTING GRAVEL ROADS.
- 2. ALL STAGING/LAYDOWN/STORAGE AREA ARRANGEMENTS, DESIGNS, AND FEATURES WILL BE COORDINATED WITH AND APPROVED BY THE PROGRAM MANAGER. THE CONTRACTOR IS RESPONSIBLE FOR ALL FEES, ARRANGEMENTS, APPROVALS, AND PERMITS FOR THE STAGING/LAYDOWN/STORAGE AREAS.
- 3. THE CONTRACTOR SHALL NOT PARK EQUIPMENT OR STORE MATERIALS WITHIN 10 FEET OF AOA FENCE.
- 4. THE CONTRACTOR SHALL MAINTAIN EXISTING DRAINAGE PATTERNS AT THE STAGING AND STOCKPILE AREAS AND PROVIDE TEMPORARY ROUTING OF STORMWATER AROUND THE AREAS.
- 5. IN ORDER TO PREVENT SEDIMENT FROM LEAVING THE CONTRACTOR STAGING AREA, THE CONTRACTOR SHALL INSTALL TEMPORARY SILT FENCE AROUND THE STAGING AREA (SEE SHEET C-GD-10 FOR DETAIL).
- 6. THE CONTRACTOR SHALL PROVIDE TEMPORARY UTILITIES TO THE SITE IN ACCORDANCE WITH SECTION 01500. ALL COSTS ASSOCIATED WITH TEMPORARY UTILITIES WILL BE THE CONTRACTOR'S RESPONSIBILITY.
- 7. THE CONTRACTOR SHALL SUPPLY TRASH AND RUBBISH DUMPSTERS AND ALL OTHER CONTAINERS FOR REMOVAL OF TRASH, RUBBISH, AND DEBRIS RESULTING FROM THE WORK OF THE CONTRACT. DUMPSTERS SHALL BE EMPTIED AT LEAST ONCE A WEEK OR MORE FREQUENTLY IF NECESSARY. THE CONTRACTOR AT ANY TIME SHALL NOT ALLOW DUMPSTERS TO OVERFLOW.
- 8. ALL COSTS ASSOCIATED WITH EXISTING FENCE DEMOLITION, ADJUSTMENT, RELOCATION, OR REPLACEMENT FOR THE STAGING/LAYDOWN/STORAGE AREA WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 9. ANY REQUIRED GRADING, SURFACING, AND LIGHTING OF THE STAGING/LAYDOWN/STORAGE AREA WILL BE THE RESPONSIBILITY OF THE CONTRACTOR. DUE TO NIGHTTIME AIRFIELD OPERATIONS, NIGHT LIGHTING MAY BE SUBJECT TO CERTAIN RESTRICTIONS. ALL COSTS ASSOCIATED WITH CLEARING, GRADING, SURFACING, AND LIGHTING OF THE LAYDOWN/STAGING AREA WILL BE THE CONTRACTOR'S RESPONSIBILITY.
- 10. EXISTING POLE MOUNTED TRANSFORMERS FOR POWER ARE AVAILABLE ON-SITE FOR THE CONTRACTOR TO USE. CONTACT MLGW FOR CONNECTION AND BILLING ARRANGEMENTS. ALL COSTS ASSOCIATED WITH UTILITY CONNECTIONS. EXTENSIONS. PERMITS. AND MONTHLY BILLING WILL BE THE CONTRACTOR'S RESPONSIBILITY. AT THE END OF THE PROJECT, THE CONTRACTOR IS RESPONSIBLE FOR RETURNING THE STAGING AREA TO ITS PRE-CONSTRUCTION CONDITION.
- 11. WATER, TELEPHONE AND GAS ARE AVAILABLE NEARBY AT LOUIS CARRUTHERS DRIVE. CONTACT MLGW AND/OR AT&T FOR CONNECTION AND BILLING ARRANGEMENTS. ALL COSTS ASSOCIATED WITH UTILITY CONNECTIONS. EXTENSIONS. PERMITS. AND MONTHLY BILLING WILL BE THE CONTRACTOR'S RESPONSIBILITY.
- 12. THE STAGING/LAYDOWN/STORAGE AREA WILL BE MAINTAINED IN A NEAT AND ORDERLY CONDITION THROUGHOUT THE LIFE OF THE PROJECT. THE GRASS SHALL BE CUT BI-WEEKLY, INCLUDING UNDER ANY SURROUNDING FENCES. AFTER PROJECT COMPLETION, THE AREA WILL BE DEMOLISHED, GRADED TO DRAIN, AND SEEDED AND MULCHED AS DIRECTED BY THE PROGRAM MANAGER. ALL COSTS ASSOCIATED WITH MAINTENANCE. STORAGE. AND DEMOLITION OF THE AREA WILL BE THE CONTRACTOR'S RESPONSIBILITY.
- 13. ALL DEMOLITION ITEMS NOT NOTED OTHERWISE SHALL BECOME PROPERTY OF THE CONTRACTOR AND BE REMOVED FROM AIRPORT PREMISES.
- 14. THE CONTRACTOR SHALL COMPLETELY CLEAN UP AND RESTORE THE ENTIRE STAGING AND STORAGE AREAS, AS APPROVED BY THE PROGRAM MANAGER WITHIN 15 DAYS OF FINAL ACCEPTANCE OF THE PROJECT.

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## OPERATIONAL NOTES

- 1. ALL OF THE CONTRACTOR'S MOVEMENTS AND ACTIVITIES WITHIN THE AIRPORT PROPERTY SHALL BE CLOSELY COORDINATED WITH THE PROGRAM MANAGER AND SHALL BE IN COMPLIANCE WITH THE PROVISIONS OF THE CONTRACT DOCUMENTS AND FAA ADVISORY CIRCULAR 150/5370-2F. "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION". NO WORK WILL BE ALLOWED WITHIN TAXIWAY OBJECT FREE AREAS, WHICH ARE DESIGNATED AS AREAS, WITHIN 129.5' OF CENTERLINE OF ANY AIRPLANE DESIGN GROUP (ADG) IV TAXIWAY AND WITHIN 160' OF CENTERLINE OF ANY AIRPLANE DESIGN GROUP (ADG) V TAXIWAY, UNLESS FULLY COORDINATED WITH THE PROGRAM MANAGER AND/OR UNLESS THE AFFECTED FACILITY OR PAVEMENT HAS BEEN CLOSED TO AIRCRAFT. UNDER NO CIRCUMSTANCES WILL THE CONTRACTOR BE ALLOWED TO CROSS ANY ACTIVE RUNWAY OR TAXIWAY OR AIRCRAFT MOVEMENT AREA OR OTHERWISE BE ALLOWED TO OPERATE WITHIN RUNWAY OR TAXIWAY SAFETY AREAS UNLESS THE PROGRAM MANAGER DETERMINES THAT THE WORK REQUIRES THE MOVEMENT. THIS WORK SHALL BE APPROVED BY THE PROGRAM MANAGER AND A NOTAM ISSUED.
- 2. SPECIAL ACCESS REQUIREMENTS AND OPERATING LIMITATIONS ARE REQUIRED INSIDE THE SECURITY FENCE. THE CONTRACTOR SHALL DELINEATE WORK LIMITS WITHIN THESE AREAS AS PER PHASING PLAN. CONFINE MEN, EQUIPMENT AND MATERIALS OUTSIDE OF OBJECT FREE AREA (OFA) WHEN A TAXIWAY IS ACTIVE. SEE THE SPECIFICATIONS FOR SPECIAL CONDITIONS AND FOR OTHER CONDITIONS RELATING TO SAFETY.
- 3. THE CONTRACTOR SHALL HAVE ACCESS TO THE AIRPORT ONLY AT THE LOCATIONS DESIGNATED ON THE PLANS. ALL OTHER ACCESS SHALL BE BY SPECIAL REQUEST AND SUBJECT TO APPROVAL BY THE OWNER. THE CONTRACTOR SHALL PROVIDE (CONTRACTOR FURNISHED) SECURITY GUARD HOUSES EQUIPPED WITH POWER, AIR CONDITIONING AND HEAT FOR SECURITY PERSONNEL. THE CONTRACTOR SHALL COORDINATE WITH MSCAA SECURITY TO PROVIDE SECURITY PERSONNEL TO CONTROL MOVEMENTS THROUGH THE CONTRACTOR'S ACCESS GATE UNLESS THE GATE REMAINS LOCKED. GATE GUARDS WILL BE PROVIDED BY MSCAA, THE CONTRACTOR WILL PROVIDE A PORTABLE TOILET FACILITY FOR THE EXCLUSIVE USE OF THE GATE GUARD AT NO ADDITIONAL COST TO THE OWNER.
- 4. RUNWAY, TAXIWAYS, APRON PAVEMENTS, NAVAIDS AND UNDERGROUND FACILITIES (TO REMAIN) MUST BE PROTECTED FROM DAMAGE CAUSED BY THE CONTRACTOR'S EQUIPMENT. ANY DAMAGE TO PAVEMENTS OR FACILITIES CAUSED BY THE CONTRACTOR'S PERSONNEL OR EQUIPMENT SHALL BE REPAIRED IN A MANNER APPROVED BY THE PROGRAM MANAGER BY THE CONTRACTOR AT NO COST TO THE OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND IDENTIFICATION OF ALL EXISTING UTILITIES AND UNDERGROUND PIPELINES IN THE CONSTRUCTION AREA. ANY DAMAGES TO EXISTING UTILITIES OR UNDERGROUND PIPELINES ON OR OFF AIRPORT PROPERTY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ALL REPAIR WORK SHALL MEET THE APPROVAL OF THE OWNER OF THE DAMAGED UTILITY. NO REIMBURSEMENT WILL BE ALLOWED FOR UTILITY/PIPE REPAIR OR REPLACEMENT.
- 5. AIRCRAFT OPERATIONS SHALL HAVE PRIORITY OVER ALL VEHICLES, EQUIPMENT AND PERSONNEL AT ALL TIMES. THE CONTRACTOR SHALL EMPLOY STRICT MEASURES TO PREVENT ANY CONFLICT BETWEEN HIS/HER PERSONNEL AND AIRCRAFT ON ANY ACTIVE AIRFIELD PAVEMENT. THE CONTRACTOR AND SUB-CONTRACTORS SHALL REMAIN CLEAR OF ACTIVE TAXIWAYS AND TAXILANES.
- 6. IN ORDER FOR THE CONTRACTOR TO OPERATE WITHIN AIRPORT PROPERTY, APPROPRIATE NOTICES TO AIRMEN (NOTAMS) MUST BE ISSUED BY THE OWNER THROUGH THE FAA FLIGHT SERVICE STATION. THESE NOTICES PROVIDE INFORMATION ON CLOSED, LIMITED, OR HAZARDOUS CONDITIONS TO AIRMEN AND USERS OF THE AIRPORT. A 48-HOUR NOTICE IS REQUIRED FOR ISSUANCE OF THE PROPER NOTAM. ALL CONSTRUCTION OPERATIONS MUST BE CLOSELY COORDINATED WITH THE PROGRAM MANAGER FOR NOTAM ISSUANCE.
- 7. CONTRACTOR SHALL KEEP CONSTRUCTION EQUIPMENT BELOW PART 77 SURFACES AT ALL TIMES. IF THE PROJECT REQUIRES THE CONTRACTOR TO PENETRATE ANY PART 77 SURFACE WITH CONSTRUCTION EQUIPMENT DURING CONSTRUCTION THE CONTRACTOR MUST NOTIFY THE PROGRAM MANAGER AND/OR THE OWNER AND A NOTAM MUST BE ISSUED BEFORE ANY PENETRATION OCCURS.
- 8. THIS PROJECT INVOLVES WORK WITH IN THE SECURITY IDENTIFICATION DISPLAY AREA (SIDA) AND AIRCRAFT OPERATIONS AREA (AOA). ALL OF THE CONTRACTOR'S MOVEMENTS AND CONSTRUCTION ACTIVITIES WITHIN THESE AREAS WILL BE CLOSELY COORDINATED AND APPROVED BY THE MSCAA AND THE PROGRAM MANAGER AS STATED IN THE AIRCRAFT DOCUMENTS.
- 9. ALL OF THE CONTRACTOR'S MOVEMENTS AND ACTIVITIES WITHIN THE AIRPORT PROPERTY WILL BE CLOSELY COORDINATED WITH. AND APPROVED BY. THE PROGRAM MANAGER AND WILL BE IN COMPLIANCE WITH THE PROVISIONS STATED IN THE CONTRACT DOCUMENTS AND FAA ADVISORY CIRCULAR 150/5370-2F, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- 10. THE OWNER AND/OR PROGRAM MANAGER RESERVES THE RIGHT TO PROHIBIT THE CONTRACTOR FROM WORKING DURING PERIODS OF INSTRUMENT FLIGHT RULES (IFR) WEATHER CONDITIONS IN ORDER TO MAINTAIN SAFETY FOR AIRCRAFT OPERATIONS. WHEN AS DETERMINED BY THE AIR TRAFFIC CONTROL TOWER. THE RUNWAY VISUAL RANGE (RVR) IS 1200 FEET OR LESS, WORK INCLUDING HAULING MAY NOT PROCEED UNTIL APPROVED BY THE PROGRAM MANAGER. WEATHER RECORDS INDICATE APPROXIMATELY FIVE (5) LESS-THAN-1200 RVR EVENTS PER YEAR, WITH A DURATION OF ABOUT 2 HOURS EACH. SUCH WORK PROHIBITION WILL NOT BE THE BASIS OF CLAIMS FOR ADDITIONAL CONTRACT TIME OR COSTS, BUT WILL BE COUNTED AS WEATHER TIME.
- 11. CERTAIN PHASES OF THIS PROJECT REQUIRE WORK INSIDE THE TAXIWAY OBJECT FREE AREA. THE FOLLOWING CONSTRAINTS APPLY TO WORK IN THESE AREAS:
  - a) THE WORK MUST BE SCHEDULED IN ADVANCE WITH THE PROGRAM MANAGER FOR NOTAM ISSUANCE AND SECURITY COORDINATION.
  - b) THE OWNER WILL PROVIDE A SECURITY GUARD TO PREVENT UNWANTED PERSONNEL FROM ENTERING THE AOA.
  - c) ALL ADJACENT ACTIVE PAVEMENTS WILL BE VACUUM CLEANED, AND ALL TURF AREAS CLEARED OF TRASH AND DEBRIS. A PAVEMENT INSPECTION BY MSCAA PERSONNEL WILL BE REQUIRED PRIOR TO REOPENING THE TAXIWAY.
- 12. THE OWNER WILL PROVIDE FLAGMEN, ESCORT PERSONNEL AND VEHICLES, SAFETY GATE OPERATORS, AND PERSONNEL TO MAN GUARDHOUSES TO COORDINATE AND CONTROL CONSTRUCTION TRAFFIC WHEN OPERATING WITHIN THE AOA. IF FLAGMEN ARE REQUIRED DUE TO THE CONTRACTOR'S OPERATIONS. THEN THE CONTRACTOR WILL FURNISH AND PAY FOR SAID FLAGMEN.
- 13. REFER TO PROJECT SPECIFICATIONS FOR LIGHTING REQUIREMENTS OF NIGHT CONSTRUCTION OPERATIONS. THE TYPE OF LIGHTING WILL BE SUCH THAT EXCESSIVE GLARE IS NOT PRODUCED BETWEEN THE CONTROL TOWER AND THE AIRFIELD AREA UNDER AIR TRAFFIC TOWER CONTROL. ALL LIGHTING OF CONSTRUCTION OPERATIONS AND THE STAGING/LAYDOWN/STORAGE AREA IS SUBJECT TO PROGRAM MANAGER APPROVAL, LIGHTING RESTRICTIONS WILL NOT BE THE BASIS OF CLAIMS FOR ADDITIONAL COSTS OR CONTRACT TIME.

- 14. PERFORM CONSTRUCTION OPERATIONS AS NECESSARY TO MINIMIZE ATTRACTION TO BIRDS CAUSED BY PONDED WATER, GRASS SEED. TRASH AND OTHER CONSTRUCTION FEATURES.
- 15. DUE TO EXCESSIVE NOISE. THE PROGRAM MANAGER MAY RESTRICT THE TIMES THAT CERTAIN DEMOLITION OR OTHER CONSTRUCTION OPERATIONS CAN OCCUR. SUCH RESTRICTIONS MAY NOT BE THE BASIS OF CLAIMS FOR ADDITIONAL COSTS OR CONTRACT TIME.

## BARRICADE NOTES

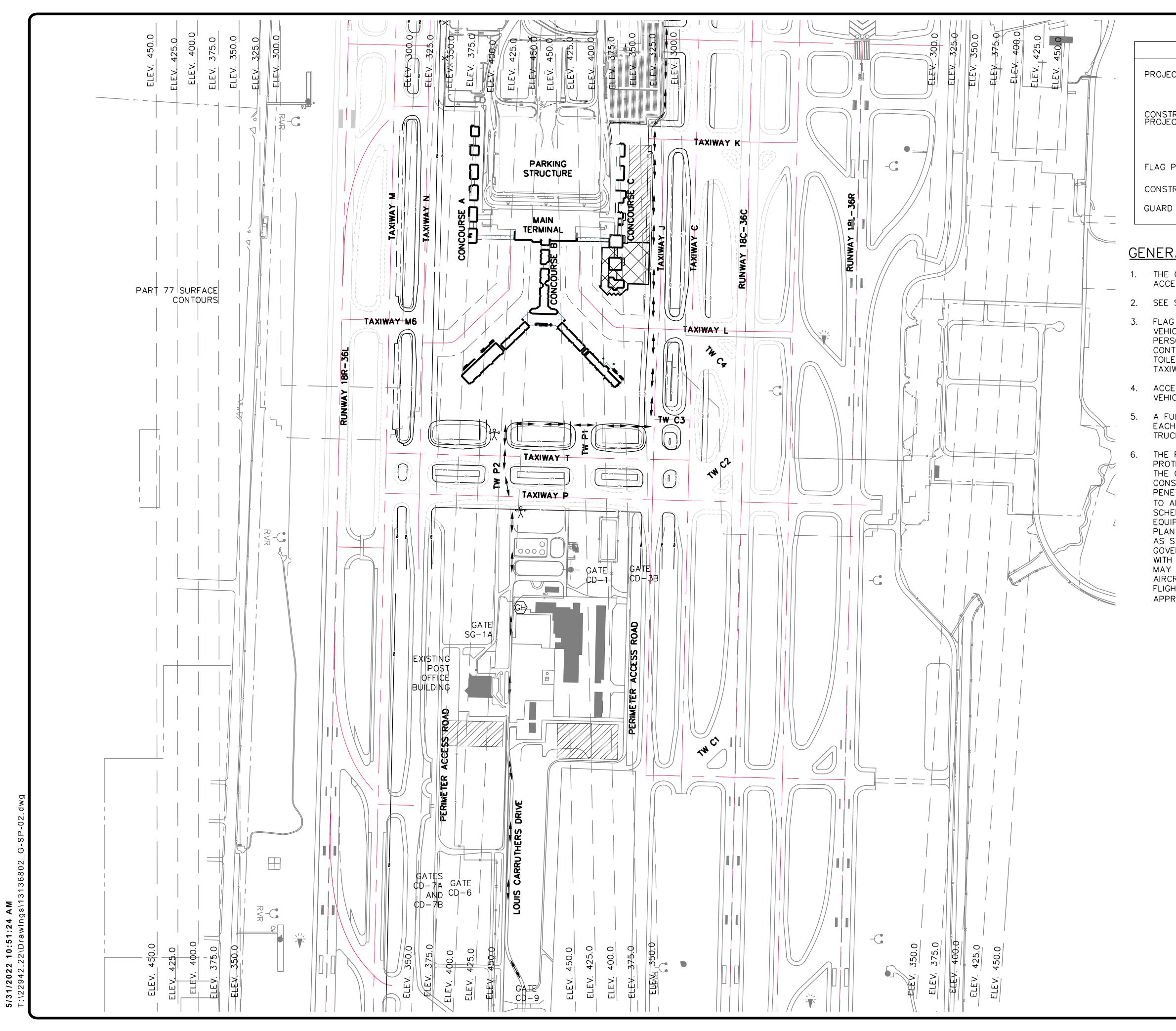
- 1. BARRICADES SHALL BE PLACED AS SHOWN ON THE OPERATIONAL PLAN SHEETS G-SP-03 AND G-SP-04 TO DELINEATE THE CONTRACTORS WORK AREAS.
- 2. BARRICADES SHALL BE AS SHOWN ON SHEETS G-SP-03 AND G-SP-04. ALL INCIDENTAL CONNECTORS, SPACERS, SPLICE PLATES, ETC.. SHALL BE PAINTED WHITE,
- 3. THE CONTRACTOR SHALL MAINTAIN THE LIGHTS AND BARRICADES IN AN OPERABLE CONDITION FOR THE DURATION OF THE PROJECT.
- 4. ALL BARRICADES SHALL BE CHECKED VISUALLY FOR SIGNS OF WEAR AND TEAR ON A WEEKLY BASIS AND SHALL BE REPAINTED AND/OR REPLACED WHEN DEEMED APPROPRIATE BY THE PROGRAM MANAGER. THE CONDITION OF LIGHTING UNITS SHALL BE CHECKED DAILY. ALL LIGHT FIXTURES SHALL BE VERIFIED OPERATING BY THE CONTRACTOR ON A DAILY BASIS BEFORE THE CONTRACTOR CEASES OPERATION FOR THE DAY. THE AREAS AROUND ALL BARRICADES SHALL BE CLEANED AT LEAST ONCE EACH WEEK AND THE CONTRACTOR SHALL SWEEP UP ACCUMULATED DEBRIS AND REMOVE IT FROM THE SITE, ALL ACTIVITIES CONDUCTED ADJACENT TO ACTIVE RUNWAYS OR TAXIWAYS SHALL BE COORDINATED WITH THE PROGRAM MANAGER.
- 5. ALTERNATE FORMS OF BARRICADES MAY BE PROPOSED BY THE CONTRACTOR WHICH MEET THESE FUNCTIONAL REQUIREMENTS. APPROVALS OF ANY SUCH SUBSTITUTION (IF GRANTED) SHALL BE BY THE AIRPORT AUTHORITY'S OPERATIONS DEPARTMENT AND THE PROGRAM MANAGER.
- 6. THE FINAL LOCATION FOR THE BARRICADES SHALL BE ESTABLISHED IN THE FIELD WITH CONCURRENCE FROM THE PROGRAM MANAGER AND AIRPORT OPERATIONS.
- 7. ALL LOW PROFILE BARRICADES (TYPE 1) SHALL REMAIN THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED AFTER COMPLETION OF THE PROJECT.
- 8. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PROPER POSITIONING OF ALL BARRICADES.
- 9. ALL COSTS ASSOCIATED WITH FURNISHING, PLACEMENT, MAINTENANCE, AND SUBSEQUENT RELOCATION OF THE LOW PROFILE BARRICADES ARE THE CONTRACTOR'S RESPONSIBILITY.
- 10. THE CONTRACTOR WILL PLACE LOW PROFILE SAFETY BARRICADES WITH LIGHTS ALONG TAXIWAY EDGES WHENEVER OPEN EXCAVATIONS OR IRREGULAR GRADES ARE LEFT WITHIN THE SAFETY AREA OF AN ACTIVE TAXIWAY, OR WHERE TEMPORARY PAVEMENT CLOSURES OR AIRCRAFT LIMITATIONS ARE REQUIRED. BARRICADES WILL BE SPACED NOT MORE THAN 10' O.C. OR CLOSER AS INDICATED ON THE PLANS ALONG THE AFFECTED PAVEMENT EDGE OR ACROSS A CLOSED TAXIWAY.

## REOPENING OF A CLOSED SURFACE

- 1. PRIOR TO THE COMPLETION OF ANY PHASE, ALL TAXIWAY AND APRON SURFACES TO BE REOPENED AT THE COMPLETION OF THE PHASE SHALL BE SWEPT AND VACUUMED CLEAN. ALL WORK SHALL BE COMPLETE AND ACCEPTED BY THE PROGRAM MANAGER PRIOR TO REOPENNING OF AIRCRAFT TRAFFIC.
- 2. PRIOR TO THE REMOVAL OF ANY BARRICADE OR LIGHTED CONE AND OPENING PAVEMENT TO AIRCRAFT, THE CONTRACTOR SHALL REQUEST, THROUGH THE PROGRAM MANAGER. AN INSPECTION BY AIRPORT OPERATIONS. A FOUR HOUR NOTICE MUST BE GIVEN FOR THE INSPECTION. NO TAXIWAY OR APRON THAT HAS BEEN CLOSED FOR WORK MAY BE REOPENED UNTIL AIRPORT OPERATIONS HAS PERFORMED THE INSPECTION AND APPROVED THE REOPENING. SURFACES BELOW AND AROUND THE BARRICADES MUST BE CLEANED PRIOR TO REQUESTING THE INSPECTION.
- 3. ONCE THE INSPECTION IS COMPLETE AND AIRPORT OPERATIONS HAS APPROVED REOPENING OF THE FACILITY, BARRICADES MUST BE QUICKLY REMOVED AND/OR RELOCATED. AFTER THE BARRICADES HAVE BEEN MOVED OR RELOCATED AND THE CONTRACTOR HAS VACATED THE AREA, AIRPORT OPERATIONS WILL REOPEN THE FACILITY.
- 4 PRIOR TO OPENING FOR USE. THE CONTRACTOR WILL HAVE READY FOR OPERATIONS AND ALLOW ADEQUATE TIME FOR INSPECTION BY AIRPORT OPERATIONS ANY PAVEMENT OR APRON THAT HAS BEEN CLOSED FOR WORK, OR THAT HAS BEEN USED FOR A CROSSING POINT OR HAUL ROUTE BY THE CONTRACTOR. NO BARRICADES SHALL BE REMOVED UNTIL THE INSPECTION BY AIRPORT OPERATIONS IS COMPLETE.

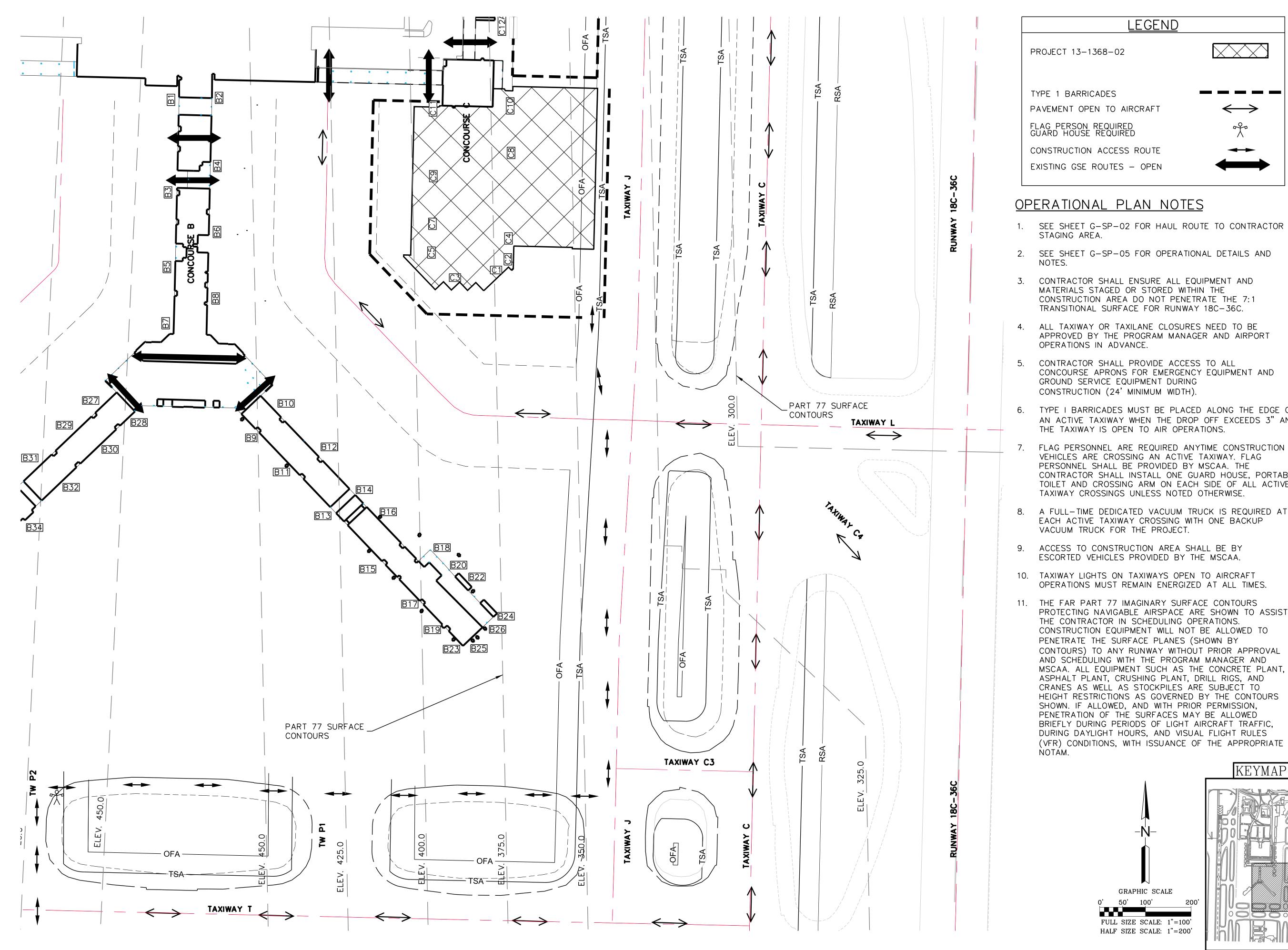
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MEI	ARCHITECT Urban associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com					
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<b>22942.22</b> This information is released solely for its use in the interest of the Memphis–Shelby County Airport Authority (MSCAA) or its Tenants. All drawings received from MSCAA or produced for or produced for MSCAA as a work product shall be regarded as privileged information, not to be released to others without the express written permission of MSCAA. Your acceptance of the released materials establishes your understanding of, and commitment to, providing						
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## LEGEND

PAVEMENT OPEN TO AIRCRAFT

CONSTRUCTION ACCESS ROUTE EXISTING GSE ROUTES - OPEN

## OPERATIONAL PLAN NOTES

1. SEE SHEET G-SP-02 FOR HAUL ROUTE TO CONTRACTOR

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2. SEE SHEET G-SP-05 FOR OPERATIONAL DETAILS AND

3. CONTRACTOR SHALL ENSURE ALL EQUIPMENT AND MATERIALS STAGED OR STORED WITHIN THE CONSTRUCTION AREA DO NOT PENETRATE THE 7:1 TRANSITIONAL SURFACE FOR RUNWAY 18C-36C.

4. ALL TAXIWAY OR TAXILANE CLOSURES NEED TO BE APPROVED BY THE PROGRAM MANAGER AND AIRPORT OPERATIONS IN ADVANCE.

5. CONTRACTOR SHALL PROVIDE ACCESS TO ALL CONCOURSE APRONS FOR EMERGENCY EQUIPMENT AND GROUND SERVICE EQUIPMENT DURING CONSTRUCTION (24' MINIMUM WIDTH).

TYPE I BARRICADES MUST BE PLACED ALONG THE EDGE OF AN ACTIVE TAXIWAY WHEN THE DROP OFF EXCEEDS 3" AND THE TAXIWAY IS OPEN TO AIR OPERATIONS.

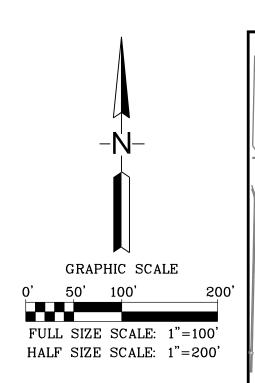
FLAG PERSONNEL ARE REQUIRED ANYTIME CONSTRUCTION VEHICLES ARE CROSSING AN ACTIVE TAXIWAY. FLAG PERSONNEL SHALL BE PROVIDED BY MSCAA. THE CONTRACTOR SHALL INSTALL ONE GUARD HOUSE, PORTABLE TOILET AND CROSSING ARM ON EACH SIDE OF ALL ACTIVE TAXIWAY CROSSINGS UNLESS NOTED OTHERWISE.

8. A FULL-TIME DEDICATED VACUUM TRUCK IS REQUIRED AT EACH ACTIVE TAXIWAY CROSSING WITH ONE BACKUP VACUUM TRUCK FOR THE PROJECT.

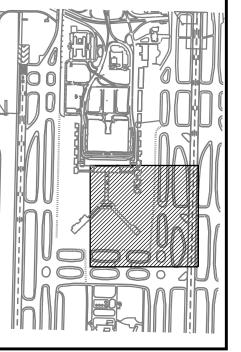
9. ACCESS TO CONSTRUCTION AREA SHALL BE BY ESCORTED VEHICLES PROVIDED BY THE MSCAA.

10. TAXIWAY LIGHTS ON TAXIWAYS OPEN TO AIRCRAFT OPERATIONS MUST REMAIN ENERGIZED AT ALL TIMES.

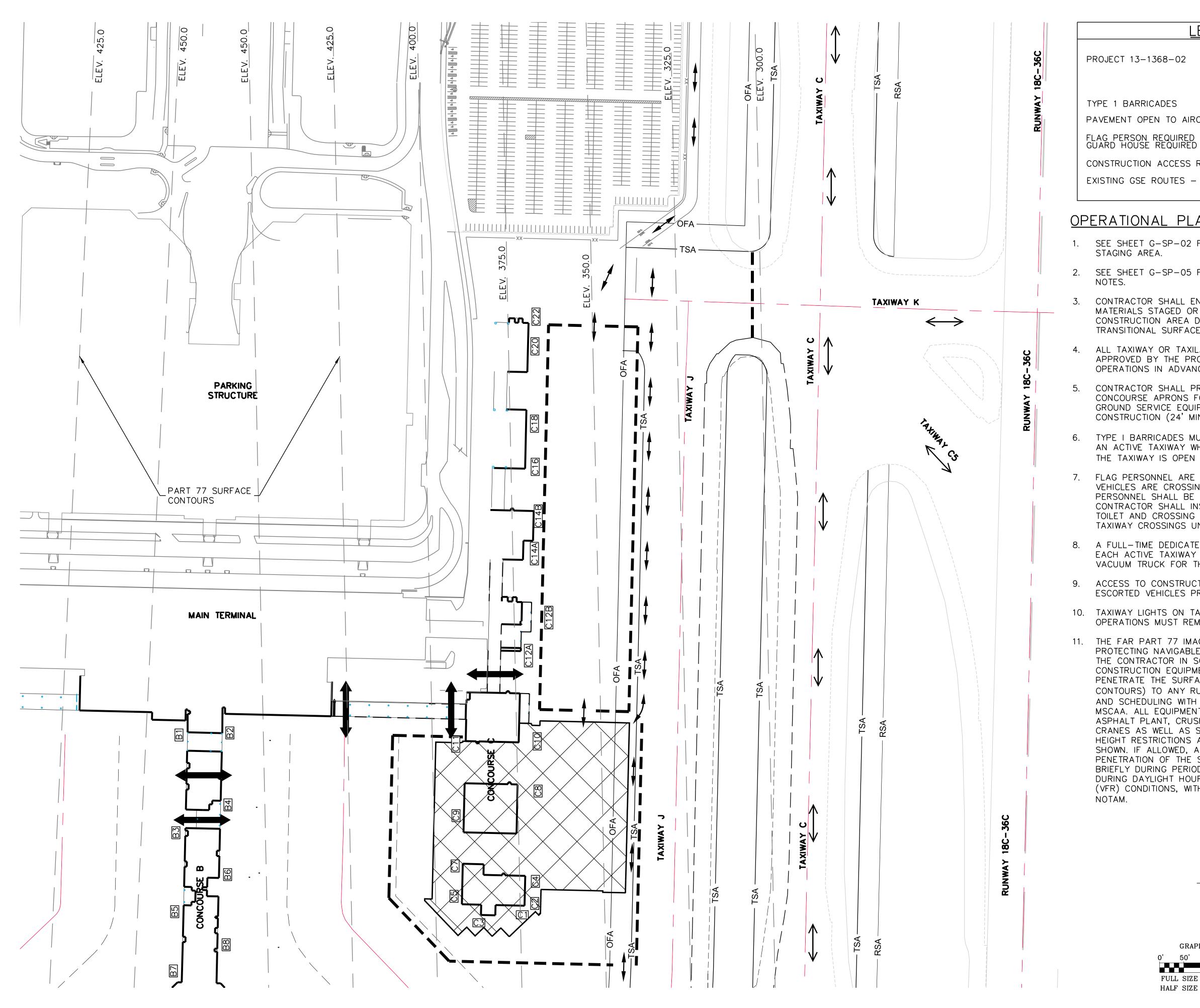
11. THE FAR PART 77 IMAGINARY SURFACE CONTOURS PROTECTING NAVIGABLE AIRSPACE ARE SHOWN TO ASSIST THE CONTRACTOR IN SCHEDULING OPERATIONS. CONSTRUCTION EQUIPMENT WILL NOT BE ALLOWED TO PENETRATE THE SURFACE PLANES (SHOWN BY CONTOURS) TO ANY RUNWAY WITHOUT PRIOR APPROVAL AND SCHEDULING WITH THE PROGRAM MANAGER AND MSCAA. ALL EQUIPMENT SUCH AS THE CONCRETE PLANT, ASPHALT PLANT, CRUSHING PLANT, DRILL RIGS, AND CRANES AS WELL AS STOCKPILES ARE SUBJECT TO HEIGHT RESTRICTIONS AS GOVERNED BY THE CONTOURS SHOWN. IF ALLOWED, AND WITH PRIOR PERMISSION, PENETRATION OF THE SURFACES MAY BE ALLOWED BRIEFLY DURING PERIODS OF LIGHT AIRCRAFT TRAFFIC, DURING DAYLIGHT HOURS, AND VISUAL FLIGHT RULES



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## **LEGEND**

PAVEMENT OPEN TO AIRCRAFT

CONSTRUCTION ACCESS ROUTE EXISTING GSE ROUTES - OPEN

## OPERATIONAL PLAN NOTES

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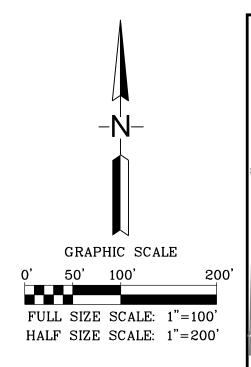
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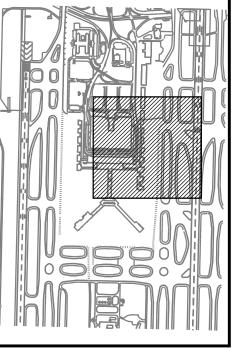
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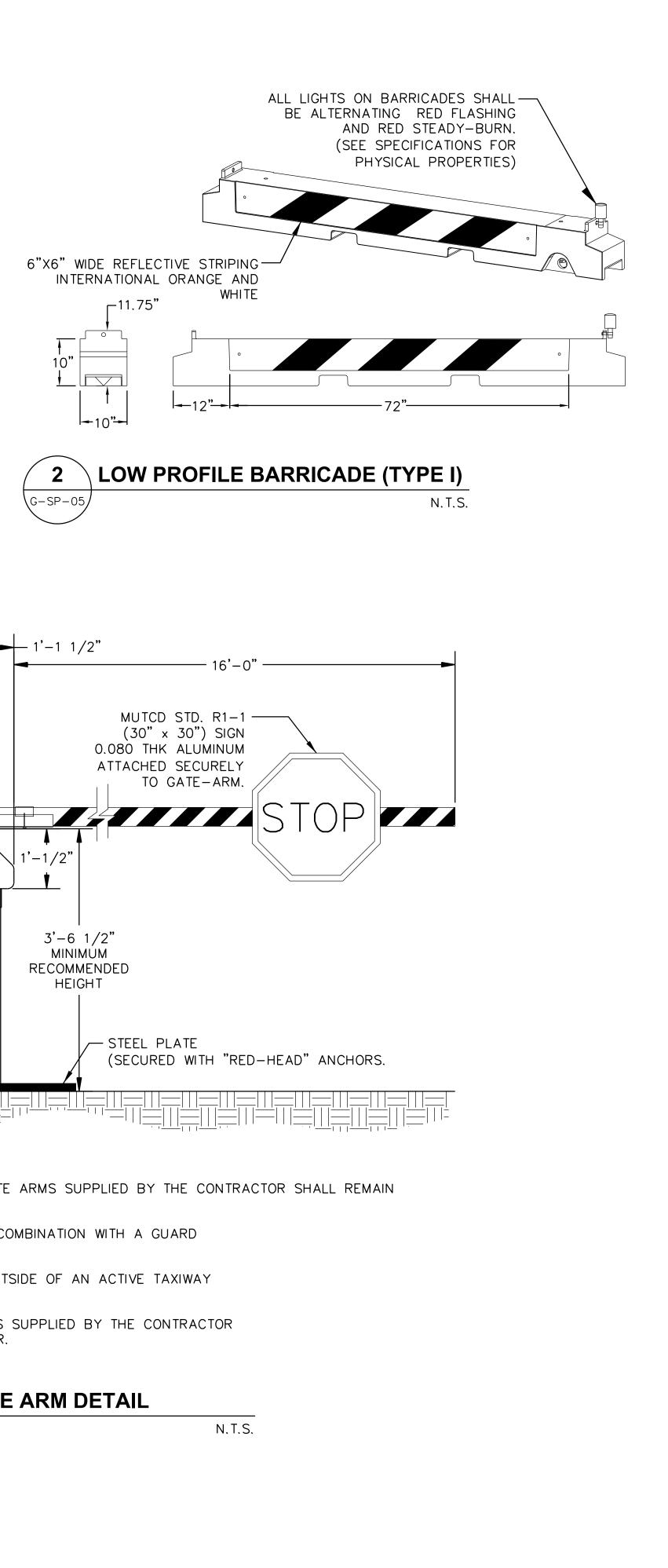
RED FLASHING LIGHT, WI PLATE TO ALLOW MOUNT CONE,
10 LB. ORANGE PVC TRAFFIC CONE
6" & 4" WHITE REFLECTORIZED COLLARS
LIGHT MOUNTING BRACKETS AND ANCHORING METHOD TO BE APPROVED BY THE PROGRAM MANAGER PRIOR TO USE.
REFLECTIVE CONE NOTES:
<ol> <li>TEMPORARY ORANGE REFLECTIVE CONES SHALL BE PLACED IN POSITION AS SHOWN ON PLANS. EACH CONE SHALL INCLUDE A 10-POUND PREFORMED WEIGHT.</li> </ol>

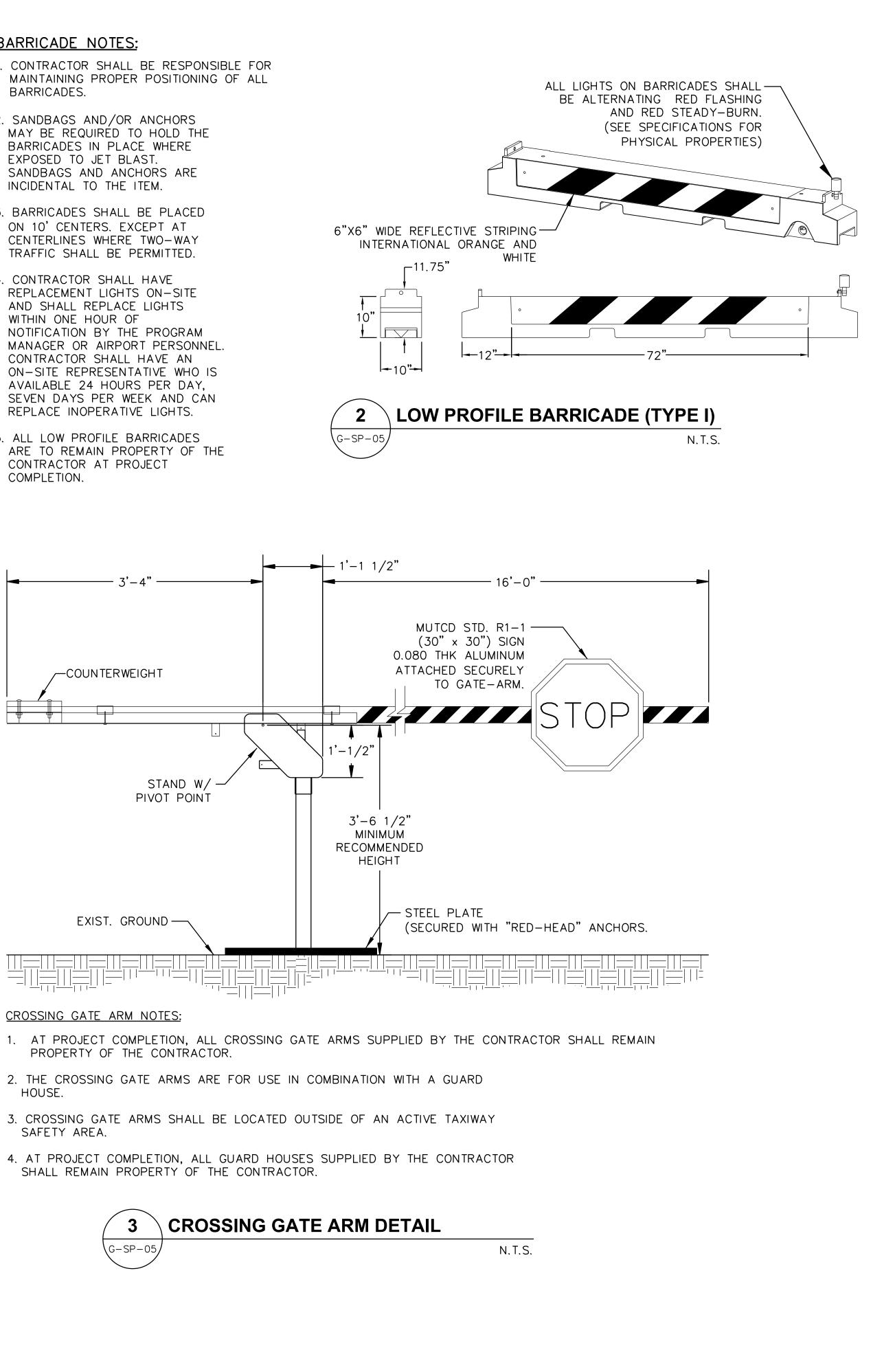
- 2. CONTRACTOR SHALL REPLACE USED CONES WITH NEW CONES AT ANY TIME THEIR VISIBILITY IS REDUCED BY AGING OR DAMAGE AS DIRECTED BY PROGRAM MANAGER OR MSCAA STAFF.
- 3. A RED FLASHING LIGHT SHALL BE PROVIDED AT EACH CONE. THE CONTRACTOR SHALL HAVE REPLACEMENT LIGHTS ON SITE AND SHALL REPLACE LIGHTS WITHIN ONE HOUR OF NOTIFICATION BY THE PROGRAM MANAGER OR AIRPORT PERSONNEL. CONTRACTOR SHALL PROVIDE THE NAME AND TELEPHONE NUMBER OF AN ON-CALL REPRESENTATIVE 24 HOURS PER DAY, SEVEN DAYS PER WEEK TO REPLACE INOPERATIVE LIGHTS.
- 4. CONES AND LIGHTS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, FOR NIGHT TIME VISIBILITY.
- 5. CONES SHALL EXTEND 15' BEYOND THE CONSTRUCTION LIMITS OR AS DIRECTED BY THE PROGRAM MANAGER.
- 6. ALL CONES ARE TO REMAIN PROPERTY OF THE CONTRACTOR AT PROJECT COMPLETION.



## **BARRICADE NOTES:**

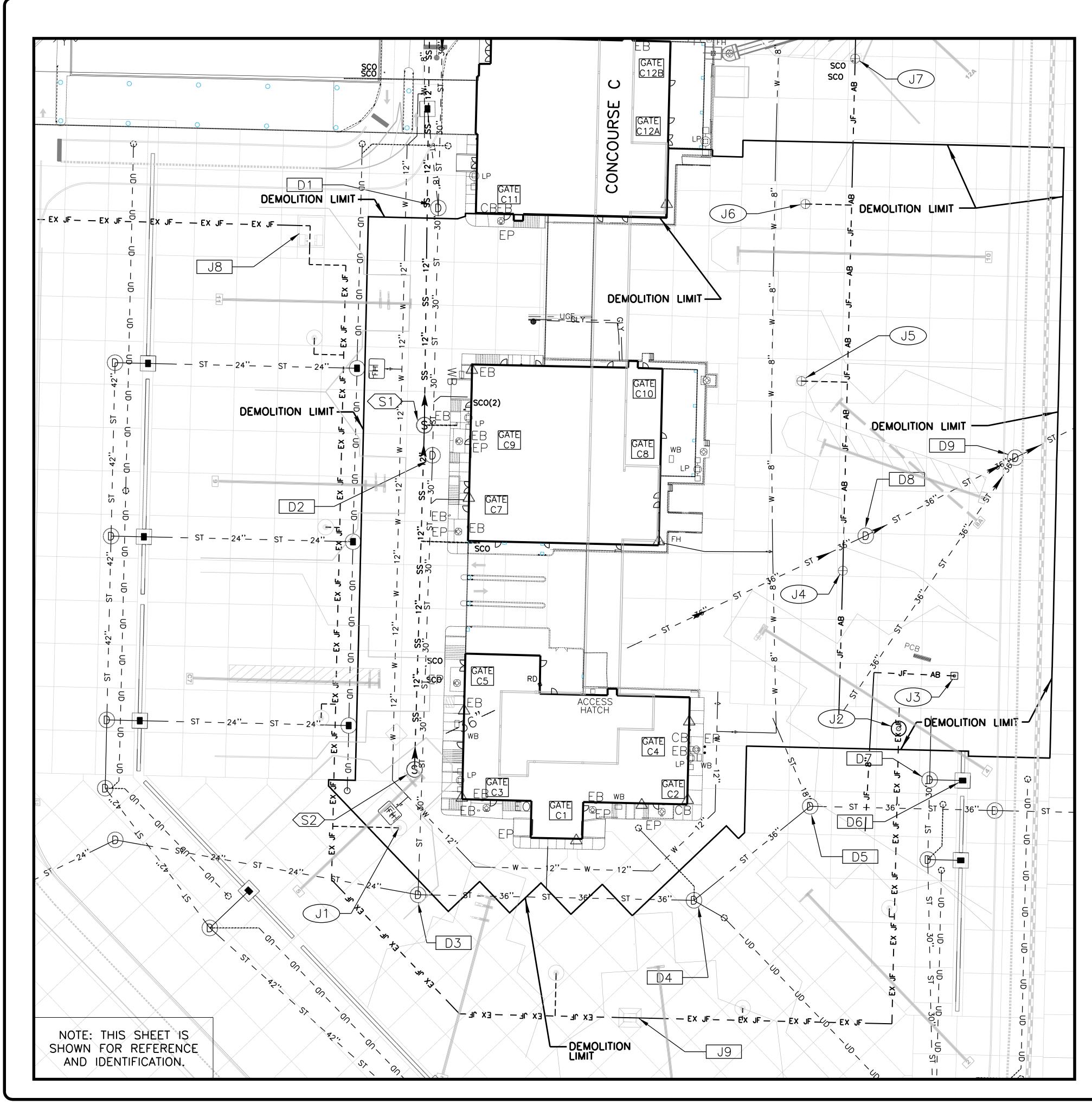
- 1. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING PROPER POSITIONING OF ALL BARRICADES.
- 2. SANDBAGS AND/OR ANCHORS BARRICADES IN PLACE WHERE EXPOSED TO JET BLAST. SANDBAGS AND ANCHORS ARE INCIDENTAL TO THE ITEM.
- 3. BARRICADES SHALL BE PLACED ON 10' CENTERS. EXCEPT AT CENTERLINES WHERE TWO-WAY TRAFFIC SHALL BE PERMITTED.
- 4. CONTRACTOR SHALL HAVE REPLACEMENT LIGHTS ON-SITE AND SHALL REPLACE LIGHTS WITHIN ONE HOUR OF NOTIFICATION BY THE PROGRAM MANAGER OR AIRPORT PERSONNEL. CONTRACTOR SHALL HAVE AN ON-SITE REPRESENTATIVE WHO IS AVAILABLE 24 HOURS PER DAY, SEVEN DAYS PER WEEK AND CAN REPLACE INOPERATIVE LIGHTS.
- 5. ALL LOW PROFILE BARRICADES CONTRACTOR AT PROJECT COMPLETION.







TAKE OFF WITH US. A MEMPHIS INTERNATIONAL AIRPORT 2491 WINCHESTER RD. SUITE 113 38116–3856 38116–3856
Urban Baran Associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
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CONSULTANT Pickering Firm Incorporated
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NO. DESCRIPTION DATE BY REVISIONS
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SOUTH DEMO SHEET TITLE: OPERATIONAL DETAILS AND NOTES



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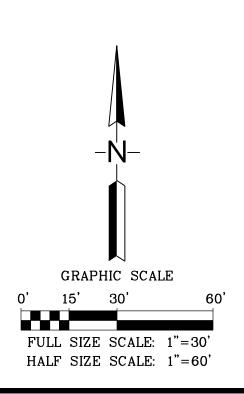
E	EXISTING STORM DRAINAGE SYSTEM STRUCTURES					
STRU. #	STRU. TYPE	TOP ELEV.	GRATE ELEV.	INV. IN PIPE SIZE	INV. OUT PIPE SIZE	
D1	DMH	295.43		289.79–18" N	282.70-30"S	
D2	DMH	295.51		282.38-30" N	282.33-30"S	
D3	DMH	295.56		281.58-30" N	281.08-36"E	
D4	DMH	294.97		280.62-36"W	280.57-36" NE	
D5	DMH	294.29		280.33-36"SW	280.28-36"E	
D6	INLET	293.76		_	287.50-30"W	
D7	DMH	293.85		286.46-24"N 287.45-30"E	285.96-30"S	
D8	DMH	292.99		282.65-??" W	282.44–36" NE	
D9	DMH	291.96		281.42-36"SW 280.72-36"S	280.68–36" NE	
SERVIC	CE LINE	S TO BUILI	DINGS ARE	4" TO 6"ø		

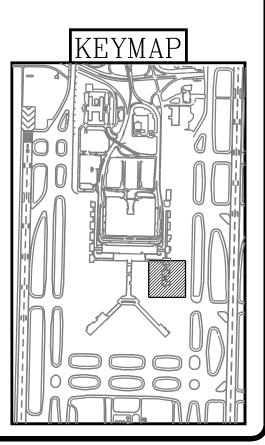
E	EXISTING HYDRANT FUEL SYSTEM STRUCTURES						
STRU. #	STRU. TYPE	TOP ELEV.	TOP OF VALVE	TOP OF PIPE	BOTTOM OF PIT		
	FHP	295.40	_	292.48	291.40		
J2	_	_	—	_	_		
JJ	FHP	293.41	—	290.51	289.41		
J4	JFA	293.69	_	_	28 <del>9</del> .41		
J5	JFA	293.70	—	_	_		
J6	JFA	293.87	—	_	_		
J7	JFA	293.66		_	_		
J8	JFV	295.80	_	_	_		
J9	IV	295.60	_	_	_		

FHP – FUEL HYDRANT PIT
JFV – JET FUEL VAULT
HP – HIGH POINT VENT
IV – ISOLATION VALVE PIT
LP – LOW POINT DRAIN
SV – SUMP VAULT
TOP = TOP OF STRUCTURE COVER
JFA – ABANDONED (NO SERVICE)
LINES TO FUEL HYDRANT PITS ARE 6"Ø

EXIST	ING SA	NITARY S	EWER SYSTEM	STRUCTURES	
STRU. #	STRU. TYPE	TOP ELEV.	INV. IN PIPE SIZE	INV. OUT PIPE SIZE	
(S1)	SMH	295.99	293.25-??"E 287.85-12"S	287.82-??"N	
S2	SMH	295.92	_	288.78-12" N	
SERVICE LINES TO BUILDINGS ARE 4" TO 6"Ø					

## HYDRANT FUEL STRUCTURE TYPE TABLE DEFINITIONS





TAKE OFF WITH US. TAKE OFF WITH US. A MARATIONAL AIRPORT 2491 WINCHESTER RD. SUITE 113 MEMPHIS, TENNESSEE 38116-3856					
ARCHITECT Urban Acchitect Associates, p.c. A98 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com					
DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN CONSULTANT					
JOB NO. 22942.22					
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NO. DESCRIPTION DATE BY REVISIONS PROJECT:					
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO					
SHEET TITLE: EXISTING CONDITIONS MSCAA NO. 13-1368-02 DWG. FILE NAME 13136802_C-EC-01 DATE DWG NO. 05/31/22					
SCALE         C-EC-01           1" = 30'         SHEET NO.					

# **5/31/2022 10:49:56 AM** T:\22942.22\Drawings\1

## DEMOLITION NOTES:

- DEMOLITION.

POTABLE WATER CABINETS 1						
GATE	OWNER	MANUFACTURER	SERIAL NO.	CABINET SIZE		
				(LxWxH) (IN)		
C2	Delta Airlines	Semler Industries, Inc	12237	48x30x63		
C4	Delta Airlines	Semler Industries, Inc	12231	48x30x63		
C5	Delta Airlines	Semler Industries, Inc	12233	48x30x63		
C8	Delta Airlines	Semler Industries, Inc	N/A	48x30x63		
C9	Delta Airlines	Semler Industries, Inc	12234	48x30x63		
		R CABINETS TO BE RE MSCAA. APPROXIMATE 1000 LBS.				

1. THE DEMOLITION PLAN SHEETS C-DP-01 THRU C-DP-02 INCLUDE ALL PROJECT RELATED

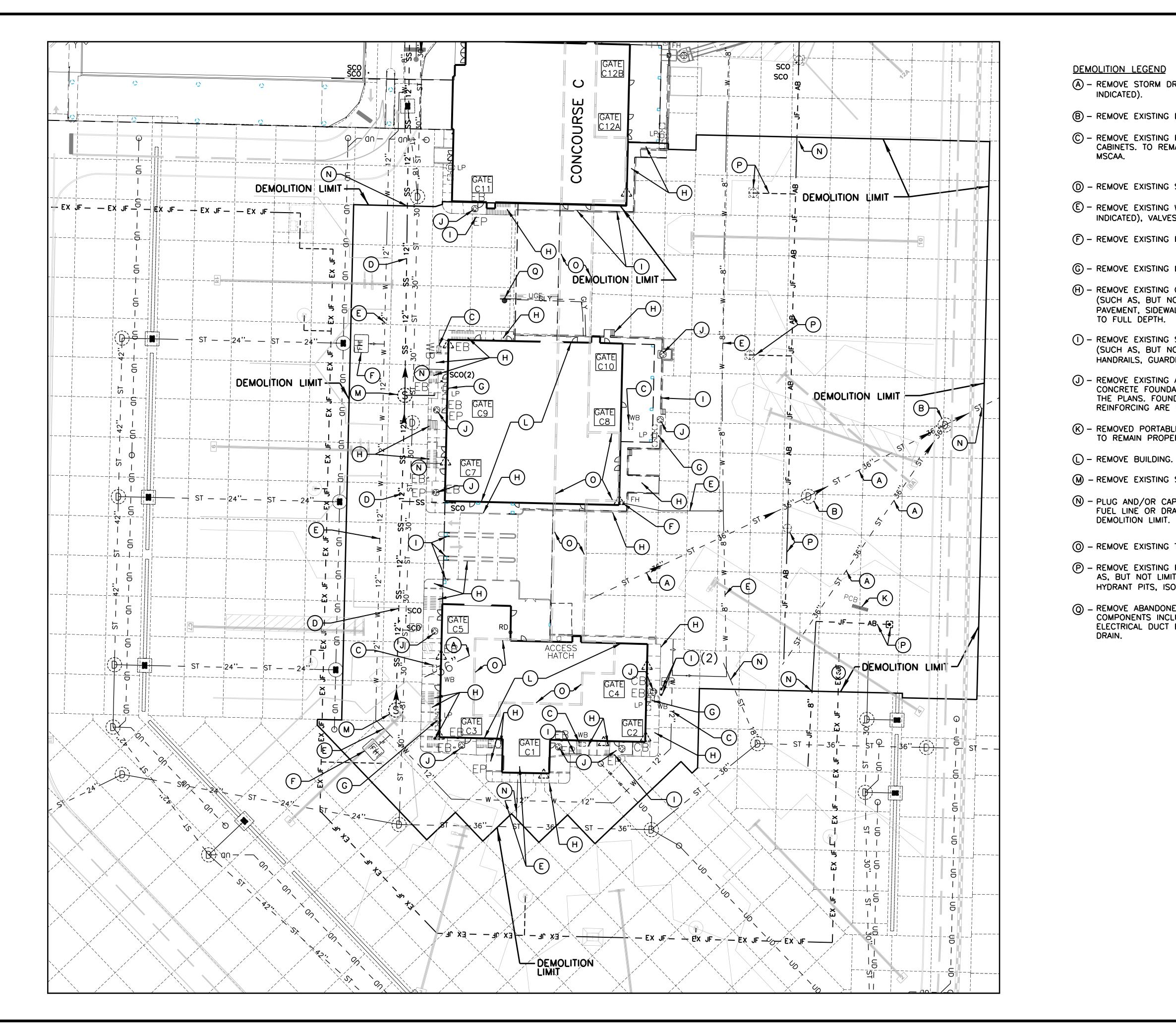
2. ALL DEMOLITION ITEMS INDICATED AS ''TO REMAIN PROPERTY OF MSCAA'' SHALL BE CAREFULLY REMOVED AND STOCKPILED AT A LOCATION ON AIRPORT PROPERTY, AS DIRECTED BY THE PROGRAM MANAGER. THE PROGRAM MANAGER MAY REQUIRE ANY ITEMS DAMAGED DURING DEMOLITION, THAT ARE TO REMAIN MSCAA PROPERTY, TO BE REPAIRED OR REPLACED AT NO ADDITIONAL CONTRACT COST.

3. ALL UNDERGROUND FEATURES AS SHOWN HEREIN ARE BASED ON A COMBINATION OF FIELD SURVEYS, MAPS, RECORD DRAWINGS AND OTHER DOCUMENTS AND MAY NOT REPRESENT CURRENT CONDITIONS.

4. UNLESS NOTED OTHERWISE, ALL EXISTING ELECTRICAL CONDUIT, WIRE, AND CABLE WITHIN THE PROJECT DEMOLITION LIMITS THAT FEED ITEMS TO BE DEMOLISHED SHALL BE REMOVED AND BECOME PROPERTY OF CONTRACTOR.

5. A FULL DEPTH VERTICAL SAWCUT IS REQUIRED AT ALL CONCRETE AND ASPHALT SURFACES AND PAVEMENTS AT THE DEMOLITION LIMIT, UNLESS A JOINT IS IN PLACE AT THE LIMIT.

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Ur Associ 498 SO MEMPHIS, T P 901.578.7173 WWW.U	ARCHITECT Urban associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com					
DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN	PICK W. PICE ED FA VICE DULTON NOT DULTON NOMMERCE OF TEN		ANNIA CONTRACTOR			
JOB NO.	Pickering Firm Incorporated					
This information is releas interest of the Memphis- (MSCAA) or its Tenants. All drawings received if or produced for MSCAA regarded as privileged in released to others withou permission of MSCAA. Your acceptance of the your understanding of, a	Shelby County A rom MSCAA or p as a work produ- formation, not to ut the express w released material nd commitment	irport Au oroduced ct shall o be ritten s establ to, provi	for be ishes ding			
ISSUED FOR BID/CONSTRUCTION						
	RIPTION	DATE	BY			
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REV PROJECT: TERMINA RECONS CONCO SOUT SHEET TITLE: DEMOLI NOTES &	AL APF TRUC DURSE H DEM		Z Z			
REV PROJECT: TERMINA RECONS CONCO SOUT SHEET TITLE: DEMOLI NOTES & MSCAA NO. 13- DWG. FILE NAME 131368	AL APF TRUC DURSE H DEM		Z Z			
REV PROJECT: TERMINA RECONS CONCO SOUT SHEET TITLE: DEMOLI NOTES 2 MSCAA NO. 13- DWG. FILE NAME 131368 DATE 05/31/22	AL APF TRUC DURSE H DEM TION P & DETA					
REV PROJECT: TERMINA RECONS CONCC SOUT SHEET TITLE: DEMOLI NOTES & MSCAA NO. 13- DWG. FILE NAME 131368	AL APF TRUC DURSE H DEM					



(A) - REMOVE STORM DRAIN PIPE (SIZE AS (B) – REMOVE EXISTING DMH. © – REMOVE EXISTING POTABLE WATER CABINETS. TO REMAIN PROPERTY OF  $\bigcirc$  – REMOVE EXISTING SS PIPE. E – REMOVE EXISTING WATER LINE (SIZE AS INDICATED), VALVES & FITTINGS.  $\bigcirc$  – REMOVE EXISTING FIRE HYDRANT. © – REMOVE EXISTING LIGHT POLE. (H) – REMOVE EXISTING CONCRETE FEATURE (SUCH AS, BUT NOT LIMITED TO, PAVEMENT, SIDEWALK, STEPS, AND RAMPS) TO FULL DEPTH. () – REMOVE EXISTING SITE APPURTENANCES (SUCH AS, BUT NOT LIMITED TO, HANDRAILS, GUARDRAILS & BOLLARDS). (J) - REMOVE EXISTING ABANDONED P.B.B. CONCRETE FOUNDATION AS INDICATED IN THE PLANS. FOUNDATION DIMENSIONS & REINFORCING ARE UNKNOWN. (K) – REMOVED PORTABLE CONCRETE BARRIER. TO REMAIN PROPERTY OF MSCAA. M - REMOVE EXISTING SMH.  $\bigcirc$  - PLUG AND/OR CAP EXISTING SEWER LINE, FUEL LINE OR DRAINAGE LINE AT DEMOLITION LIMIT. O – REMOVE EXISTING TUNNEL. P - REMOVE EXISTING FUEL SYSTEM (SUCH AS, BUT NOT LIMITED TO FUEL PIPE, HYDRANT PITS, ISOLATION VALVES, ETC.). Q – REMOVE ABANDONED GLYCOL SYSTEM COMPONENTS INCLUDING GLYCOL PIPES, ELECTRICAL DUCT BANK, AND LOW POINT GRAPHIC SCALE 0' 15' 30' 60' FULL SIZE SCALE: 1"=30' HALF SIZE SCALE: 1"=60' KEYMAP  $\bigcirc \bigcirc \bigcirc \bigcirc$ 

ARCHITECT ARCHITECT MEMPHIS, INLERNATIONAL AIROORT 38116-3856 38116-3856 WWW.uarch.com					
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JOB NO.	229	942.2	22		
or produced regarded as released to o permission of Your acceptar your understa adequate con	e Memphis- s Tenants. s received if for MSCAA c privileged in thers withou MSCAA. ace of the n nding of, ar trol of the n	Shelby rom MS formatic to the e released nd commaterial	County A CAA or p rk produun, not to xpress w material mitment s issued	irport Au roduced ct shall o be ritten s establ to, provi and pro	for be ishes ding duced.
BID/C	CONS	STF	RUC	TIC	DN
NO.	REVI	RIPTIO		DATE	BY
PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO					N
DEN	SHEET TITLE: DEMOLITION PLAN				
MSCAA NO	13-1	1368-(	)2		
DWG. FILE	NAME 1313680	02_C-			
05/31 SCALE	/22		C-DI	<b>&gt;</b> -02	
1" = 30'         SHEET NO.           FACILITY NO.         31 OF 87					

## DEMOLITION NOTES

- REQUIREMENTS ADJACENT TO THE CONCOURSE BUILDINGS.

- DOCUMENTS AND MAY NOT REPRESENT AS-BUILT CONDITIONS.
- "HYDRANT FUEL SYSTEM MEM 2004 CONCOURSE A AND CONCOURSE C" PROJECT NUMBER MEM 98-14 SEPTEMBER 14, 1999 BY: BURNS & MCDONNELL "1972 CONST. PLANS" FOR MEMPHIS INTERNATIONAL AIRPORT
- "TERMINAL APRON RECONSTRUCTION" FOR MEMPHIS-SHELBY COUNTY AIRPORT AUTHORITY

- UNKNOWN.
- SHOWN HEREIN.
- PAVEMENTS THAT DO NOT REQUIRE FULL PANEL REMOVAL.

		EXISTING I	PAVEMENT	SECTION	INFORMATION
DRAWING LEGEND	YEAR CONSTRUCTED (APPROX.)	SURFACE	BASE	SUB-BASE	MAJOR EMBEDMENTS AND OTHER NOTES
	1972	14 IN. THICK PLAIN PCC	10 IN. CEMENT TREATED AGGREGATE BASE	6 IN. SOIL CEMENT	PCC EMBEDMENTS INCLUDE #5 DEFORMED BARS @ 18 IN. O.C. & 30 IN. LONG
	1999	14 IN. THICK PLAIN PCC	6 IN. ECONOCRETE	NONE	PCC EMBEDMENTS INCLUDE 6×6 W5×W5 W/ 12 IN. END LAPS.
	2013	18 IN. THICK PLAIN PCC	4 IN. POROUS BITUMINOUS/ 8 IN. CEMENT TREATED AGGREGATE BASE	10 IN. SOIL CEMENT	PCC EMBEDMENTS INCLUDE: TWO MATS 6×6 – W6 × W6 WWF W/16 IN. LAP. EA. WAY & 1½ IN. X 20 IN. LONG DOWEL @ 18 IN. O.C.

1. THE PURPOSE OF THESE SHEETS IS TO INDICATE DEMOLITION OF AIRFIELD PAVEMENTS. REFER TO SHEETS C-DP-01 THRU C-DP-02 FOR OTHER DEMOLITION

2. DUE TO THE PROXIMITY OF THE CONCOURSE BUILDINGS AND UTILITY TUNNELS AS WELL AS THE EXTENSIVE NETWORK OF UNDERGROUND UTILITIES, PAVEMENT DEMOLITION METHODS ARE EXTREMELY IMPORTANT TO THE OWNER. PAVEMENT DEMOLITION MEANS AND METHODS WILL BE CONSTRAINED BY ACCEPTABLE LEVELS OF VIBRATION. CONTRACTOR TO PROVIDE PROPOSED METHODOLOGY FOR APPROVAL BY THE PROGRAM MANAGER PRIOR TO DEMOLITION ACTIVITIES.

3. EXISTING PAVEMENT JOINTING AS SHOWN HEREIN IS BASED ON FIELD SURVEYS AND DESIGN JOINTING FROM PROJECT 08-1259-02.

4. EXISTING PAVEMENT CHARACTERISTICS (THICKNESS, PAVEMENT SECTION, AND EMBEDMENTS) HAVE BEEN OBTAINED FROM RECORD DRAWINGS, MAPS, AND OTHER

5. THE FOLLOWING PROJECT DRAWINGS WERE UTILIZED TO GENERATE THESE DEMOLITION PLANS:

MSCAA PROJECT NUMBER 053C3018 MARCH 15, 1972 AS BUILT BY: ALLEN & HOSHALL CONSULTING ENGINEERS

"CONCOURSE A REGIONAL JET FACILITY" FOR MEMPHIS INTERNATIONAL AIRPORT

MSCAA PROJECT NUMBER 99019 SEPTEMBER 21, 1999 AS BUILT BY: THE CRUMP FIRM INC.

MSCAA PROJECT NUMBER 08-1259-02 APRIL 20, 2012 BY: PICKERING FIRM, INC.

6. ALL EXISTING SOIL CEMENT BASES/SUB-BASES WITHIN THE DEMOLITION LIMITS SHALL BE DEMOLISHED, PULVERIZED TO 3" MAX. DIMENSION, AND UTILIZED AS PROJECT EARTHWORK, ALL UNUSED MATERIAL SHALL BE HAULED OFF OF AIRPORT PROPERTY.

7. ALL EXISTING UNBOUND (UNSTABILIZED) GRANULAR BASES AND/OR SUB-BASES WITHIN THE DEMOLITION LIMITS SHALL BE UTILIZED AS PROJECT EARTHWORK. ALL UNUSED MATERIAL SHALL BE HAULED OFF OF AIRPORT PROPERTY OR TO A LOCATION WITHIN AIRPORT PROPERTY AS APPROVED BY THE PROGRAM MANAGER.

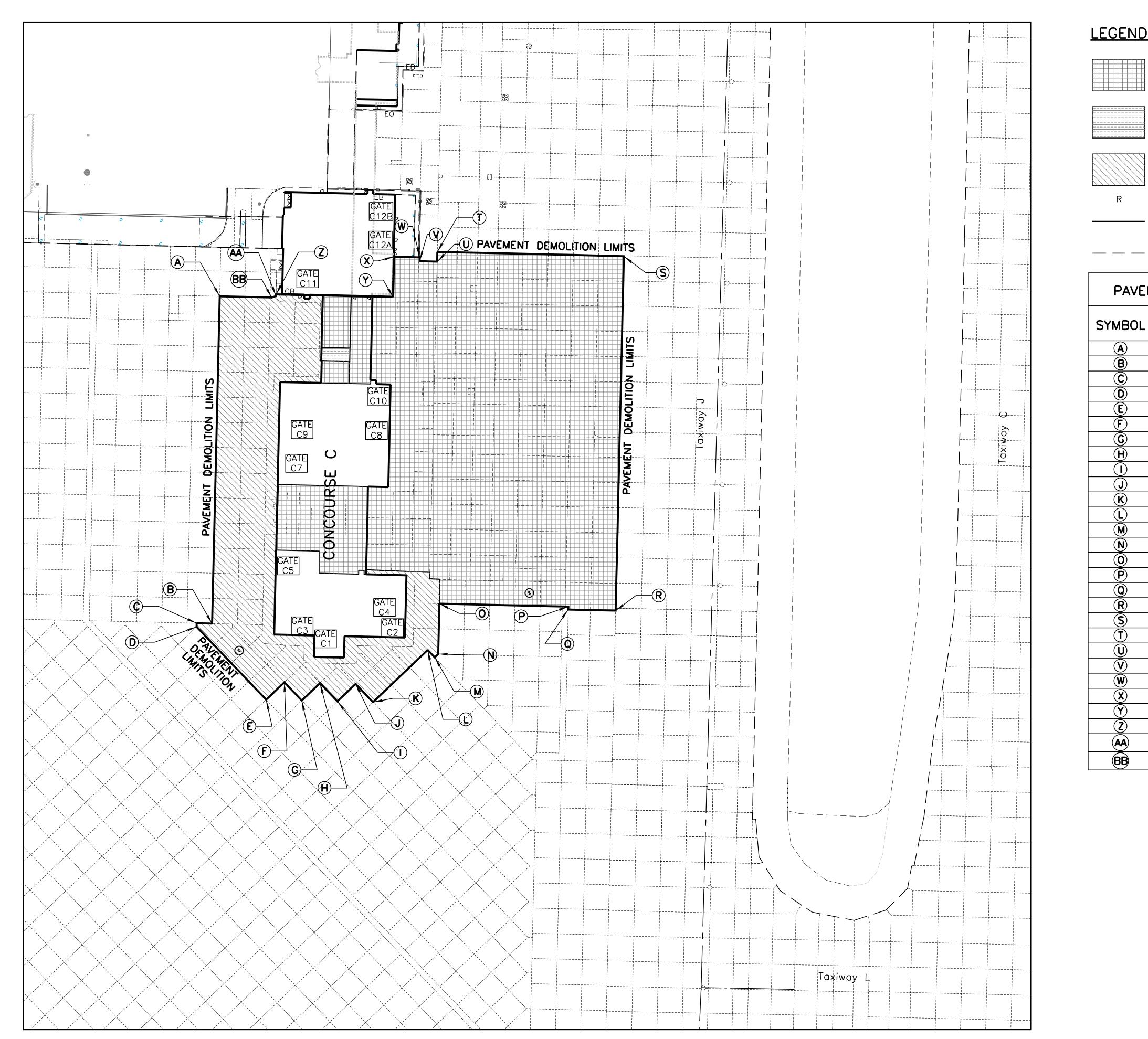
8. THE APRON PAVEMENT CONTAINS ASPHALT PATCHING WHICH ARE NOT SHOWN HEREIN. THE AGE AND THICKNESS (OVERLAY, FULL DEPTH, ETC.) OF SAID PATCHES IS

9. THE APRON PAVEMENT CONTAINS FULL-DEPTH CONCRETE PATCHES OF UNKNOWN THICKNESS OVER AN ECONOCRETE BASE OF UNKNOWN THICKNESS. WITH A PLASTIC SHEETING BOND BREAKER BETWEEN. THE LOCATION OF SAID PATCHES IS UNKNOWN.

10. FOR CLARITY EXISTING PAVEMENT PENETRATIONS SUCH AS DRAINAGE STRUCTURES, JET BRIDGE FOUNDATIONS, AND OTHER PAVEMENT PENETRATIONS ARE NOT

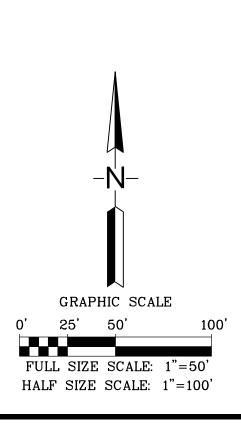
11. WHERE THE DEMOLITION LIMIT DOES NOT FALL ON A JOINT, A FULL DEPTH SMOOTH VERTICAL SAWCUT SHALL BE MADE THRU EXISTING CONCRETE

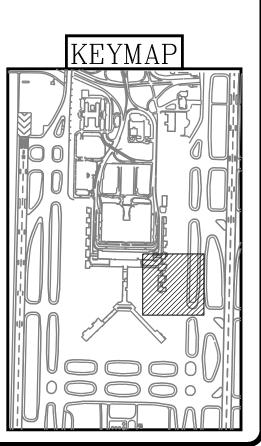
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DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN	CK W. LBED EA CONCLETENT CO			
CONSULTANT Pickering Firm Incorporated				
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	D FOR			
NO. DESCR	PIPTION DATE BY			
REVIS	SIONS			
PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO				
DEMOLITI	PAVEMENT ON NOTES			
MSCAA NO. 13-1	368-02			
DWG. FILE NAME 1313680	2_C-PD-01			
DATE 05/31/22	— Dwg no.			
SCALE	C-PD-01 SHEET NO.			
NTS FACILITY NO.	SHEET NO. 32 OF 87			

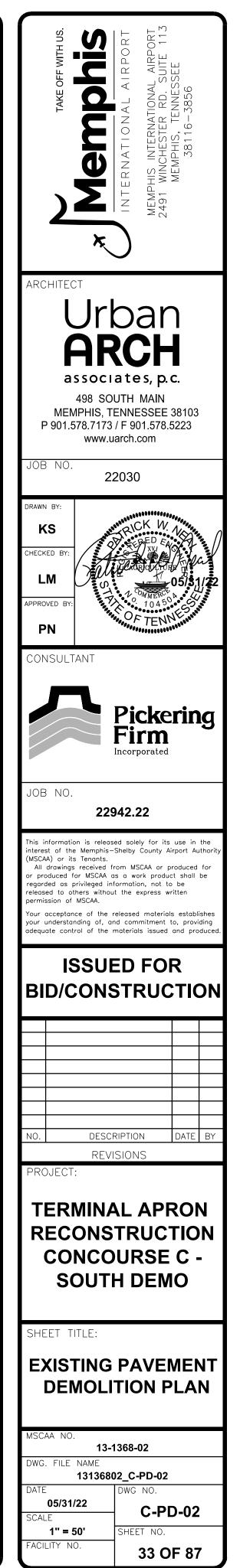


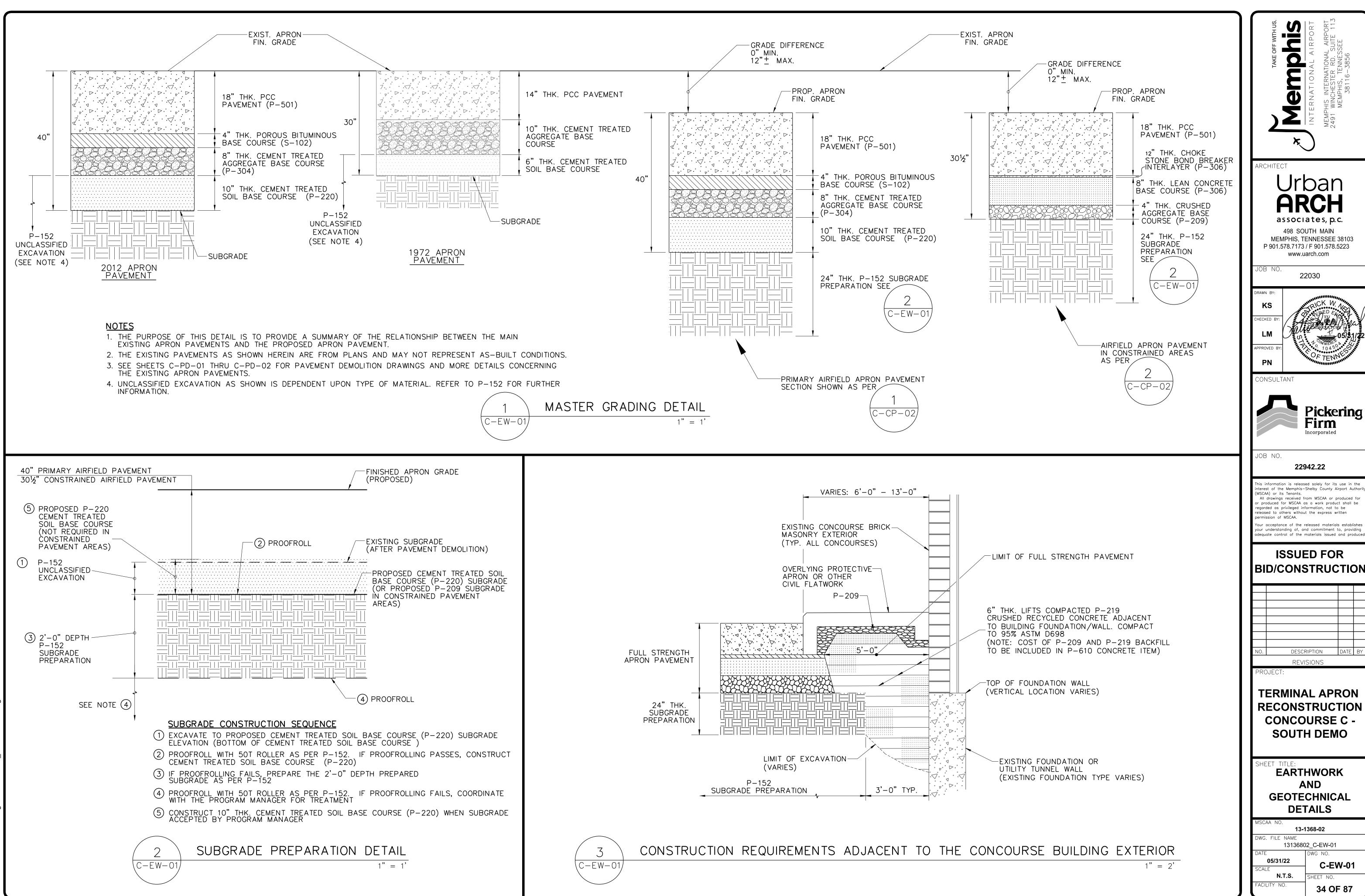
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)—	EXISTING	PAVEMENT D	<u>EMOLITION</u>			
	14 IN. PCC (APPROX. 10	TO BE REMOV ,350 S.Y.)	ED			
	14 IN. PCC (APPROX. 50	TO BE REMOV S.Y.)	ED			
	18 IN. PCC TO BE REMOVED (APPROX. 4100 S.Y.)					
1	REINFORCED	ODD PANELS				
- (	DEMOLITION LIMITS					
=	CONCOURSE	UTILITY TUNNE	ΞL			
EME	ENT DEMOLIT	ON LIMITS				
-	NORTHING (N)	EASTING (E)				
	282827.84	777386.79				
	282502.93	777379.26				
	282503.29	777363.91				
	282499.71	777363.83				
	282427.38	777432.88				
	282444.64	777450.96				
	282426.56	777468.23				
	282443.82	777486.31				
	282425.74	777503.57				
	282443.00	777521.66				
	282424.92	777538.92				
	282476.71	777593.17				
	282469.40	777600.14				
	282472.72	777603.62				
	282522.71	777604.78				
_	282519.74	777732.74				
	282516.88	777732.68				
	282515.79	77779.66				
	282867.62	777787.84				
	282871.88	777602.74				
	282862.25	777602.62				
	282862.74	777585.16				
	282866.51	777585.25				
	282867.13	777559.88				
+	282827.02	777558.91				
	282829.72	777442.78				
	282828.07	777442.74				
	282826.65	777438.31				

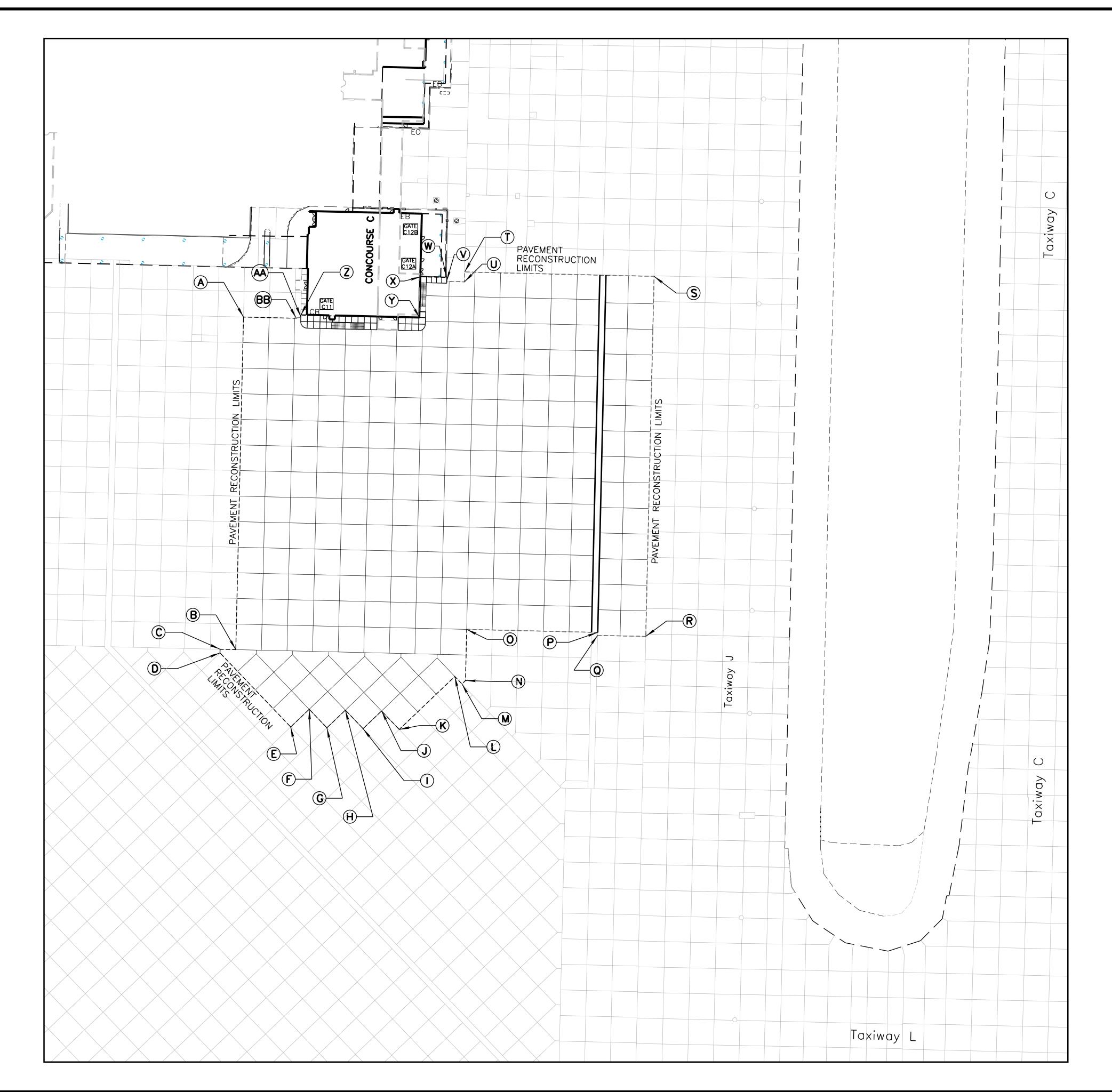








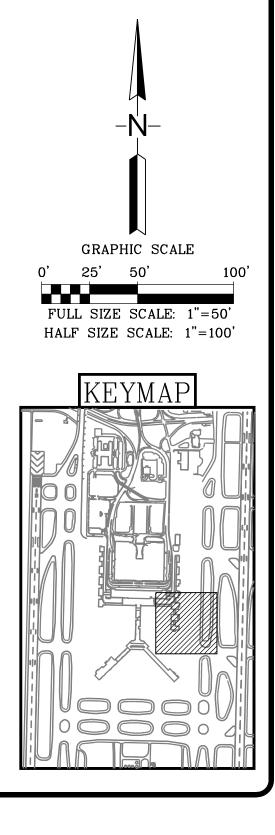
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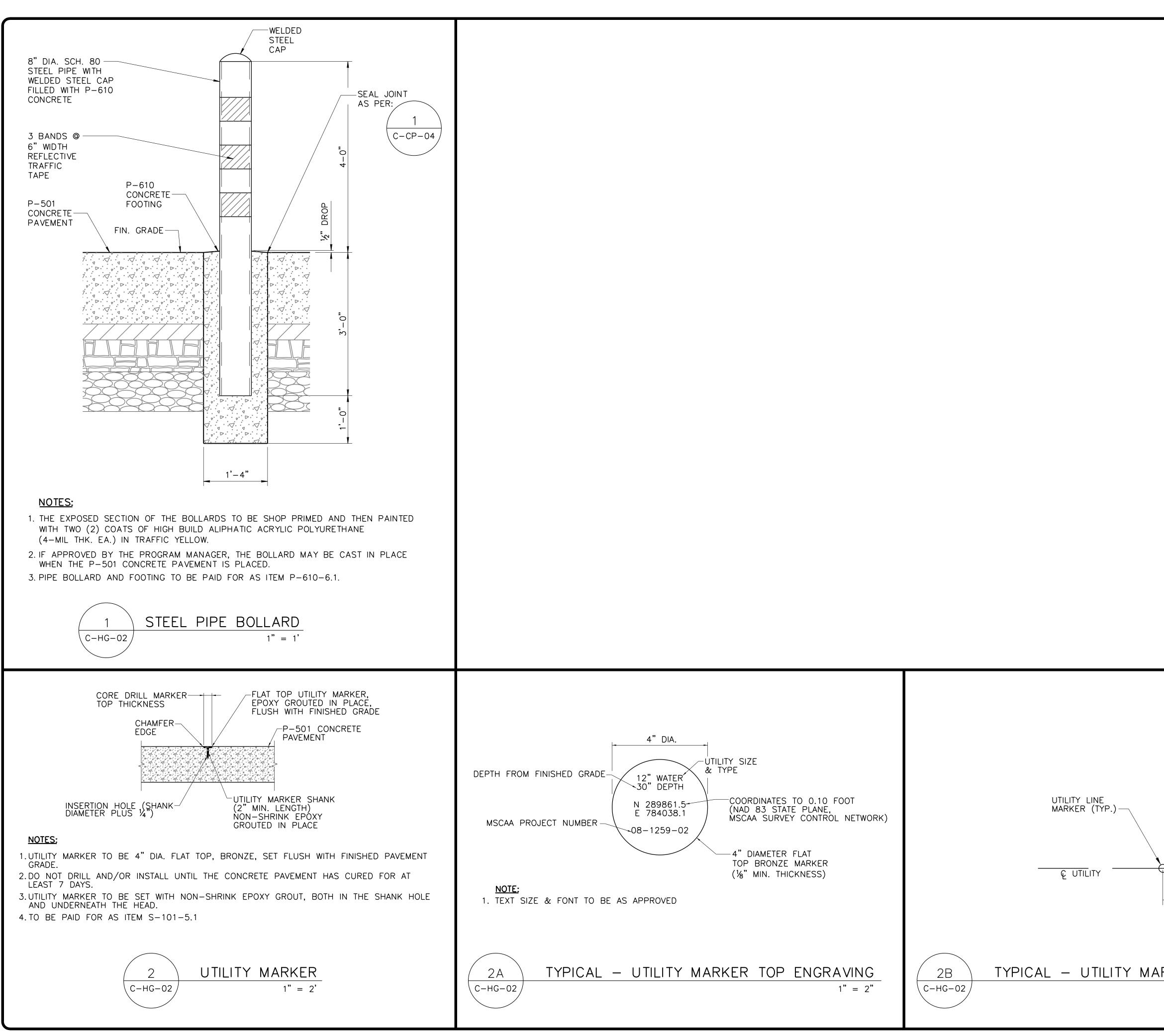
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# PAVEMEN SYMBOL A B C D E F G H I J K L M N O R S T V V X Y Z AA BB

IT RECONSTRU	ICTION LIMITS	
NORTHING (N)	EASTING (E)	
282827.84	777386.79	
282502.93	777379.26	
282503.29	777363.91	
282499.71	777363.83	
282427.38	777432.88	
282444.64	777450.96	
282426.56	6 777468.23	
282443.82	777486.31	
282425.74	777503.57	
282443.00	777521.66	
282424.92	777538.92	
282476.71	777593.17	
282469.40	777600.14	
282472.72	777603.62	
282522.71	777604.78	
282519.74	777732.74	
282516.88	777732.68	
282515.79	77779.66	
282867.62	777787.84	
282871.88	777602.74	
282862.25	777602.62	
282862.74	777585.16	
282866.51	777585.25	
282867.13	777559.88	
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282828.07	777442.74	
282826.65	777438.31	

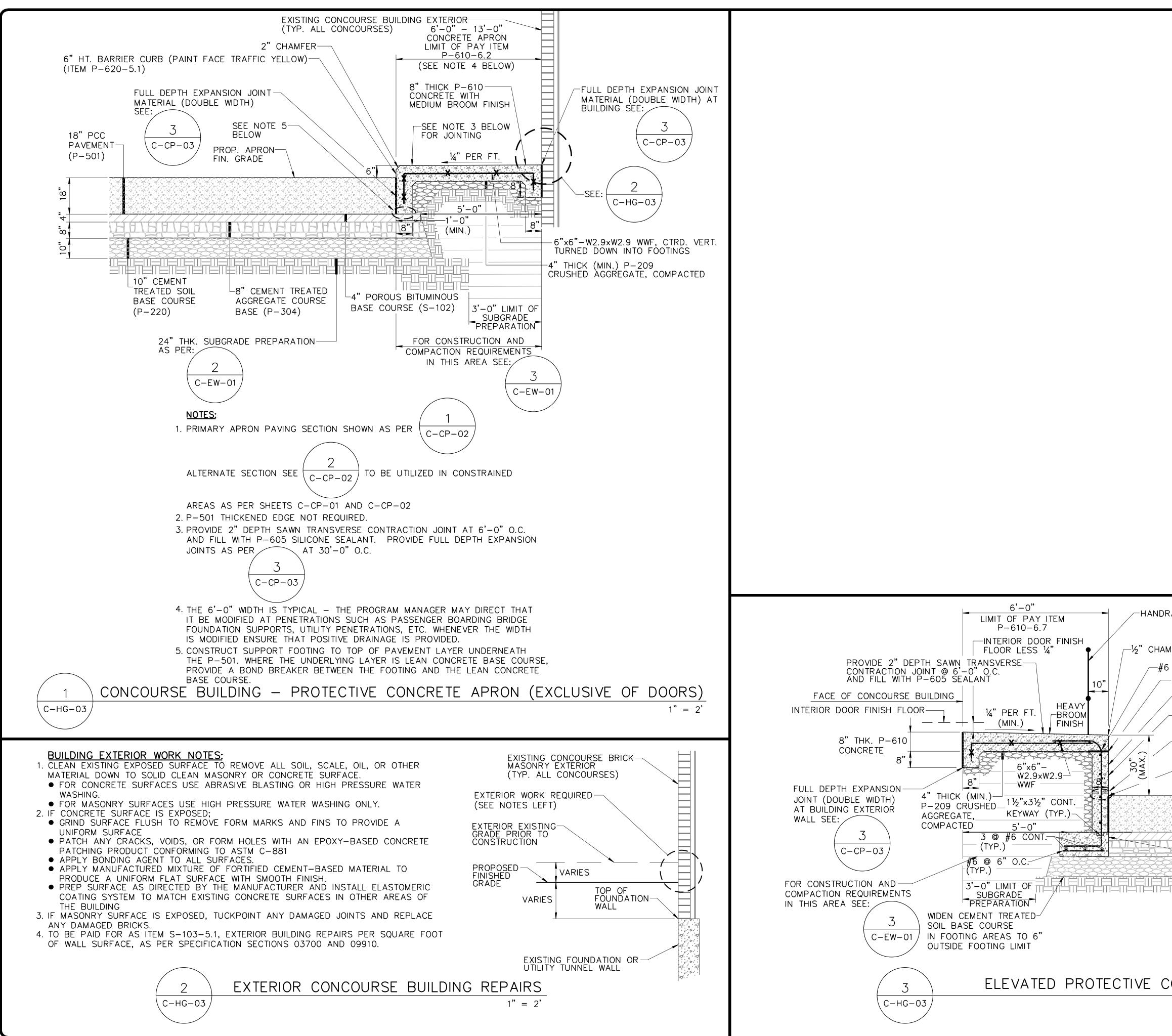


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www.u Job no.	/ F 901.578.5223 larch.com 2030	
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CONSULTANT		
	Pickering Firm	
JOB NO.	942.22	
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	ED FOR STRUCTION	
REV	RIPTION DATE BY	
PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO SHEET TITLE: HORIZONTAL		
GEOMETRIC PLAN		
MSCAA NO. <b>13-1368-02</b> DWG. FILE NAME		
13136802_C-HG-01 DATE DWG NO. 05/31/22		
05/31/22 SCALE 1" = 50'	C-HG-01 SHEET NO.	
FACILITY NO.	35 OF 87	

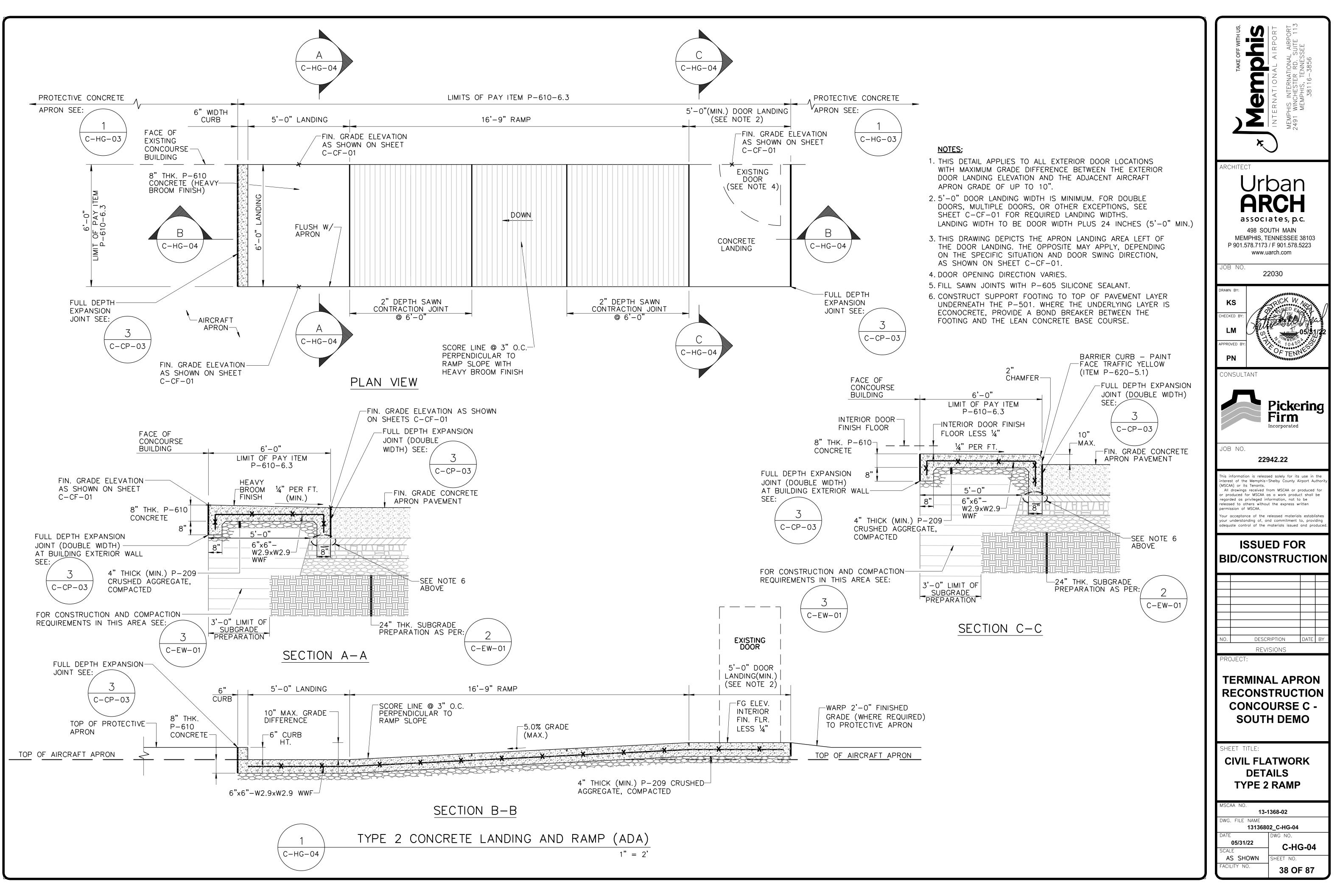


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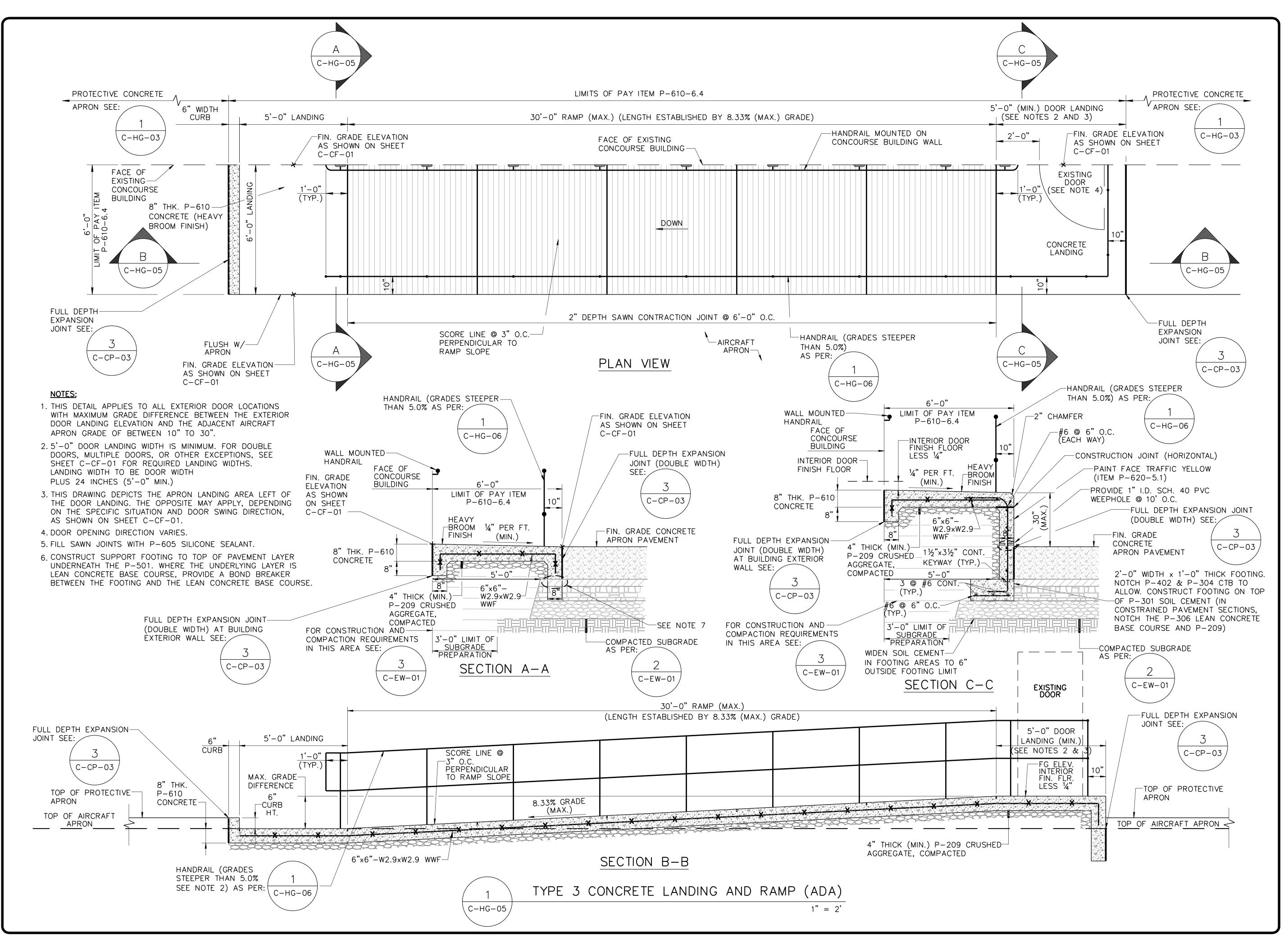
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	ARCHITECT Urban associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
	22030 DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN CONSULTANT
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2'-0" MAX	Image: state of the state
<u>RKER AT CHANGE OF DIRECTION</u> 1" = 1'	SHEET TITLE: CIVIL DETAILS BOLLARDS BOLLARDS GUARDRAIL UTILITY MARKERS MSCAA NO. 13-1368-02 DWG. FILE NAME 13136802_C-HG-02 DWG NO. 05/31/22 SCALE AS SHOWN FACILITY NO. 26 OE 97



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	ARCHITECT Urban associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
	DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN CONSULTANT
	JOB NO.       Description         This information is released solely for its use in the interest of the Memphis–Shelby County Airport Author
DRAIL AS PER: 1 C-HG-06 AMFER 6 @ 6" O.C. (EACH WAY)	(MSCAA) or its Tenants. All drawings received from MSCAA or produced for or produced for MSCAA as a work product shall be regarded as privileged information, not to be released to others without the express written permission of MSCAA. Your acceptance of the released materials establishe your understanding of, and commitment to, providing adequate control of the materials issued and produce <b>ISSUED FOR</b> <b>BID/CONSTRUCTIO</b>
-CONSTRUCTION JOINT (HORIZONTAL) PAINT FACE TRAFFIC YELLOW (ITEM P-620-5.1) PROVIDE VERTICAL CONSTRUCTION JOINT @ 30'-0" O.C. MATCH JOINT ON HORIZONTAL SURFACE PROVIDE 1" I.D. SCH. 40 PVC WEEPHOLE @ 10' O.C. FULL DEPTH EXPANSION JOINT (DOUBLE WIDTH) SEE: FIN. GRADE CONCRETE APRON PAVEMENT (C-CP-03)	NO. DESCRIPTION DATE B REVISIONS PROJECT:
2'-0" WIDTH x 1'-0" THICK FOOTING. NOTCH S-102 & P-304 CTB TO ALLOW. CONSTRUCT FOOTING ON TOP OF P-220 CEMENT TREATED SOIL BASE COURSE. IN CONSTRAINED PAVEMENT SECTIONS, NOTCH THE P-306 LEAN CONCRETE BASE COURSE AND P-209)	RECONSTRUCTION CONCOURSE C - SOUTH DEMO
$\frac{2}{C-EW-01}$ CONCRETE APRON $1" = 2'$	BUILDING REPAIRS         MSCAA NO.       13-1368-02         DWG. FILE NAME       13136802_C-HG-03         DATE       DWG NO.         05/31/22       DWG NO.         SCALE       C-HG-03         AS SHOWN       SHEET NO.         FACILITY NO.       37 OF 87



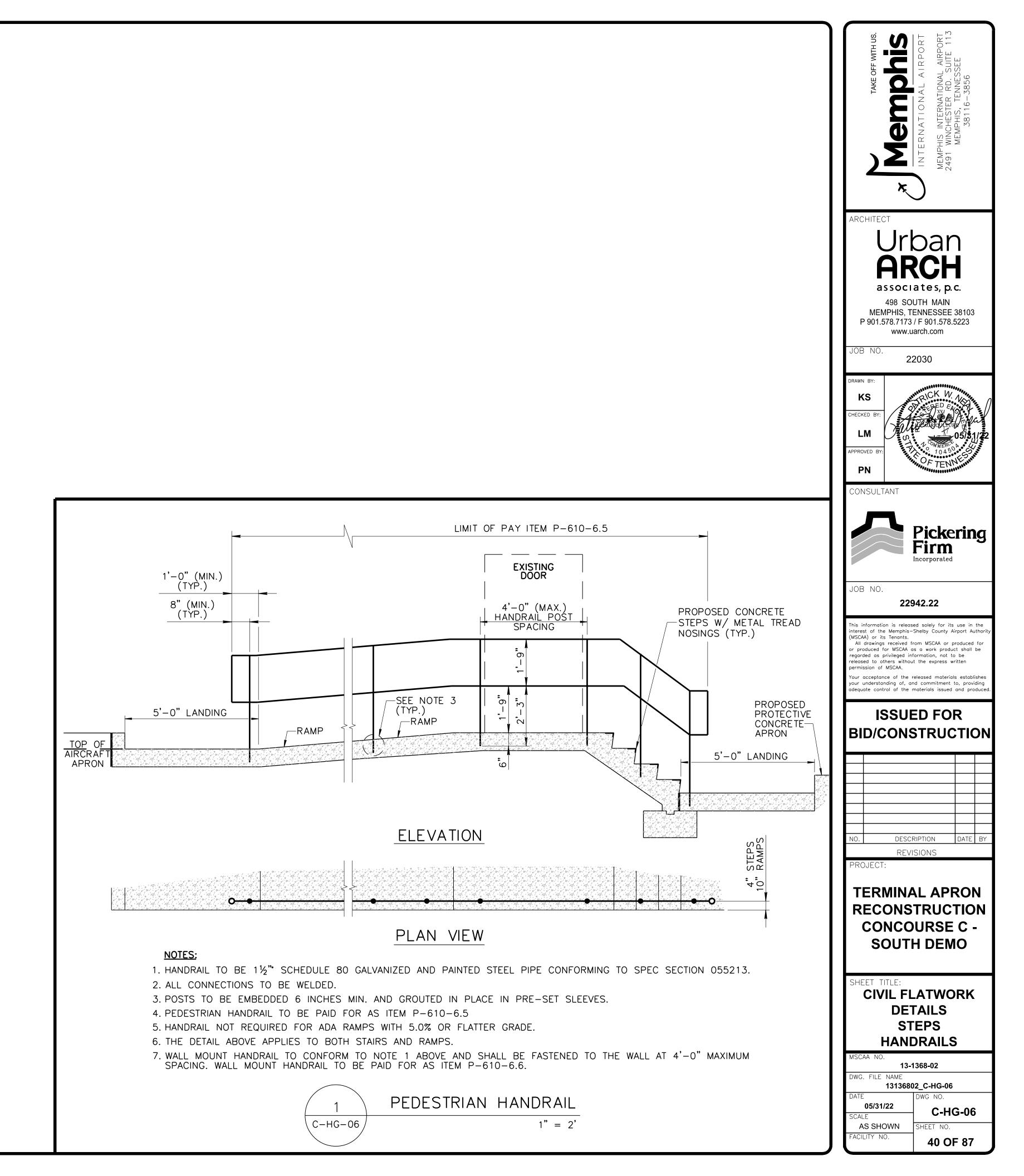
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CONSULT	Pi Fi	cke rm	rin	g
interest of the (MSCAA) or its All drawings or produced f regarded as p released to ot permission of Your acceptan your understan	received from M or MSCAA as a w privileged informat hers without the	ely for its County Ai SCAA or p ork produc ion, not to express wi d material nmitment f als issued	irport Au roduced ct shall b be ritten s establ to, provi and pro	for be ishes ding
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		RUC		
BID/C	ONST			
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	CONCOURSE C DOORS 1								
s	DC URVEY	DOR No. MSCAA	LOCATION	WIDTH (IN.)		ADA REQ'D. 3	EXTERIOR TREATMENT 2	HANDRAIL REQ'D. 4	NOTES
	C-22	N/N	GATE C12A	72	23.5	N	D	Y	USED "D" BECAUSE OF EXIST. RAMP BEING REPLACED
	C-47	C111	GATE C11	44	7.2	Y	С	Ν	

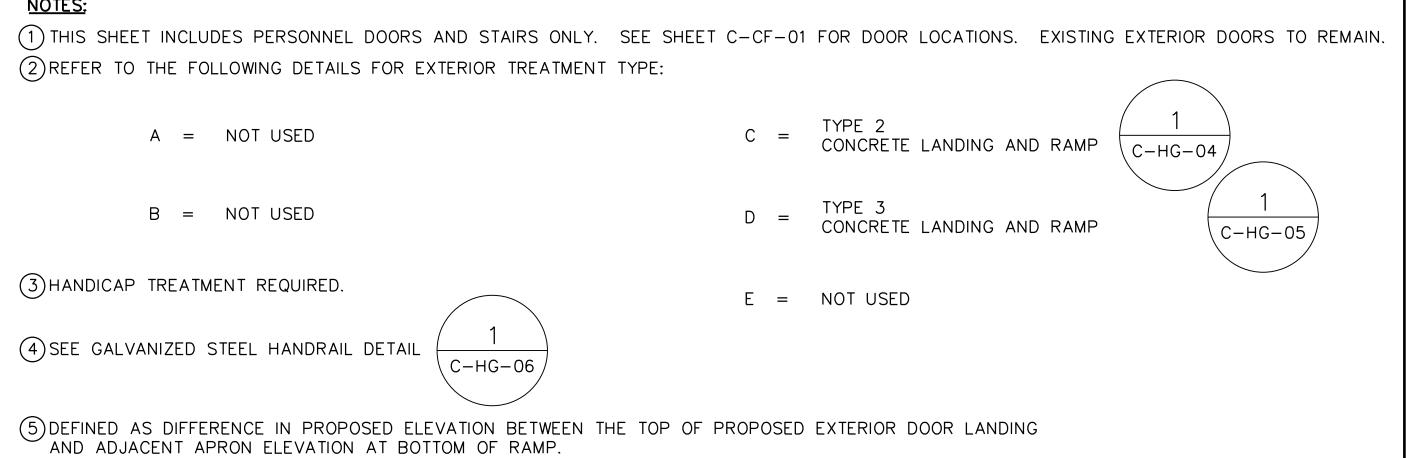
# NOTES: 2 REFER TO THE FOLLOWING DETAILS FOR EXTERIOR TREATMENT TYPE:

A = NOT USED

B = NOT USED

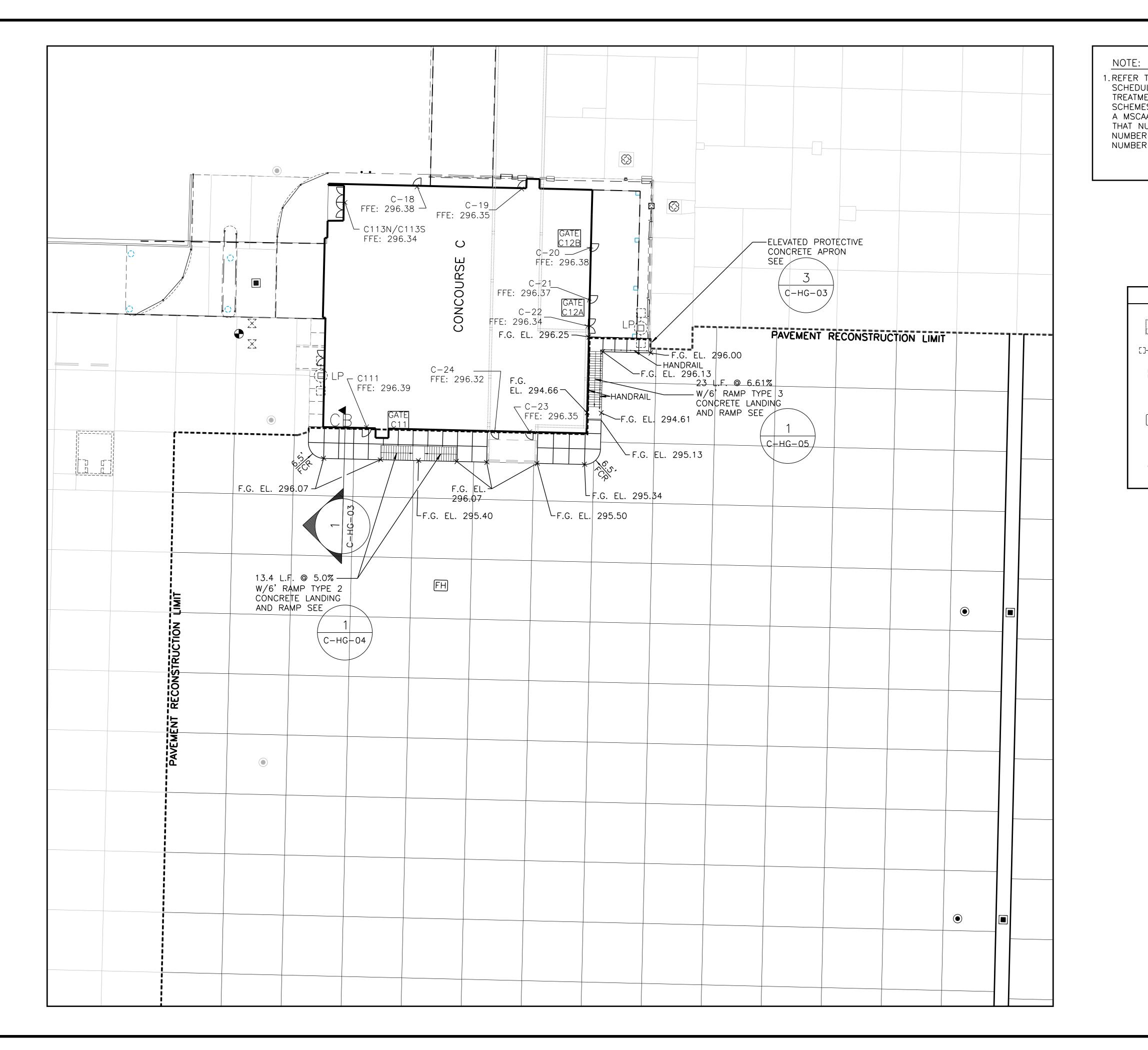
(3) HANDICAP TREATMENT REQUIRED.

	С-НС-06
5 DEFINED AS DIFFERENCE IN PROPOS	ED ELEVATION BETW
AND ADJACENT APRON ELEVATION	AT BOTTOM OF RAMF



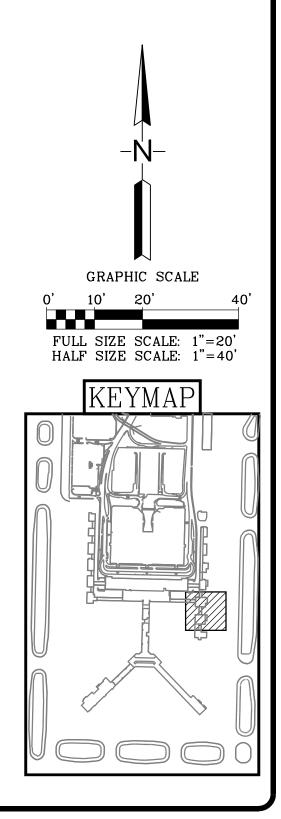
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AR associa 498 SOL MEMPHIS, TE P 901.578.7173 www.ua	DAN BACH A tes, p.c. JTH MAIN ENNESSEE 38103 / F 901.578.5223 arch.com		
DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN	CK W. N. PEDEA VIII CONTRACT 05/31/22 05/31/22 05/31/22 05/31/22		
	Pickering Firm		
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PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO			
SCHI A TREA	OR DOOR EDULE ND IMENTS		
MSCAA NO. 13-1	368-02		
DWG. FILE NAME 1313680	2_C-HG-07		
DATE 05/31/22	DWG NO. C-HG-07		
SCALE FACILITY NO.	SHEET NO. 41 OF 87		



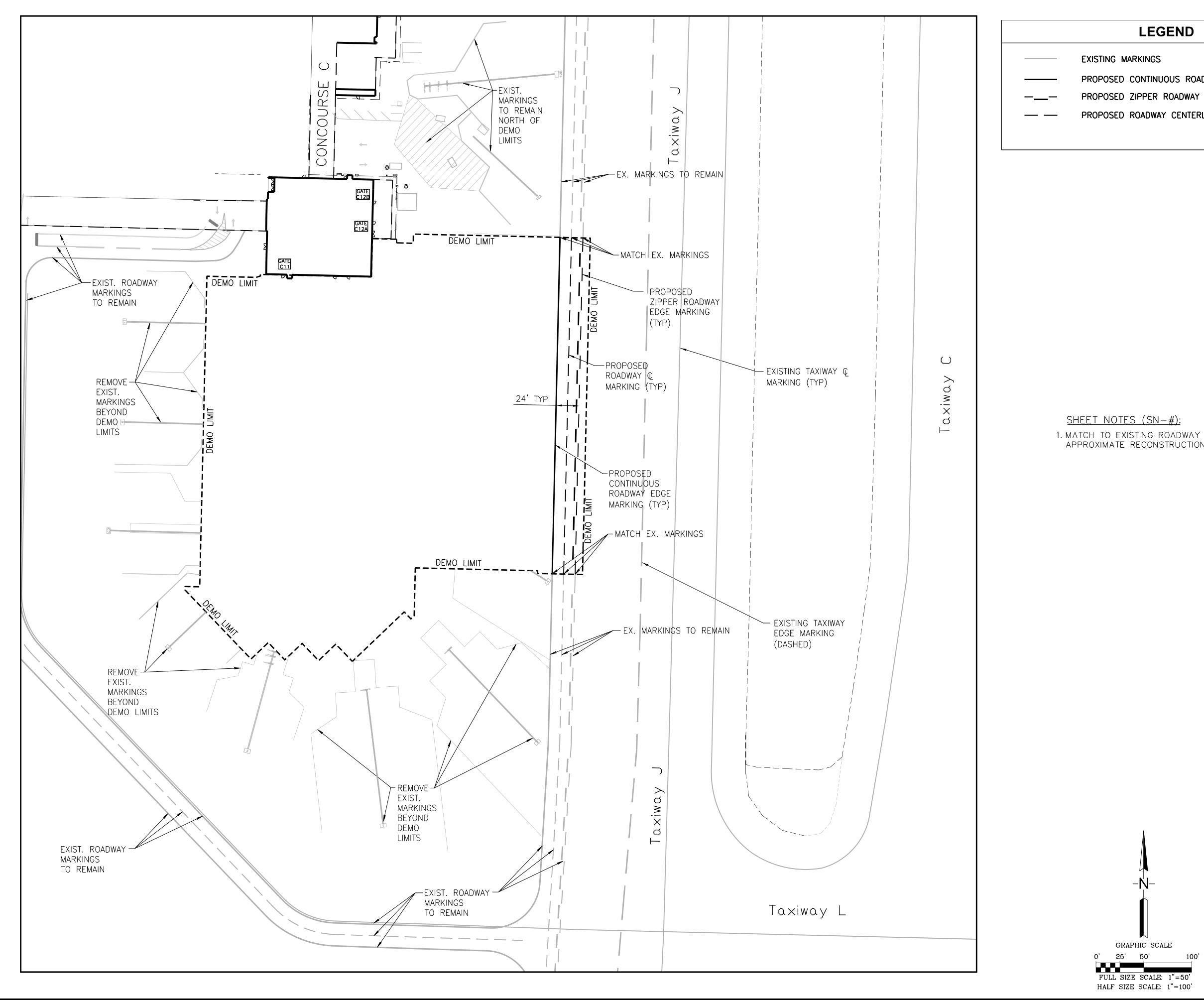


# NOTE: 1. REFER TO SHEET C-HG-07 FOR A SCHEDULE OF ALL EXISTING DOORS AND DOOR TREATMENTS. EXISTING DOORS HAVE TWO NUMBERING SCHEMES AS SHOWN ON THE DOOR SCHEDULE. IF A MSCAA NUMBER DESIGNATION WAS AVAILABLE THEN THAT NUMBER IS SHOWN HEREIN. IF NO MSCAA NUMBER WAS AVAILABLE THEN THE FIELD SURVEY NUMBER IS SHOWN.

	UTILITY LEGEND
	EXIST. JET FUEL VALVE PIT
[]{_}{]}	EXIST. HIGH MAST LIGHT POLE
► 1 ► 1	EXIST. WATER VALVE
	EXIST. DRAIN MANHOLE
FH	PROP. FIRE HYDRANT
	PROP. DRAIN INLET
۲	PROP. DRAIN MANHOLE



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REVISIONS PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO			-	
SHEET T MSCAA NO DWG. FILE DATE 05/31 SCALE	ENLA CIV FLATV PL 13-13 NAME 13136802	VIL NORP AN 68-02 <u>C-CF-01</u> WG NO.	_	
<b>1" =</b>	<b>20'</b> S	HEET NO.		

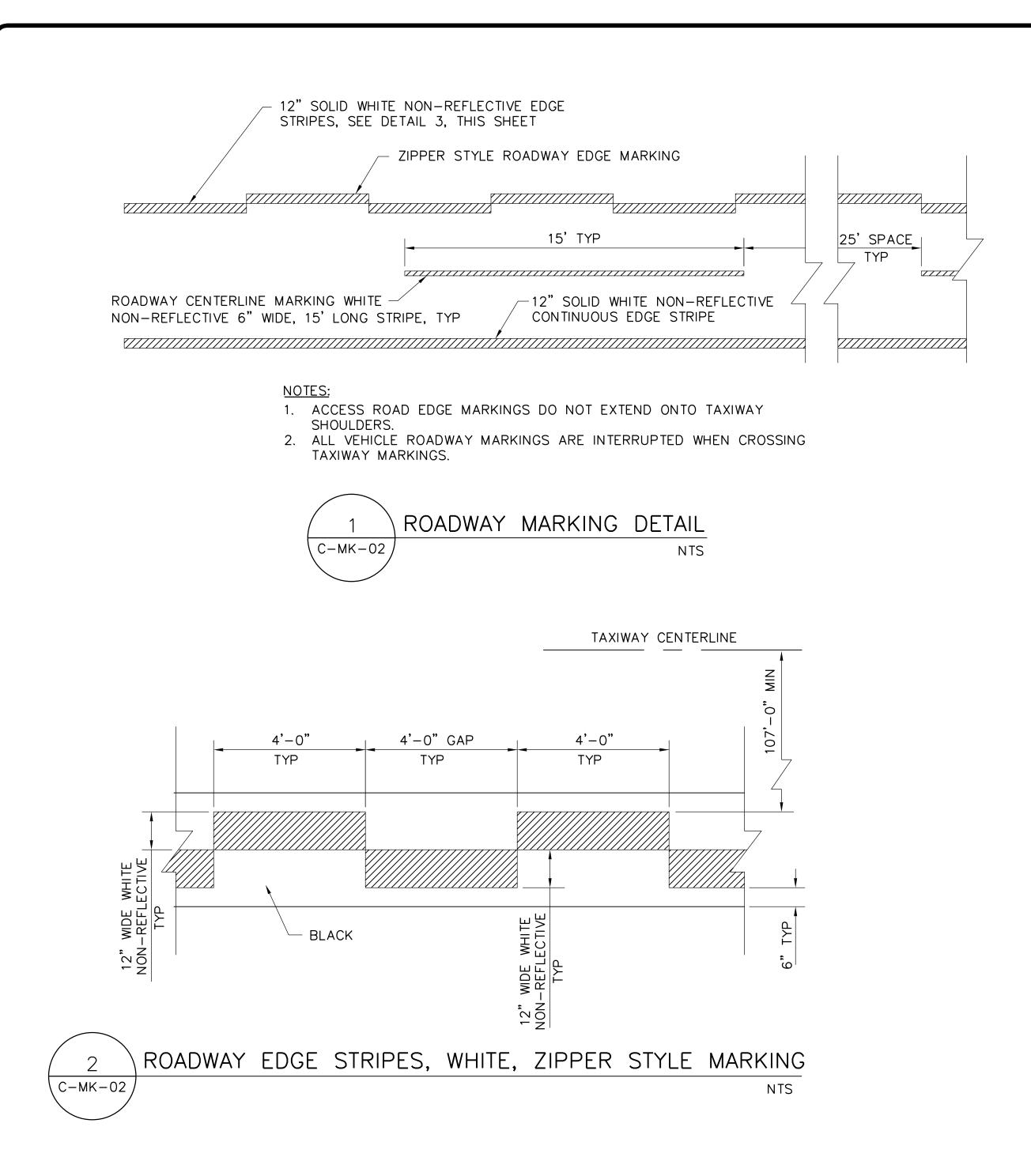


PROPOSED CONTINUOUS ROADWAY EDGE MARKING (WHITE) PROPOSED ZIPPER ROADWAY EDGE MARKING (WHITE) PROPOSED ROADWAY CENTERLINE MARKING (WHITE)

1. MATCH TO EXISTING ROADWAY MARKING AT APPROXIMATE RECONSTRUCTION LIMITS OR AS SHOWN.

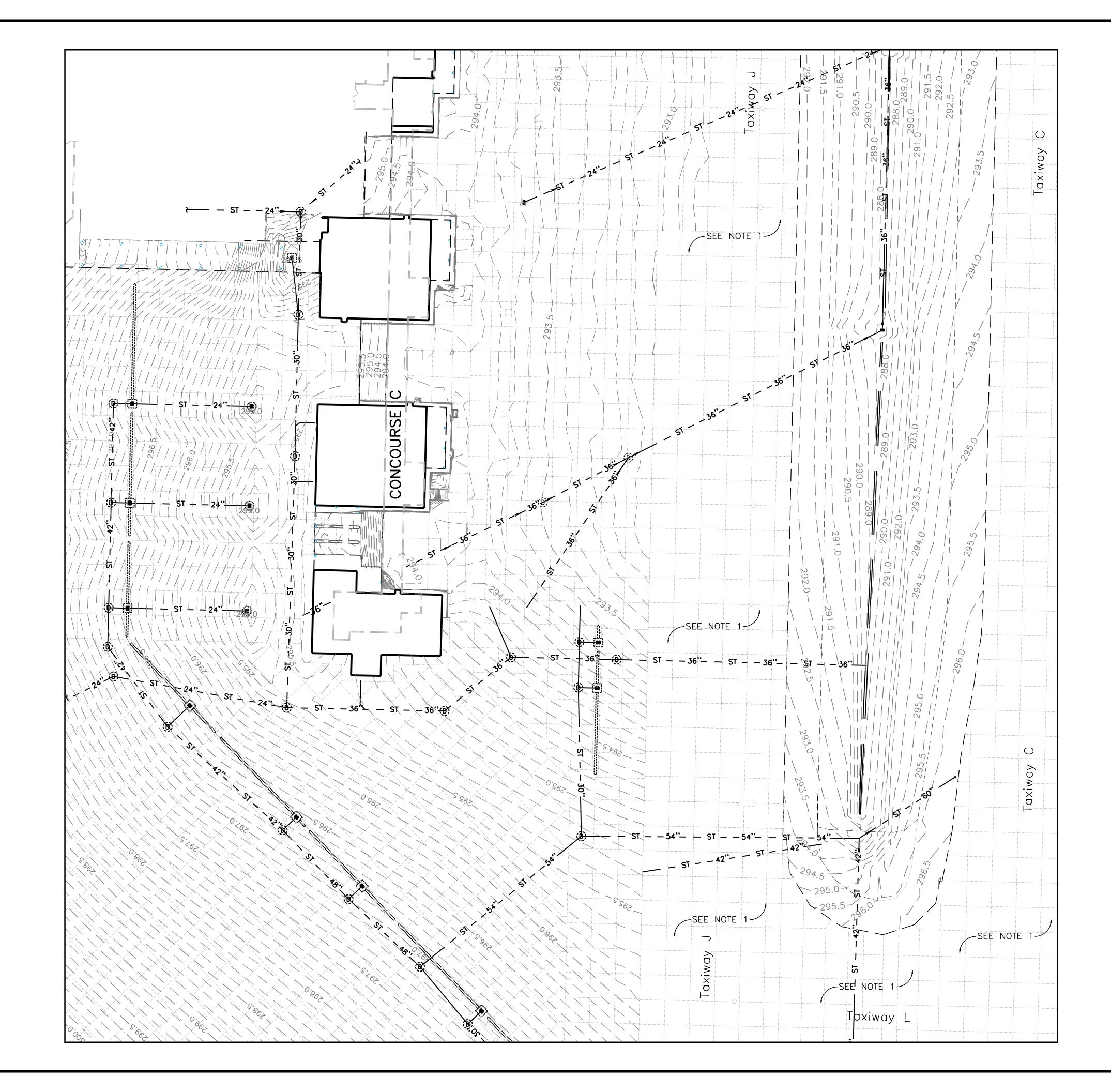
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ARCH associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
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NO. DESCRIPTION DATE BY REVISIONS
PROJECT:
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
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RECONSTRUCTION CONCOURSE C - SOUTH DEMO SHEET TITLE: PAVEMENT MARKING PLAN MSCAA NO. 13-1368-02 DWG. FILE NAME
RECONSTRUCTION CONCOURSE C - SOUTH DEMO SHEET TITLE: PAVEMENT MARKING PLAN MSCAA NO. 13-1368-02



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ARCHITECT Salie-3856 38116-3856 Salie-3				
JOB NO. <b>22030</b>				
DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN				
CONSULTANT				
Pickering Firm Incorporated				
JOB NO. <b>22942.22</b>				
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ISSUED FOR BID/CONSTRUCTION				
NO. DESCRIPTION DATE BY				
REVISIONS PROJECT:				
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO				
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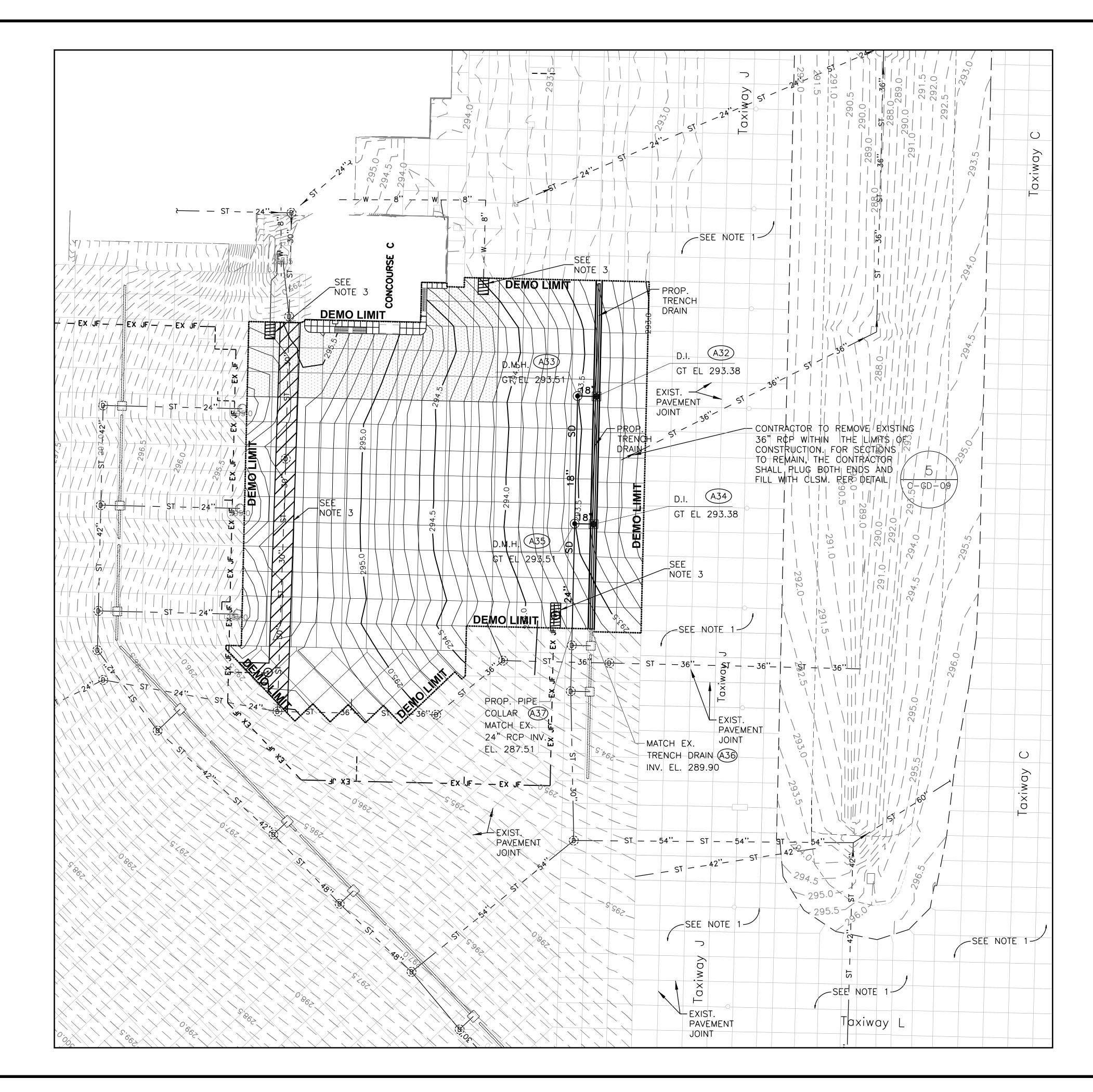
# NOTES:

2. CONTOUR INTERVAL: 0.1 FT PAVED AREAS 0.5 FT TURF AREAS 3. EXISTING DRAINAGE SHOWN FOR INFORMATIONAL PURPOSES ONLY.

1. CONTOURS NOT SHOWN IN THIS AREA BECAUSE THIS AREA IS NOT TO BE DISTURBED BY CONSTRUCTION.

-N- GRAPHIC SCALE	
0' 25' 50' 100'	
FULL SIZE SCALE: 1"=50' HALF SIZE SCALE: 1"=100'	
KEYMAP	

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Ur Assoc 498 SI MEMPHIS, P 901.578.717 WWW	ban states, p.c. OUTH MAIN TENNESSEE 38103 3 / F 901.578.5223 Juarch.com	
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PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO		
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MSCAA NO. 13 DWG. FILE NAME	3-1368-02	
13136 DATE 05/31/22	802_C-EG-01	
SCALE <b>1" = 50'</b>	SHEET NO.	
FACILITY NO.	45 OF 87	



DRAINAGE PROFILE	SHEET NO.
D.M.H. (A33) TO PIPE COLLAR (A37)	C-GD-05
D.I. (A32) TO D.M.H. (A33)	C-GD-05
D.I. (A34) TO D.M.H. (A35)	C-GD-05
TRENCH DRAIN PROFILE	SHEET NO.
TRENCH DRAIN STUB (A36) TO D.I. (A32)	C-GD-06

# GRADING AND DRAINAGE NOTES:

- ALLOWED.
- PROPOSED UTILITIES.

1. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AT ALL TIMES. PONDING OR STORING STORMWATER SHALL NOT BE

2. THE CONTRACTOR SHALL COORDINATE THE CONSTRUCTION OF THE PROPOSED DRAINAGE SYSTEM WITH THE RELOCATION, CONSTRUCTION AND PROTECTION OF ALL OTHER EXISTING AND

3. CONTRACTOR SHALL PROTECT THE EXISTING UNDERGROUND UTILITIES TO REMAIN WITHIN THE LIMIT OF DEMOLITION.

GRAPHIC SCALE
0' 25' 50' 100'
FULL SIZE SCALE: $1^{"}=50'$
HALF SIZE SCALE: 1"=100'
KEYMAP

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	PROPOSED STORM DRAINAGE STRUCTURES									
	LOCATI	ON (1)	D.A.	DESIGN	GRATE	INVERT	STRUCTURE	STRUCTURE	STRUCTURE	DETAIL
NO.	NORTHING	EASTING	(Acres)	Q10 (cfs)	EL. (2)	ELEV.	DEPTH (4)	TYPE ③	SIZE	
(A32)	282752.7507	777735.1420	0.49	3.52	293.38	288.07	5.31	D.I.	4'x4'	1, C-GD-08
(A33)	282753.1910	777716.1471	-	-	293.51	287.91	5.60	D.M.H.	5' DIA.	2A, C-GD-07
(A34)	282624.7851	777732.1760	0.48	3.47	293.38	288.08	5.30	D.I.	4'x4'	1, C-GD-08
(A35)	282625.2254	777713.1811	-	-	293.51	286.92	6.59	D.M.H.	6' DIA.	2A, C-GD-07

	PROPOSED STORM DRAINAGE PIPE								
FROM	INVERT	TO	INVERT	RCP DIA.	SLOPE	LENGTH	DESIGN	CAPACITY	VELOCITY
	ELEV.		ELEV.	(in) (1)	% (3)	(ft)	Q10 (cfs)	Q (cfs)	V (fps) (2)
(A32)	288.07	A33	288.01	18	0.40	15	3.5	7.7	2.0
(A33)	287.91	A35	287.42	18	0.40	123	3.5	7.7	2.0
(A34)	288.08	A35	288.02	18	0.40	14	3.5	7.7	2.0
(A35)	286.92	(A37)	286.46	24	0.40	116	6.8	16.7	2.2

# STORM DRAINAGE STRUCTURE NOTES

- STRUCTURE
- OF STRUCTURE.
- PIPE.

# STORM DRAINAGE PIPE NOTES

- DESIGN Q10.
- (4) FOR PIPE BEDDING AND BACKFILL, SEE DETAIL SHEET C-GD-09.

1 HORIZONTAL MEASURING POINT FOR EACH STRUCTURE TYPE IS AS FOLLOWS: D.M.H. – CENTER OF STRUCTURE

(2) VERTICAL MEASURING POINT FOR EACH STRUCTURE TYPE IS AS FOLLOWS: D.M.H. – FINISH GRADE AT CENTER OF

(3) INSIDE DIMENSIONS OR INSIDE DIAMETER

(4) DEPTH IS MEASURED AS THE VERTICAL DISTANCE FROM FINISHED GRADE AT THE CENTER OF THE STRUCTURE TO THE INVERT ELEVATION OF THE OUTLET STORM

1 ALL STORM DRAINAGE PIPE WILL BE CLASS V RCP.

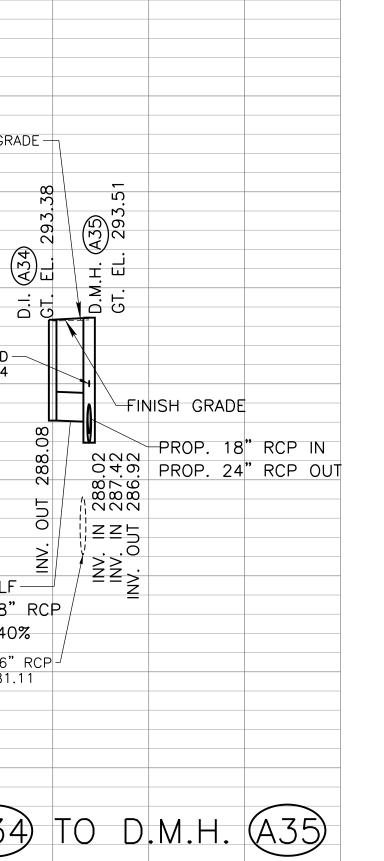
2 VELOCITY SHOWN IS ACTUAL VELOCITY BASED ON

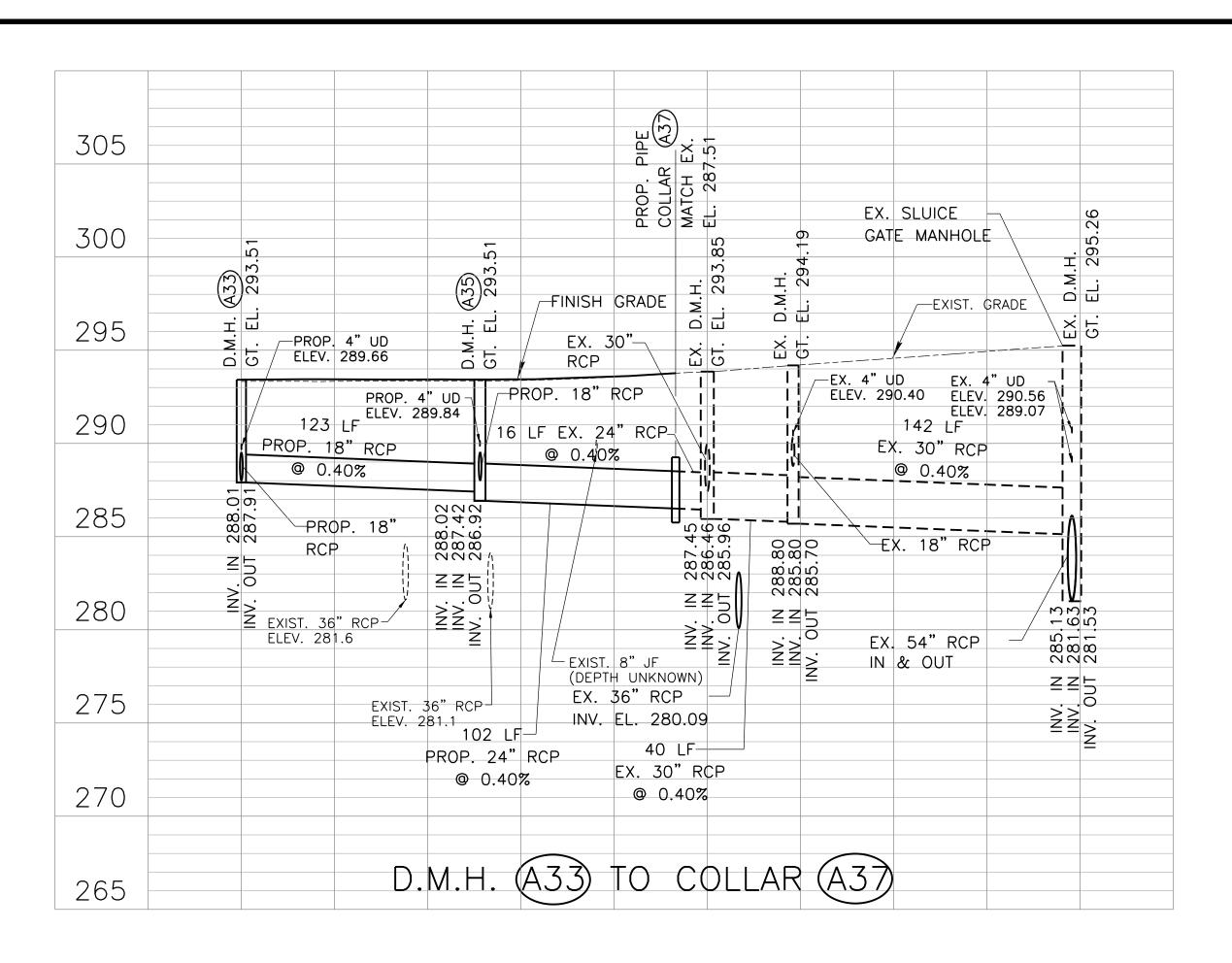
3 SLOPE CALCULATION IS BASED ON ACTUAL LENGTH OF STORM DRAIN PIPE REQUIRED FROM INSIDE FACE OF STRUCTURE TO INSIDE FACE OF STRUCTURE. LENGTHS SHOWN ON TABLE HAVE BEEN ROUNDED TO AN EVEN FOOT FOR QUANTITY ESTIMATION PURPOSES.

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NO. DESCRIPTION DATE BY REVISIONS PROJECT:
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
SHEET TITLE: GRADING AND DRAINAGE TABULATION
MSCAA NO. 13-1368-02 DWG. FILE NAME 13136802_C-GD-02 DATE DWG NO. 05/31/22 SCALE N.T.S. SHEET NO.

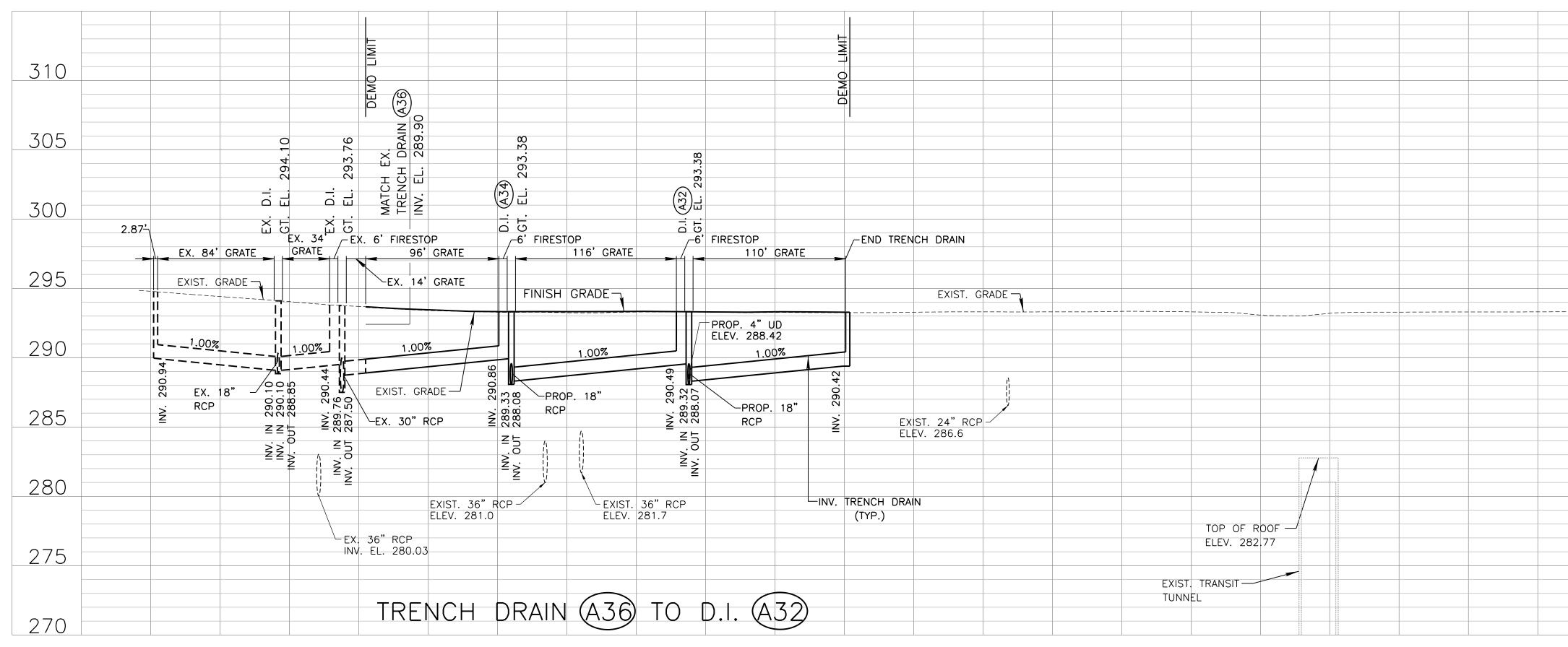
305	Image: second
300	EXIST. GRADE
295	EL. 293.
290	FINISH GRADE
285	PROP. 4" UD       PROP. 4" UD         ELEV. 288.42       ELEV. 289.66         O. O. O.       PROP. 18"         RCP       000         0000       000         0000       000         0000       000         0000       000         0000       000         0000       000         0000       000         0000       000
280	NV. OUT 2 NV. IN 2 NV 2 NV 2 NV 2 NV 2 NV 2 NV 2 NV 2 N
275	15 LF PROP. 18" RCP @ 0.40%
270	Image: selection of the selection
265	D.I. (A32) TO D.M.H. (A33)

305	
300	EXIST. GRADE –
295	FL 293.38
290	PROP. 4" UD ELEV. 289.84
285	28.08 28.08
280	NV. OUT
275	14 LF PROP. 18" RCI @ 0.40% EXIST. 36" RCP ELEV. 281.11
270	ELEV. 281.11
265	D.I. (A34)

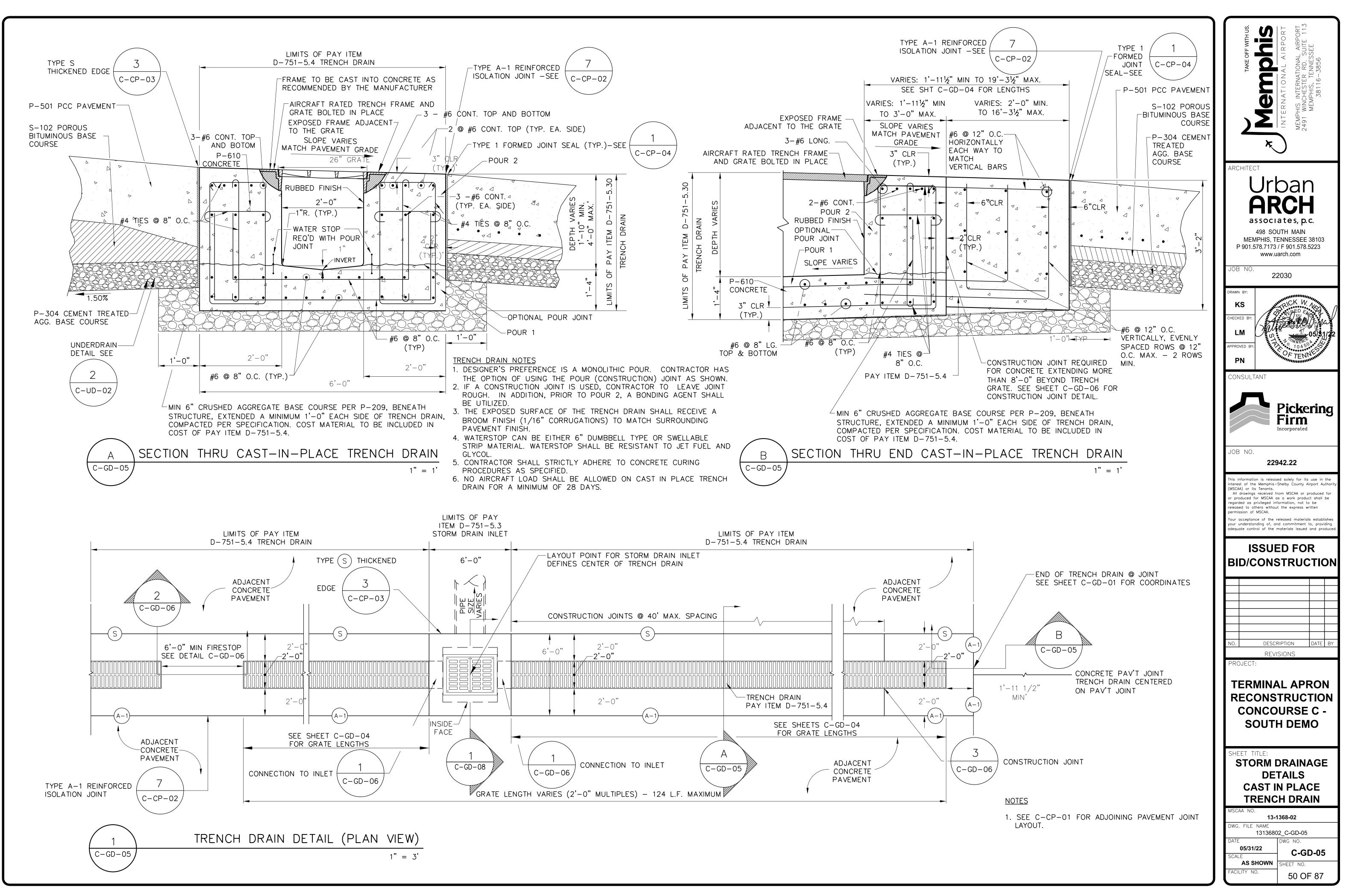




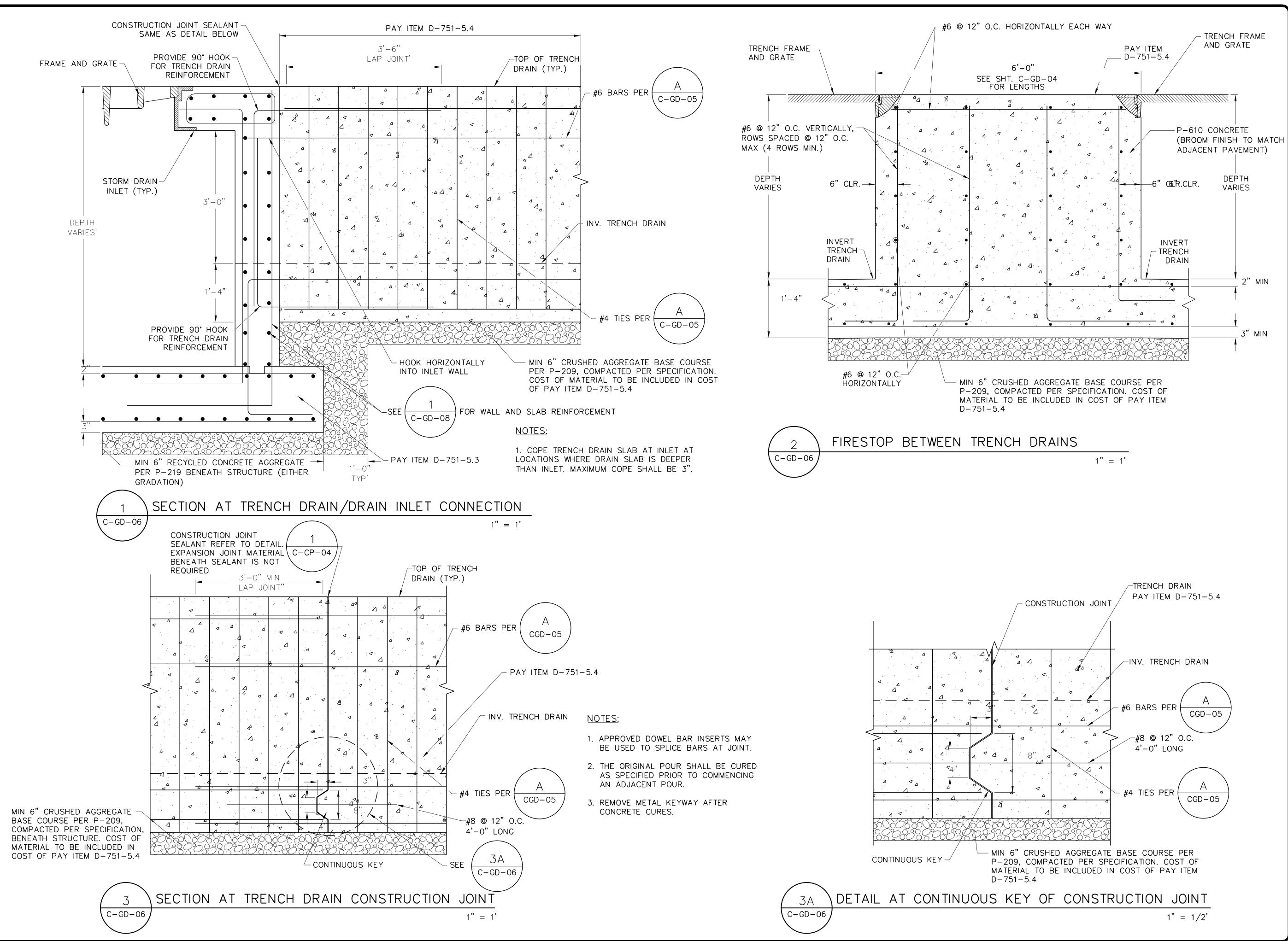
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TERMINAL APRON RECONSTRUCTION CONCOURSE C -
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO SHEET TITLE: STORM DRAINAGE PROFILES




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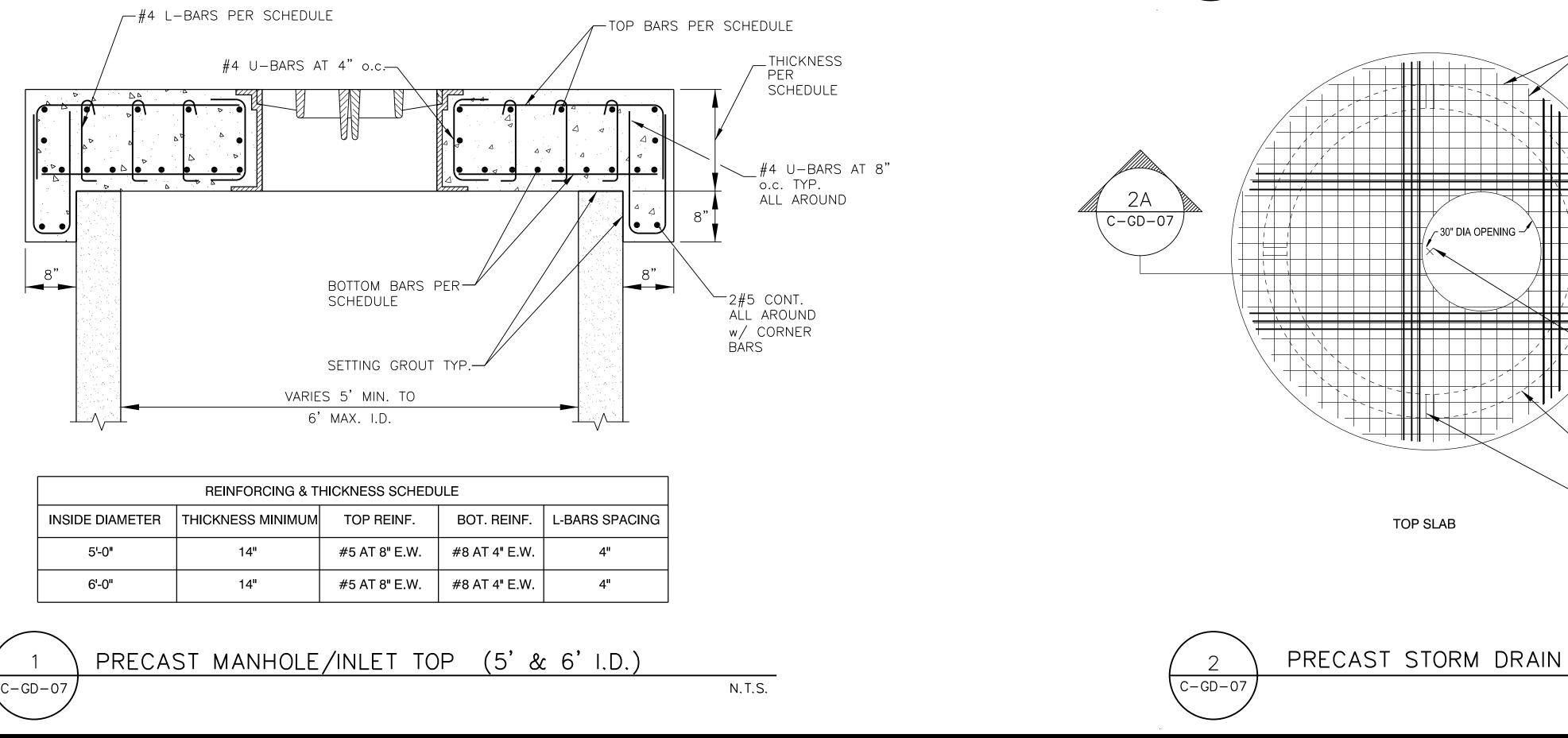


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- 1. THE PRECAST MANHOLE SHALL BE DESIGNED BASED ON THE FOLLOWING: -AT-REST LATERIAL EARTH PRESSURE : 100 PCF
  - -DESIGN AIRCRAFT : DUAL 50,000 #/WHEEL: DUAL TANDEM 75,000 #/WHEEL -LIVE LOAD ON COVER : 250 PSF
  - -MAXIMUM DEPTH : 25 FEET
  - -WHEEL LOADS SHOULD BE BASED ON THE DESIGN AIRCRAFT WHEEL LOADS ASSUMING 95 PERCENT OF THE TAXI WEIGHT IS DISTRIBUTED TO THE REAR LANDING GEAR
  - -LATERAL EARTH LOADS FROM WHEEL LOADS ADJACENT TO THE STORM DRAIN MANHOLE SHOULD BE CALCULATED USING NAVFAC DM-7.2 FIGURE 11. POSITION THE WHEELS TO OBTAIN THE MAXIMUM FORCES IN THE STORM DRAIN MANHOLE
  - -DESIGN OF THE STORM DRAIN MANHOLE SHALL CONSIDER AN AIRCRAFT BRAKING FORCE EQUIVALENT TO 0.7 G
  - -THE STORM DRAIN MANHOLE DESIGN SHALL TAKE INTO CONSIDERATION ANY JOINTS IN THE STRUCTURE. PROPER CONSIDERATION SUPPORTED BY CALCULATIONS, SHALL BE GIVEN TO THE LOAD TRANSFER ACROSS ANY JOINTS -REINFORCED CONCRETE DESIGN MAY BE BASED ON ULTIMATE STRENGTH PROCEDURES AS DEFINED IN ACI 318-05. CRACK CONTROL SHALL BE CONSIDERED IN THE DESIGN OF THE STRUCTURE BY LIMITING THE VALUE OF "Z". AS DEFINED IN PARAGRAPH 10.6.4 OF ACI 318-05
- 2. CAST-IN-PLACE CONCRETE MANHOLE WILL BE CONSIDERED AS AN ALTERNATE TO THE PRECAST MANHOLE SHOWN, PROVIDED IT IS DESIGNED BASED ON THE ABOVE REQUIREMENTS.

- THE PAVEMENT SURFACE.
- THE OPENING.

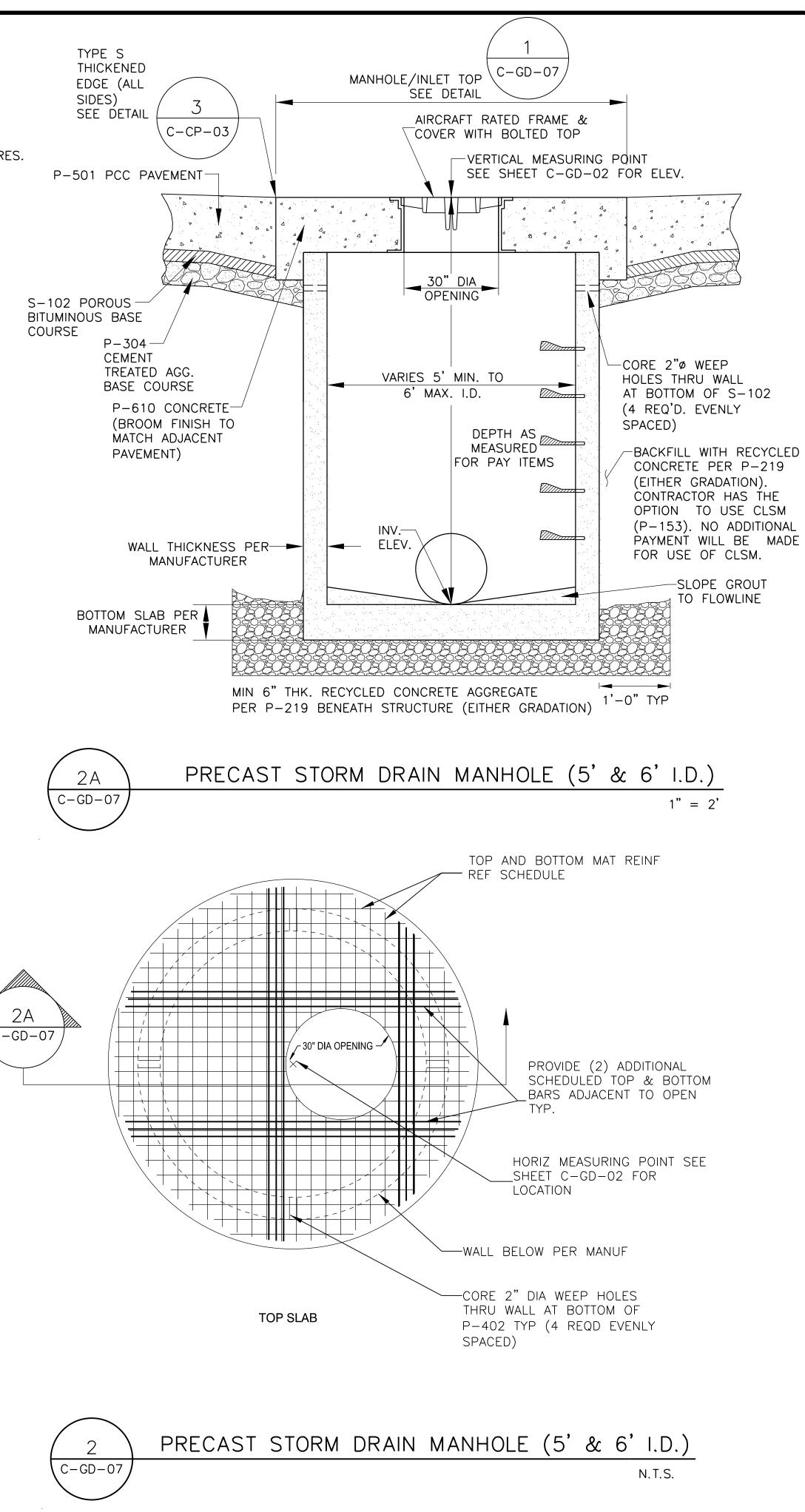
- D-751-5.2.
- CRITERIA LISTED ABOVE.

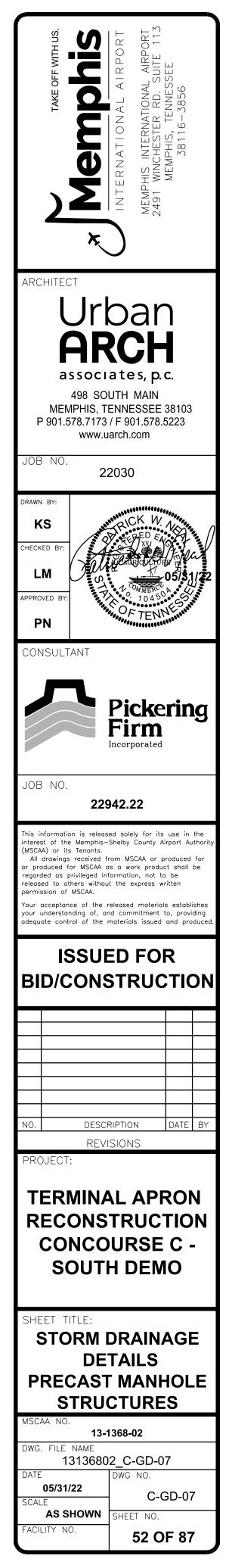


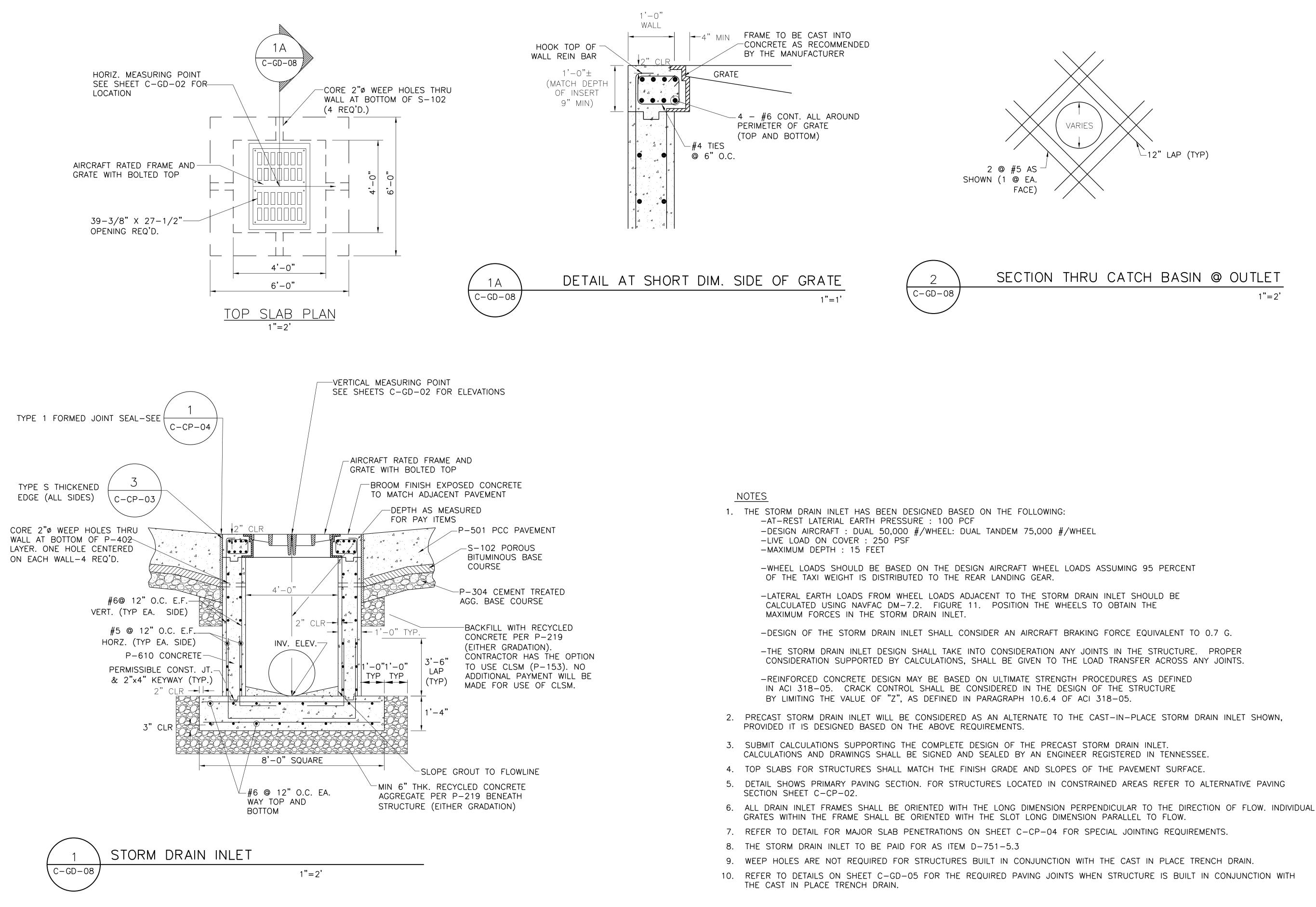
	<b>REINFORCING &amp; T</b>	HICKNESS SCHEDU	JLE	
INSIDE DIAMETER	THICKNESS MINIMUM	TOP REINF.	BOT. REINF.	L-BARS SPAC
5'-0"	14"	#5 AT 8" E.W.	#8 AT 4" E.W.	4"
6'-0"	14"	#5 AT 8" E.W.	#8 AT 4" E.W.	4"



3. SUBMIT CALCULATIONS SUPPORTING THE COMPLETE DESIGN OF THE PRECAST CONCRETE MANHOLE (OR CAST-IN-PLACE). CALCULATIONS AND DRAWINGS SHALL BE SIGNED AND SEALED BY AN ENGINEER REGISTERED IN TENNESSEE. SIDES) 4. TOP SLABS FOR STRUCTURES SHALL MATCH THE FINISH GRADE AND SLOPES OF 5. DRAIN MANHOLE COVERS IN THE PROXIMITY OF PASSENGER BOARDING BRIDGES (PBB) SHALL HAVE REASONABLY SMOOTH COVERS TO PREVENT DAMAGE TO PBB TIRES. 6. WALL THICKNESS OF PRECAST STRUCTURES SHALL BE SUFFICIENT TO PROVIDE THE MINIMUM DISTANCE SHOWN FROM THE EDGE OF THE STRUCTURE TO THE EDGE OF 7. DETAIL SHOWS PRIMARY PAVING SECTION. FOR STRUCTURES LOCATED IN CONSTRAINED AREAS, REFER TO ALTERNATE PAVING SECTION SHEET C-CP-02. 8. REFER TO DETAIL FOR MAJOR SLAB PENETRATIONS ON SHEET C-CP-04 FOR SPECIAL JIONTING REQUIREMENTS. 9. PRECAST STORM DRAIN MANHOLES TO BE PAID FOR AS ITEMS D-751-5.1 & 10. WEEP HOLES ARE NOT REQUIRED FOR STRUCTURES BUILT IN GRASSED AREAS. COURSE 11.STRUCTURES WITH DEPTHS INSUFFICIENT TO PRECAST SHALL BE BUILT USING CAST-IN-PLACE METHODS IN ACCORDANCE WITH THE DESIGN



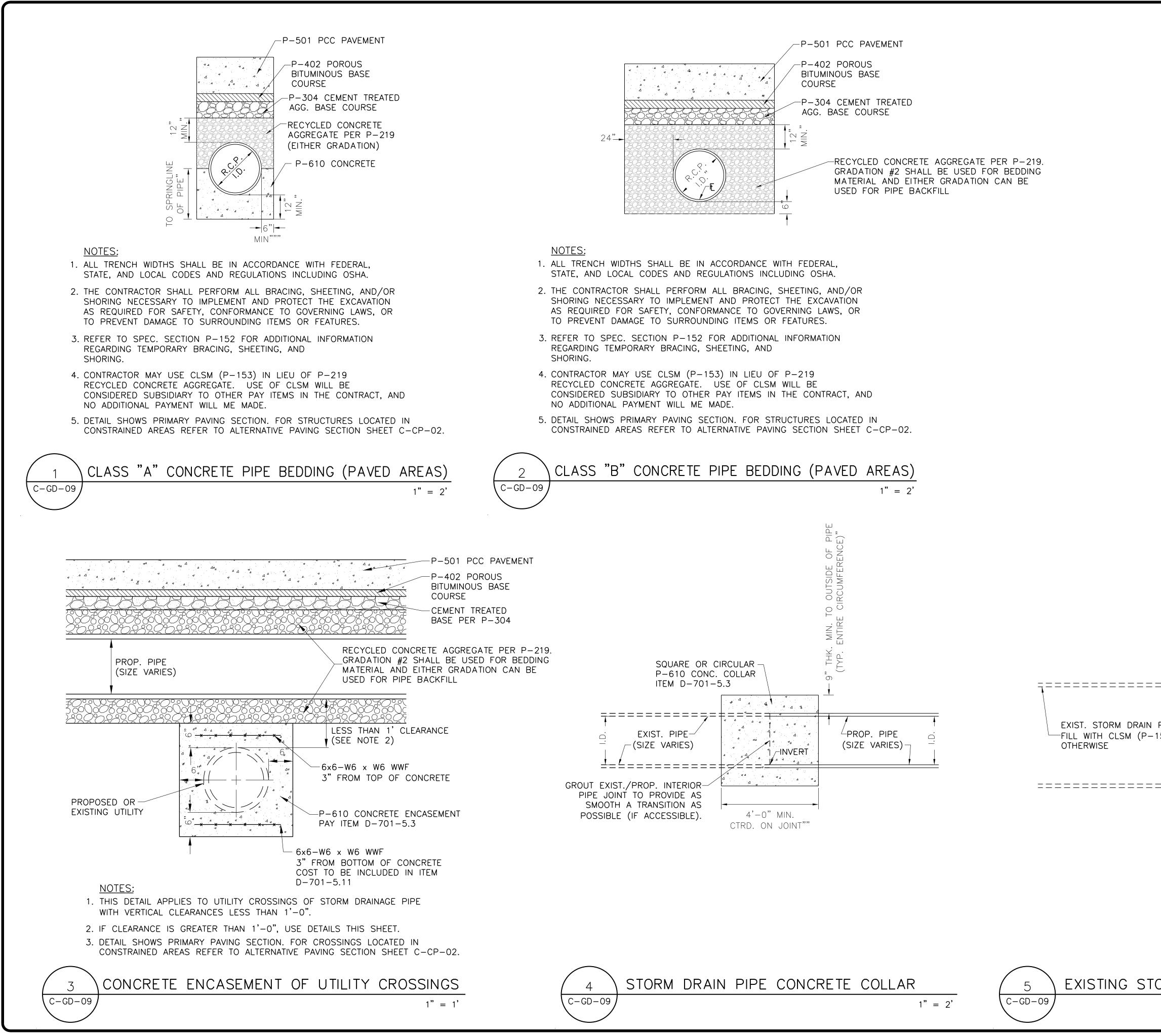




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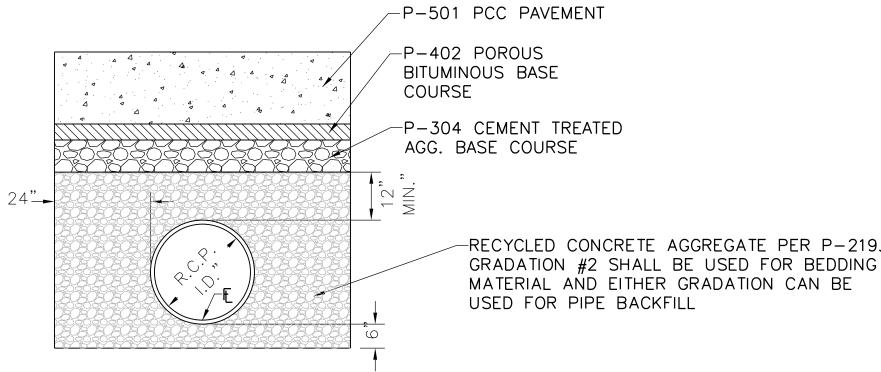
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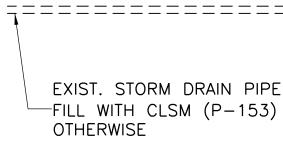
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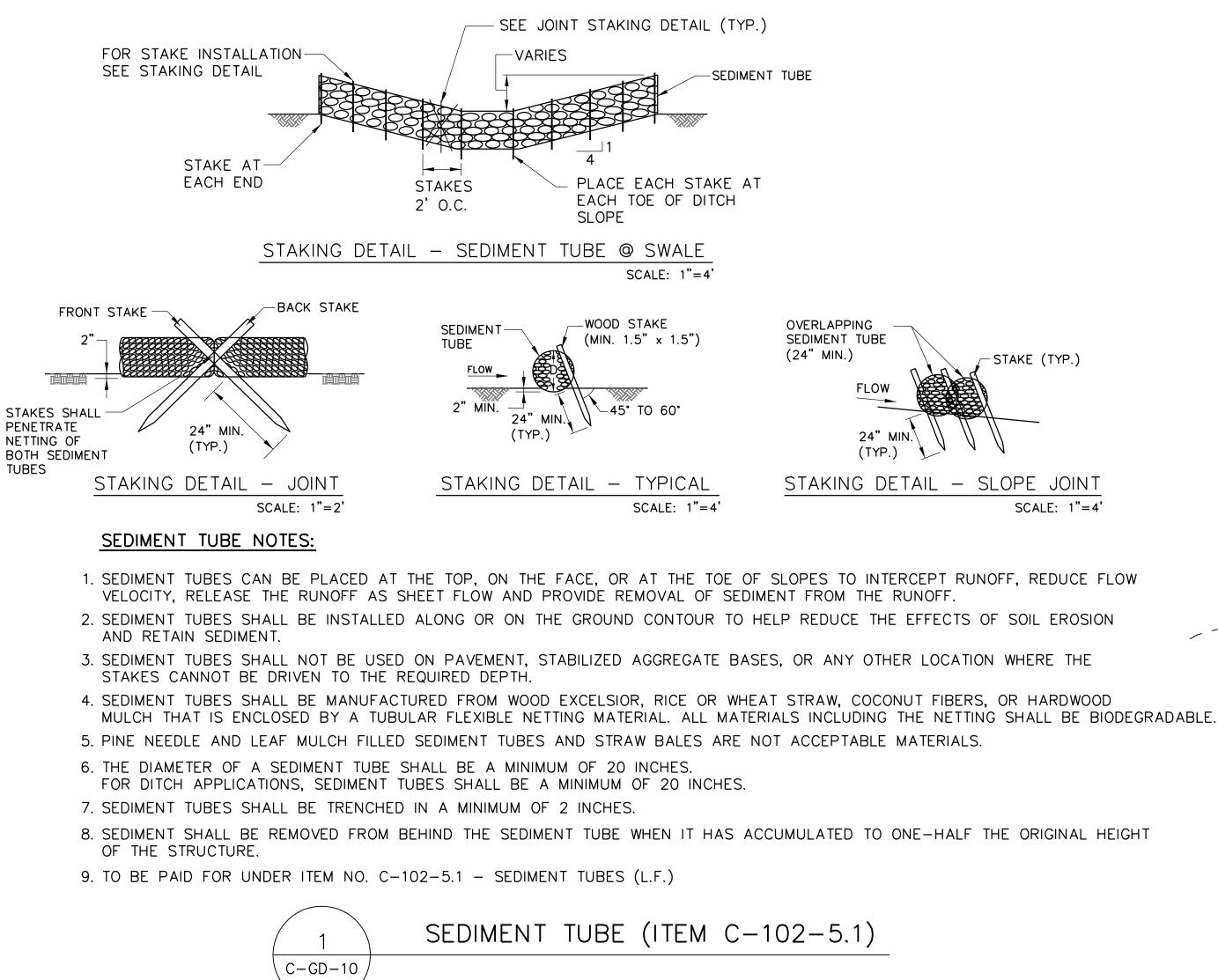


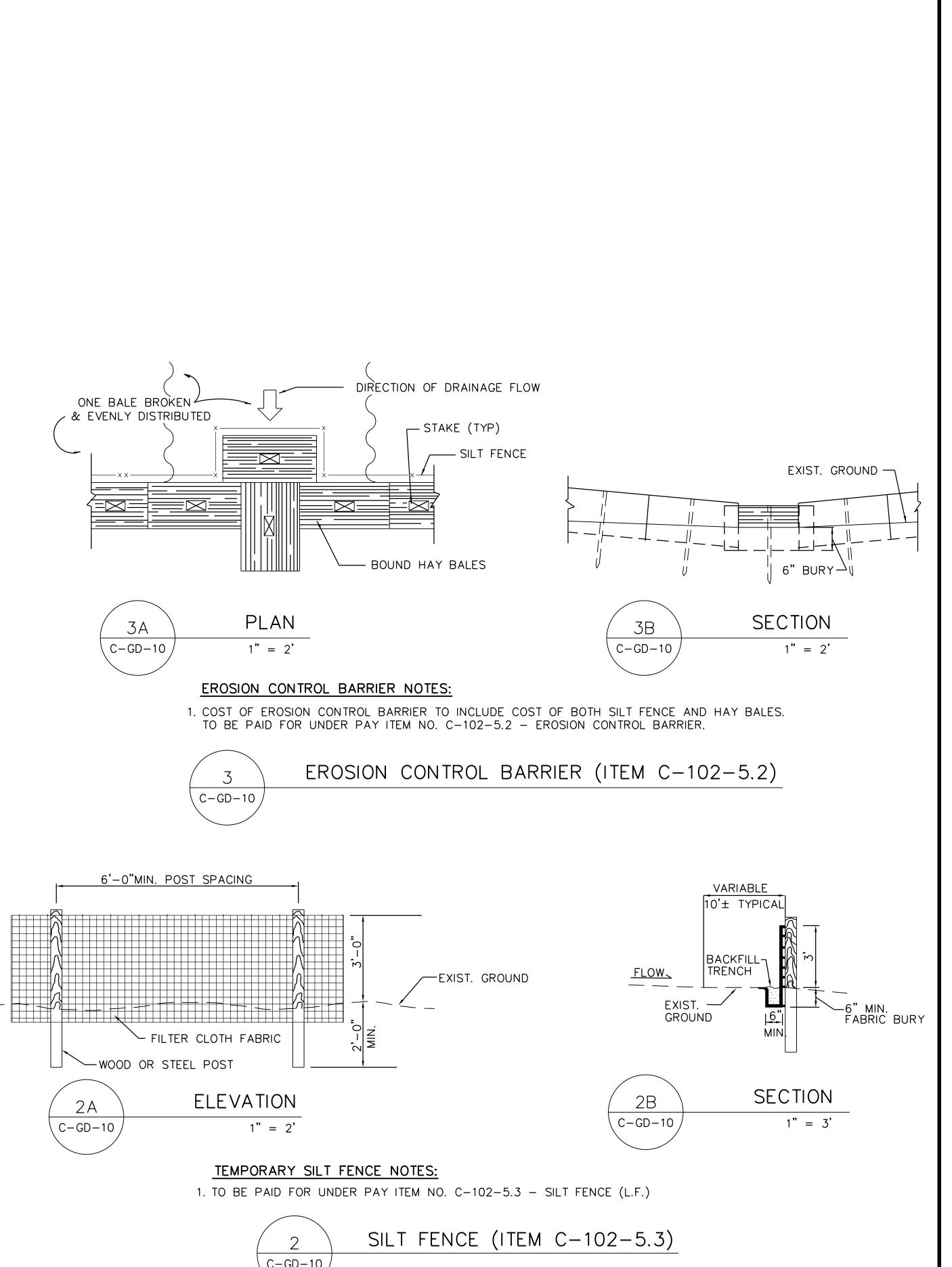


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	ARCHITECT Urban ARCHITECT Urban ARCHITECT ASSOCIATES, P.C. A98 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
	22030 DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN
	CONSULTANT Pickering Pickering Firm Incorporated JOB NO. 22942.22
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CONSTRUCT BRICK	<b>BID/CONSTRUCTION</b>
PIPE TO BE ABANDONED.	
53) UNLESS DIRECTED	NO. DESCRIPTION DATE BY REVISIONS
1'-0" (MIN.) THICK P-610 CONC. PLUG ITEM D-701-5.3	PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
	SHEET TITLE: STORM DRAINAGE DETAILS PIPE BEDDING, PLUGS, COLLARS, AND ENCASEMENT
	13-1368-02 DWG. FILE NAME 13136802_C-GD-09
DRM DRAIN PLUG $1' = 1'$	DATE DWG NO. 05/31/22 SCALE AS SHOWN
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EROSION CONTROL GENERAL NOTES:

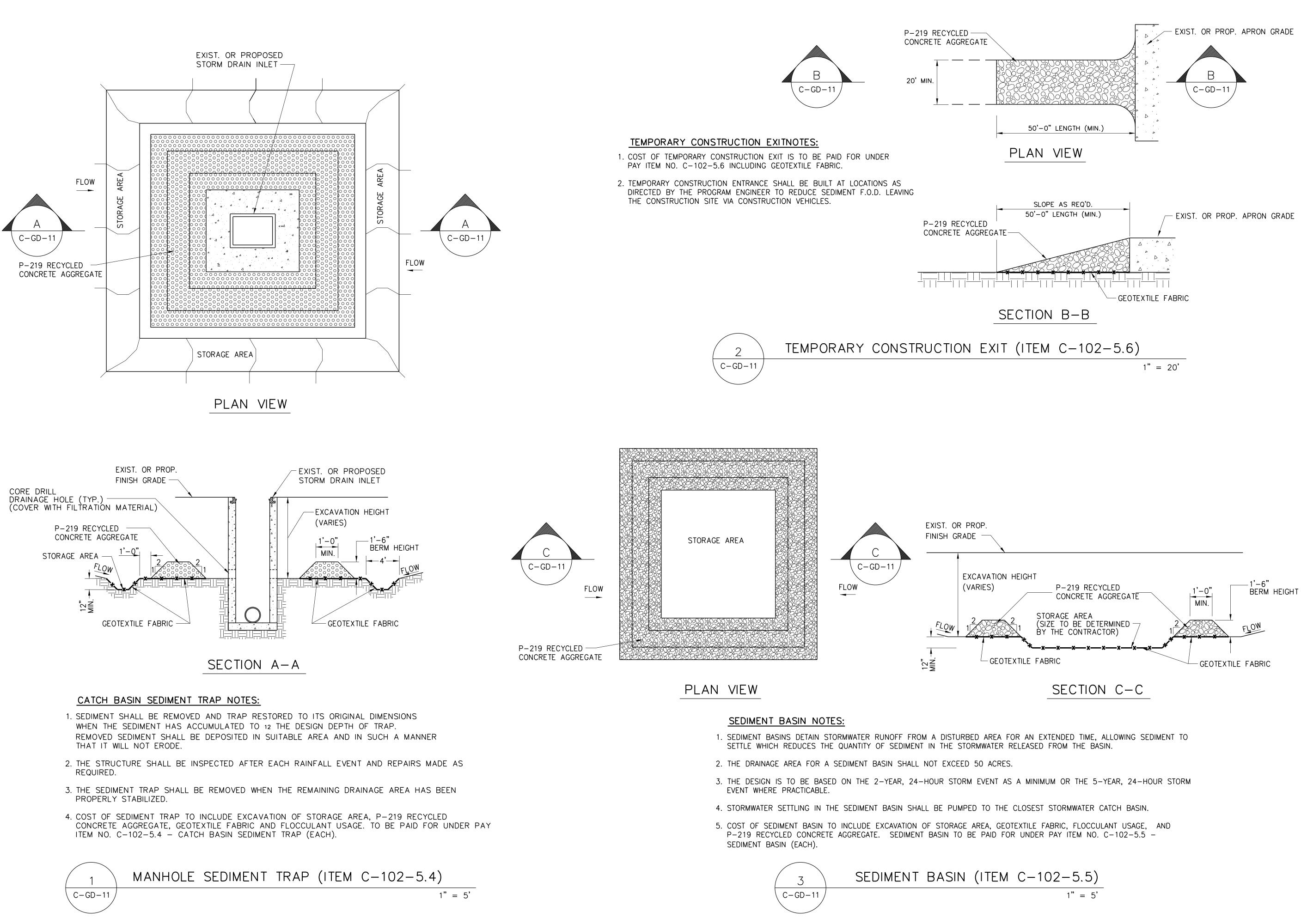
- 1. SIGNIFICANT PONDING OF STORMWATER WILL NOT BE ALLOWED DURING ANY PHASE OR SUB-PHASE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES, INCLUDING ANY NECESSARY STORMWATER PUMPING FACILITIES. ALL COSTS ASSOCIATED WITH PROVISION AND MAINTENANCE OF POSITIVE DRAINAGE FACILITIES WILL BE INCLUDED IN THE COST OF OTHER ITEMS AND WILL NOT BE MEASURED AND PAID FOR SEPARATELY.
- 2. POSITIVE DRAINAGE MUST BE MAINTAINED AT ALL TIMES AWAY FROM THE CONCOURSE UTILITY TUNNELS AS WELL AS THE CONCOURSE BUILDINGS.
- 3. EROSION CONTROL MEASURES SHALL BE MODIFIED AS NECESSARY IN COORDINATION WITH THE PROGRAM MANAGER IN ORDER THAT SAID MEASURES ARE EFFECTIVE AT ALL TIMES THROUGHOUT THE COURSE OF THE PROJECT.
- 4. WATER PUMPED FROM WORK AREAS AND EXCAVATION MUST BE HELD IN SEDIMENT BASINS AND CHEMICALLY TREATED BY POLYACRYLAMIDE (PAM) PRIOR TO ITS DISCHARGE INTO EXISTING OR PROPOSED STORM DRAINAGE SYSTEMS OR PIPES. COST OF CHEMICAL TREATMENT TO BE INCLUDED IN THE COST OF THE SPECIFIC EROSION CONTROL FEATURE, AND WILL NOT BE MEASURED AND PAID FOR SEPARATELY.
- 5. INSPECTION, REPAIR, AND MAINTENANCE OF EROSION CONTROL MEASURES/STRUCTURES IS TO BE PERFORMED DAILY. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE CARE TO ENSURE THAT STRUCTURAL COMPONENTS OF EROSION CONTROL MEASURES ARE NOT DAMAGED. COST OF SEDIMENT CONTROL TO BE INCLUDED IN THE COST OF THE SPECIFIC EROSION CONTROL FEATURE, AND WILL NOT BE MEASURED AND PAID FOR SEPARATELY.
- 6. ALL EROSION CONTROL ITEMS TO BE SECURELY STAKED OR OTHERWISE SECURED TO PREVENT FOREIGN OBJECT DEBRIS (FOD) ON ADJACENT PARTS OF THE APRON THAT ARE ACTIVE.
- 7. THE CONTRACTOR SHALL PROVIDE TREATMENT FOR THE 2-YEAR, 24-HOUR STORM EVENT AS A MINIMUM AND THE 5-YEAR, 24-HOUR STORM EVENT WHERE PRACTICABLE. THE CONTRACTOR WILL BE REQUIRED TO PROVIDE ON-SITE STORAGE OF THE DESIGN STORM VOLUME ONLY IF A VAILABLE ALTERNATIVE SEDIMENT CONTROL MEASURES ARE INEFFECTIVE AT PROVIDING THE REQUIRED TREATMENT OF STORM WATER PRIOR TO DISCHARGE TO THE RECEIVING WATERS.





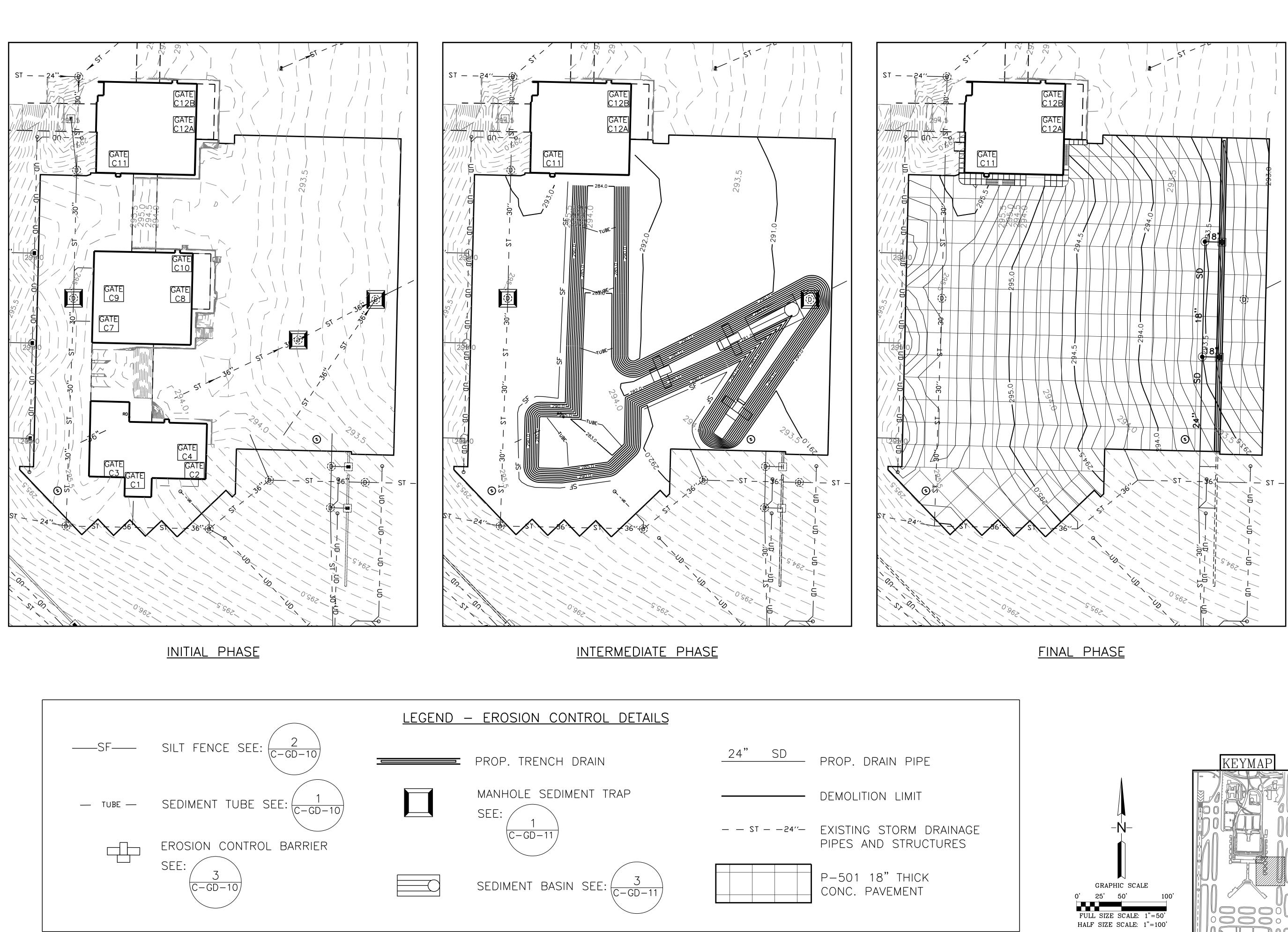
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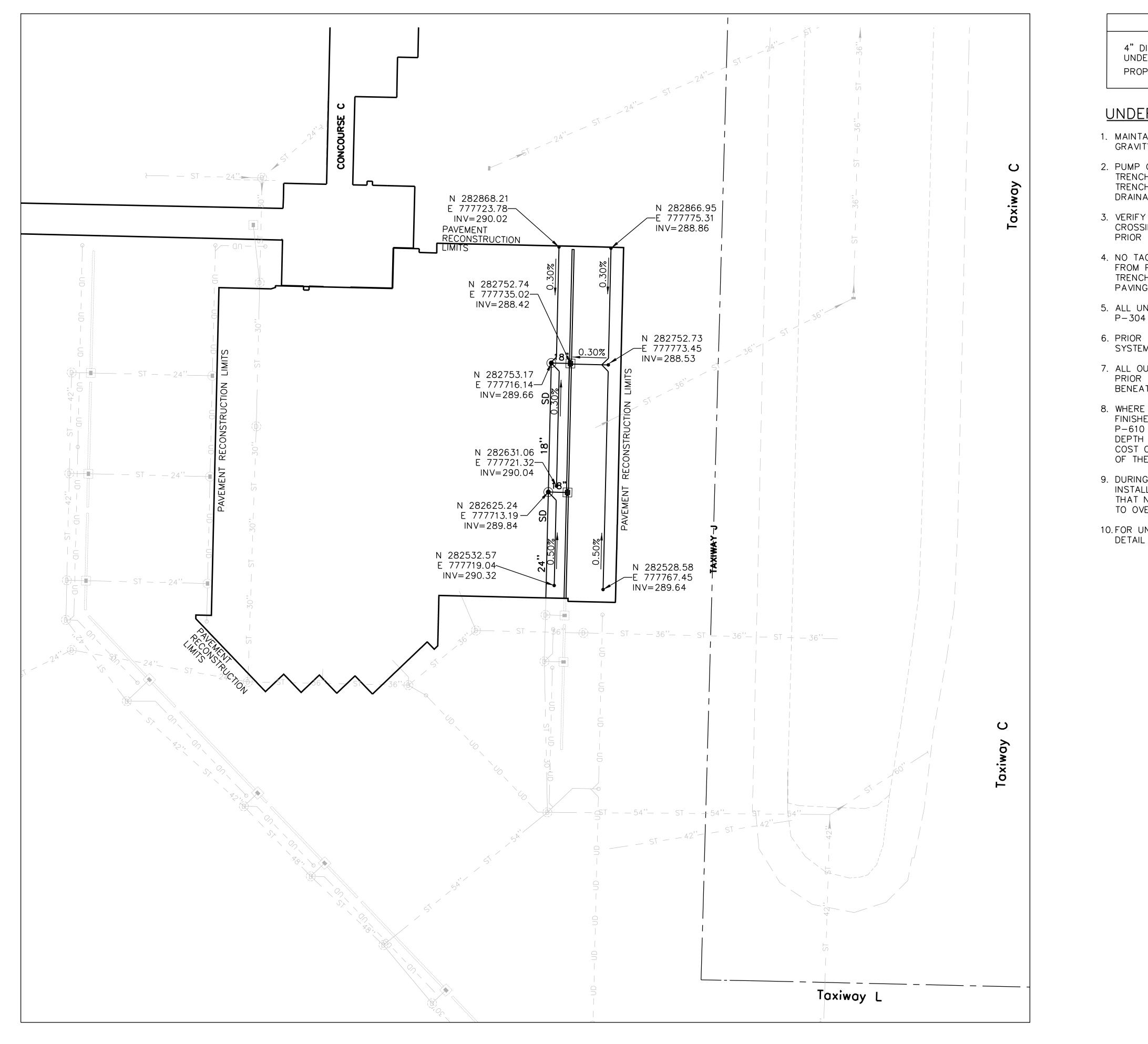
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ARCHITECT Urban associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN CONSULTANT
JOB NO. 22942.22
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<b>BID/CONSTRUCTION</b>
NO.     DESCRIPTION     DATE     BY
PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
SHEET TITLE: EROSION CONTROL
MSCAA NO. 13-1368-02 DWG. FILE NAME 13136802_C-GD-12 DATE DWG NO. 05/31/22 SCALE 1" = 50' SHEET NO.



<u> </u>	G	EI	N	<u>D</u>

4" DIAMETER PERFORATED UNDERDRAIN PIPE PROPOSED UNDERDRAIN CLEAN OUT

# UNDERDRAIN NOTES

1. MAINTAIN GRADE CONTROL TO ENSURE POSITIVE GRAVITY DRAINAGE AT ALL LOCATIONS.

2. PUMP OR BALE OUT ACCUMULATED WATER IN UNDERDRAIN TRENCHES. NO STANDING WATER WILL BE ALLOWED IN TRENCHES; DURING CONSTRUCTION, ENSURE POSITIVE DRAINAGE OUTFALLS ARE AVAILABLE FROM ALL TRENCHES.

•

3. VERIFY ELEVATIONS OF ALL EXISTING AND PROPOSED UTILITY CROSSINGS PRIOR TO CONSTRUCTION. RESOLVE CONFLICTS PRIOR TO CONSTRUCTION OF EACH OUTLET SYSTEM.

4. NO TACK COAT, PRIME COAT OR BITUMINOUS CURE COAT FROM PAVING OPERATIONS WILL BE ALLOWED ON TOP OF THE TRENCH. PROTECT UNDERDRAIN TRENCH DURING PAVING CONSTRUCTION.

 ALL UNDERDRAIN WORK SHALL BE COMPLETED PRIOR TO P-304 CEMENT TREATED AGGREGATE BASE CONSTRUCTION.

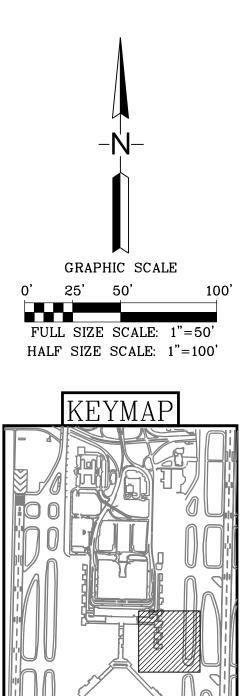
6. PRIOR TO CONSTRUCTION, STAKE OUT THE UNDERDRAIN SYSTEM IN THE FIELD FOR PROGRAM MANAGER APPROVAL.

7. ALL OUTFALL UNDERDRAIN PIPES SHALL BE CONSTRUCTED PRIOR TO THE ADJACENT UNDERDRAIN PIPE NETWORK BENEATH PAVEMENTS.

8. WHERE CLEARANCE FROM TOP OF UNDERDRAIN PIPE TO FINISHED GRADE IS LESS THAN 2 FEET, SURROUND WITH P-610 STRUCTURAL CONCRETE TO THE REQUIRED BEDDING DEPTH AND WIDTH AND TO 6 INCHES OVER TOP OF PIPE. COST OF ENCASEMENT SHALL BE INCIDENTAL TO THE COST OF THE OUTFALL PIPE.

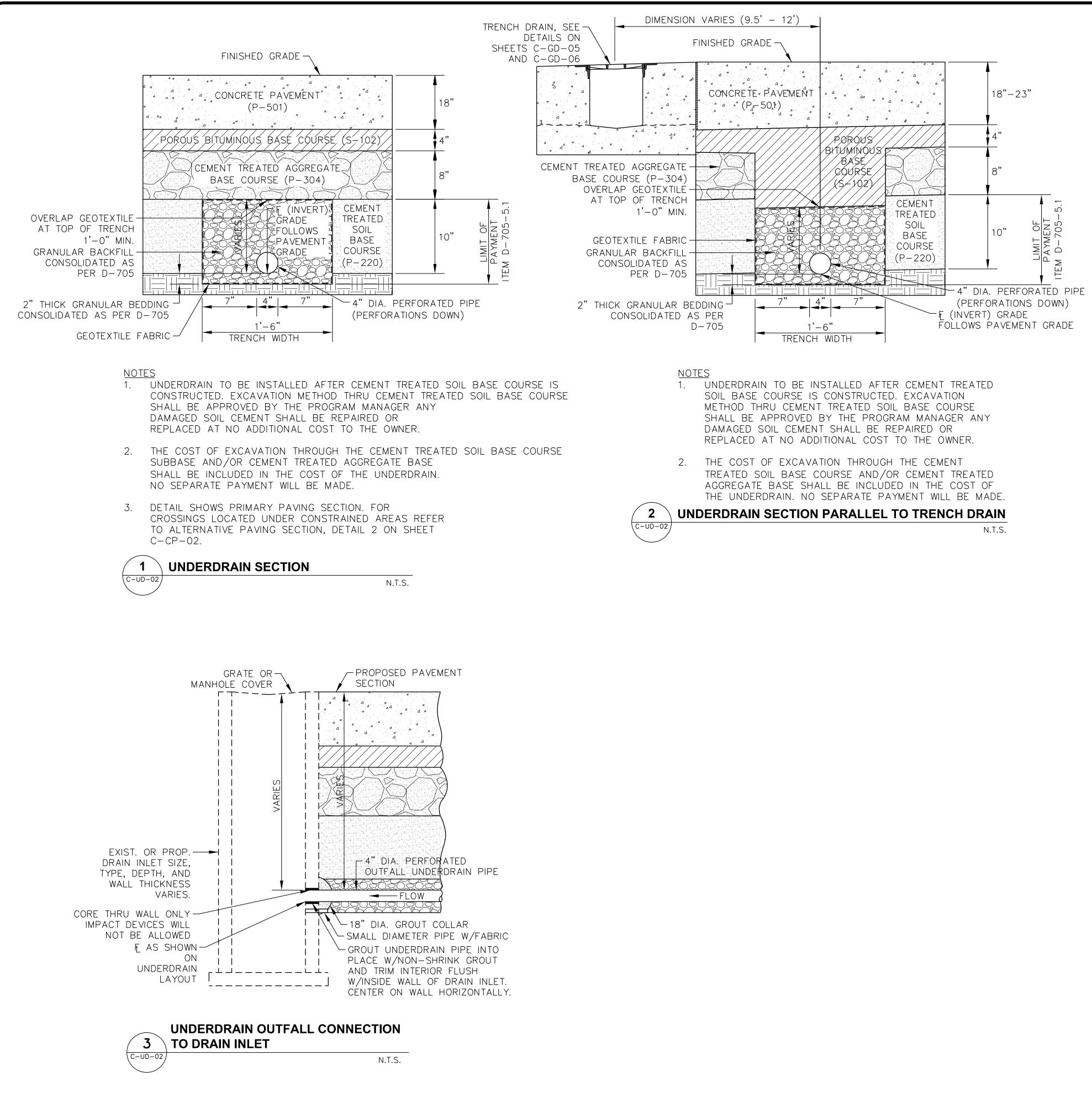
9. DURING AND AFTER UNDERDRAIN PIPE AND BACKFILL INSTALLATION, PROTECT THE UNDERDRAIN TRENCH SUCH THAT NO FINES OR EARTH MATERIAL ENTER SAME PRIOR TO OVERLYING AGGREGATE BASE COURSE CONSTRUCTION.

10. FOR UNDERDRAIN TIES TO DRAINAGE STRUCTURES SEE DETAIL 3, ON SHEET C-UD-02.



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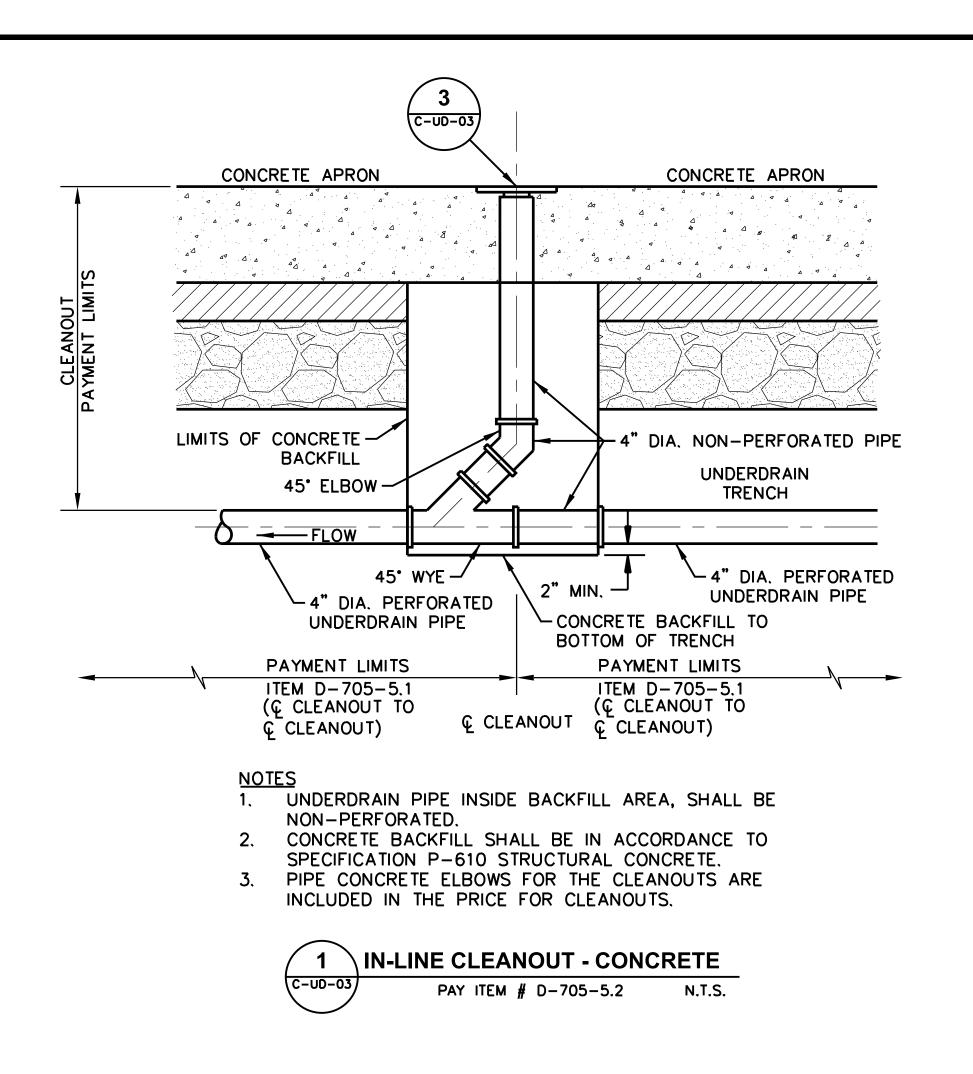
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CONSULTANT Pickering Pickering Incorporated
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ISSUED FOR BID/CONSTRUCTION
NO. DESCRIPTION DATE BY REVISIONS PROJECT:
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
SHEET TITLE: PAVEMENT UNDERDRAIN PLAN
MSCAA NO. <b>13-1368-02</b> DWG. FILE NAME <b>13136802 C-UD-01</b>

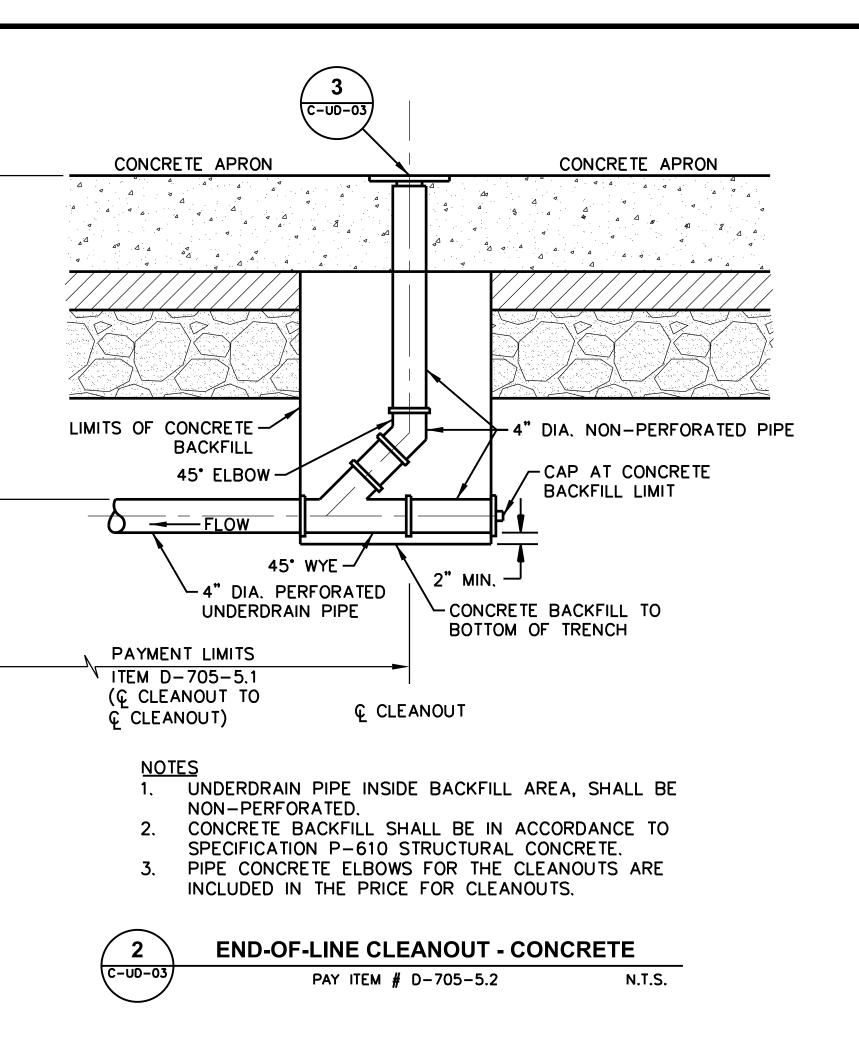


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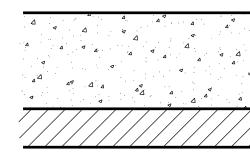
2	UNDERDRAIN SECTION PARALLEL TO TRENCH DRAIN
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DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN
CONSULTANT Pickering Pickering Firm Incorporated JOB NO. 22942.22
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NO. DESCRIPTION DATE BY REVISIONS
PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
SHEET TITLE: PAVEMENT UNDERDRAIN DETAILS
13-1368-02 DWG. FILE NAME 13136802_C-UD-02
DATE         DWG NO.           05/31/22         C-UD-02           SCALE         1"=50'           SHEET NO.         59 OF 87

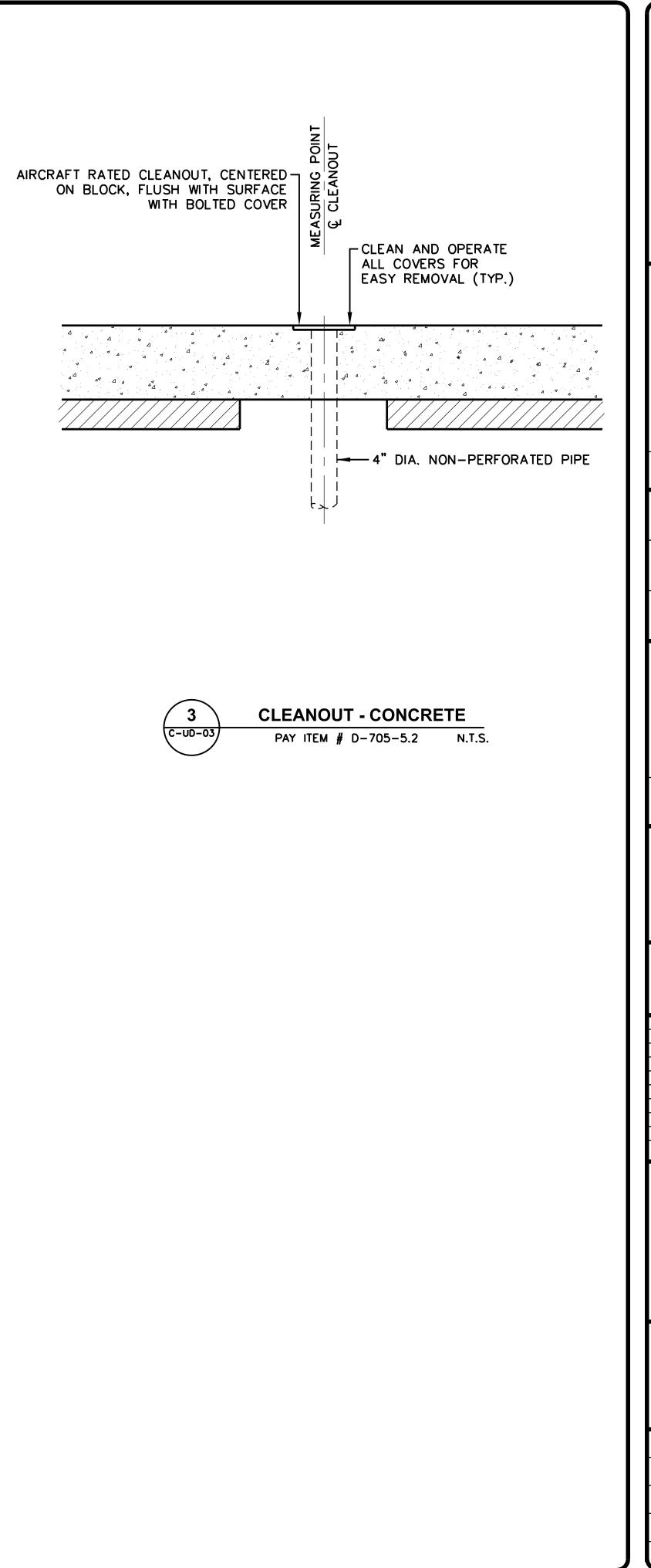




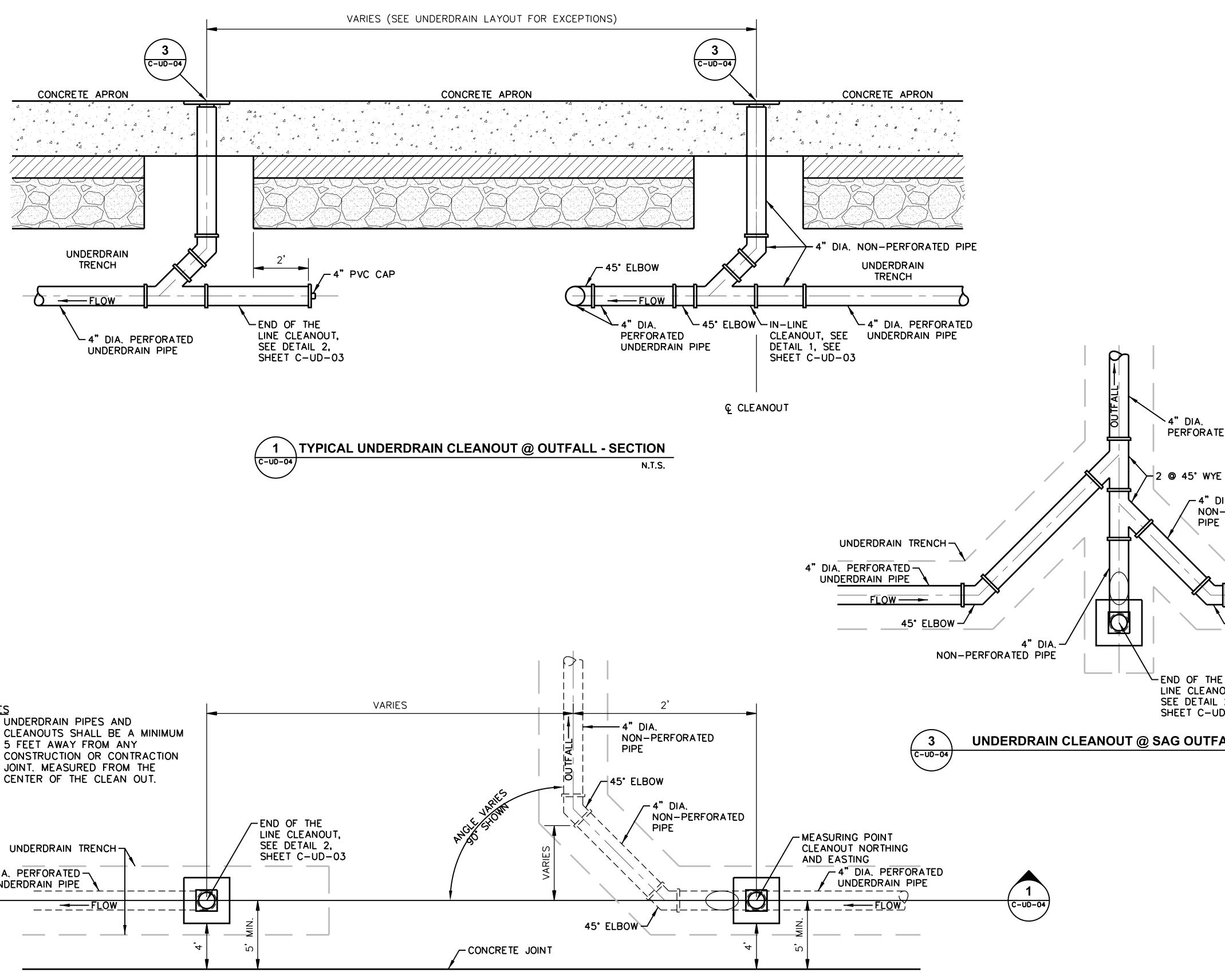
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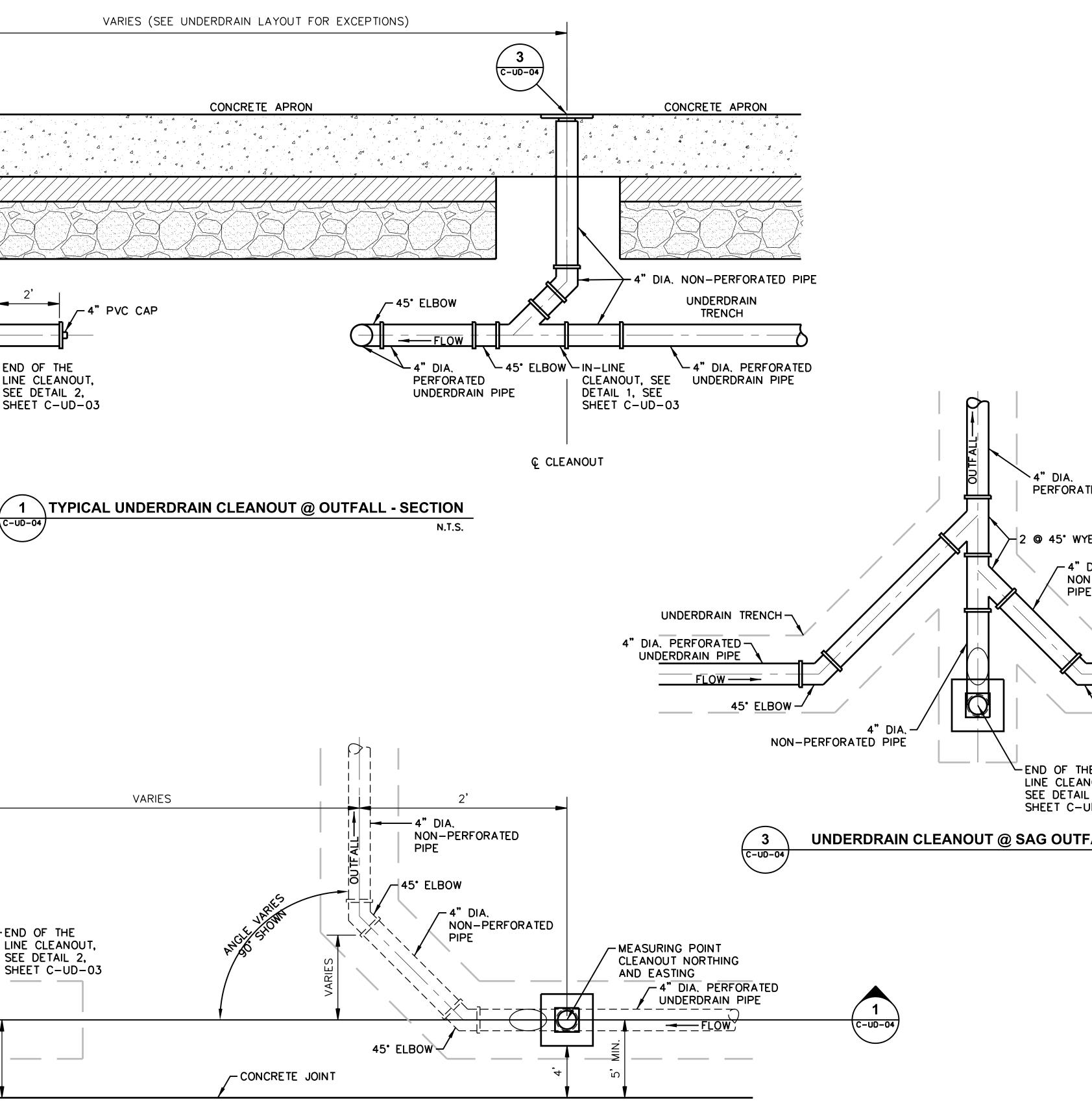


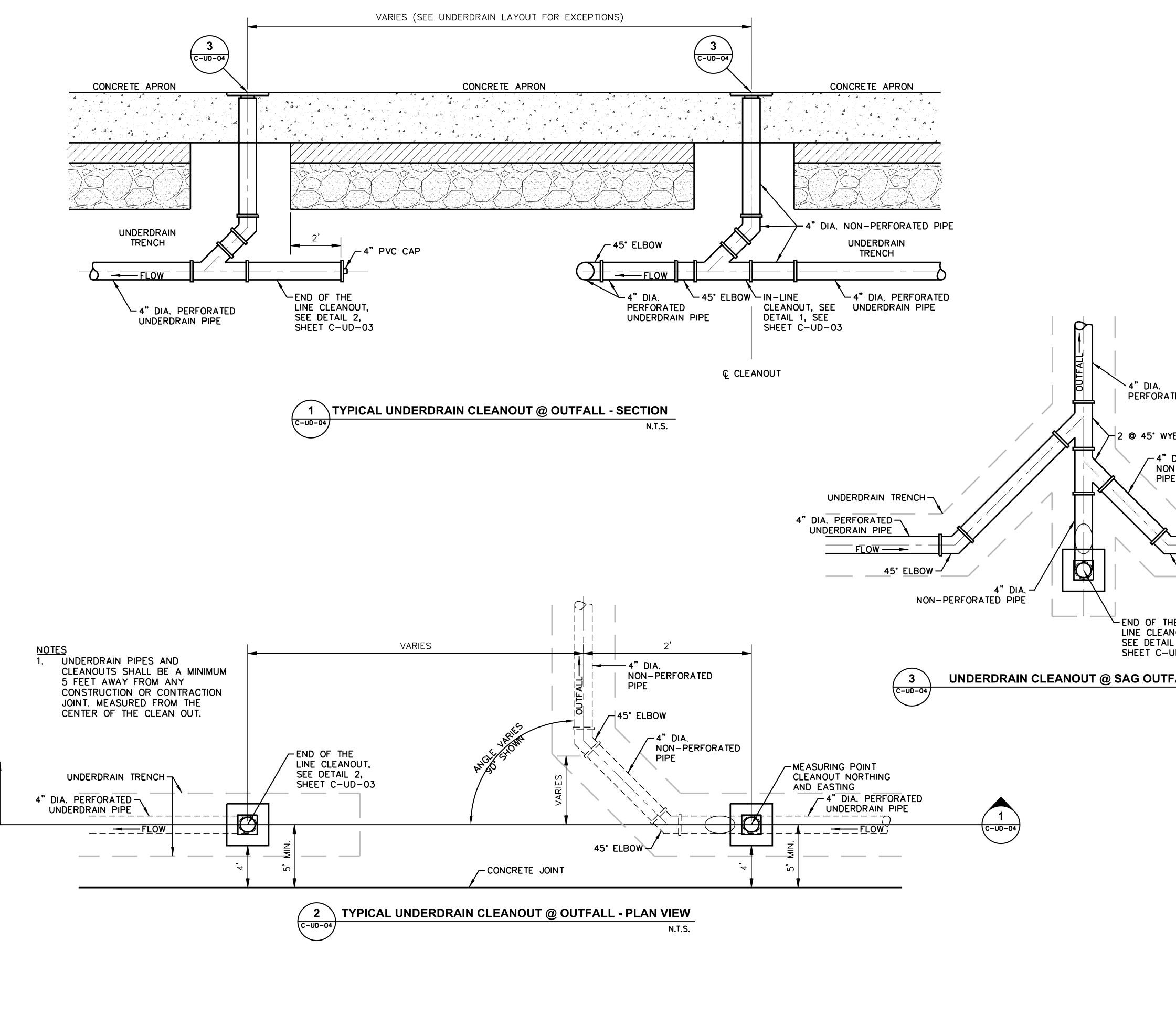
3 C-UD-03



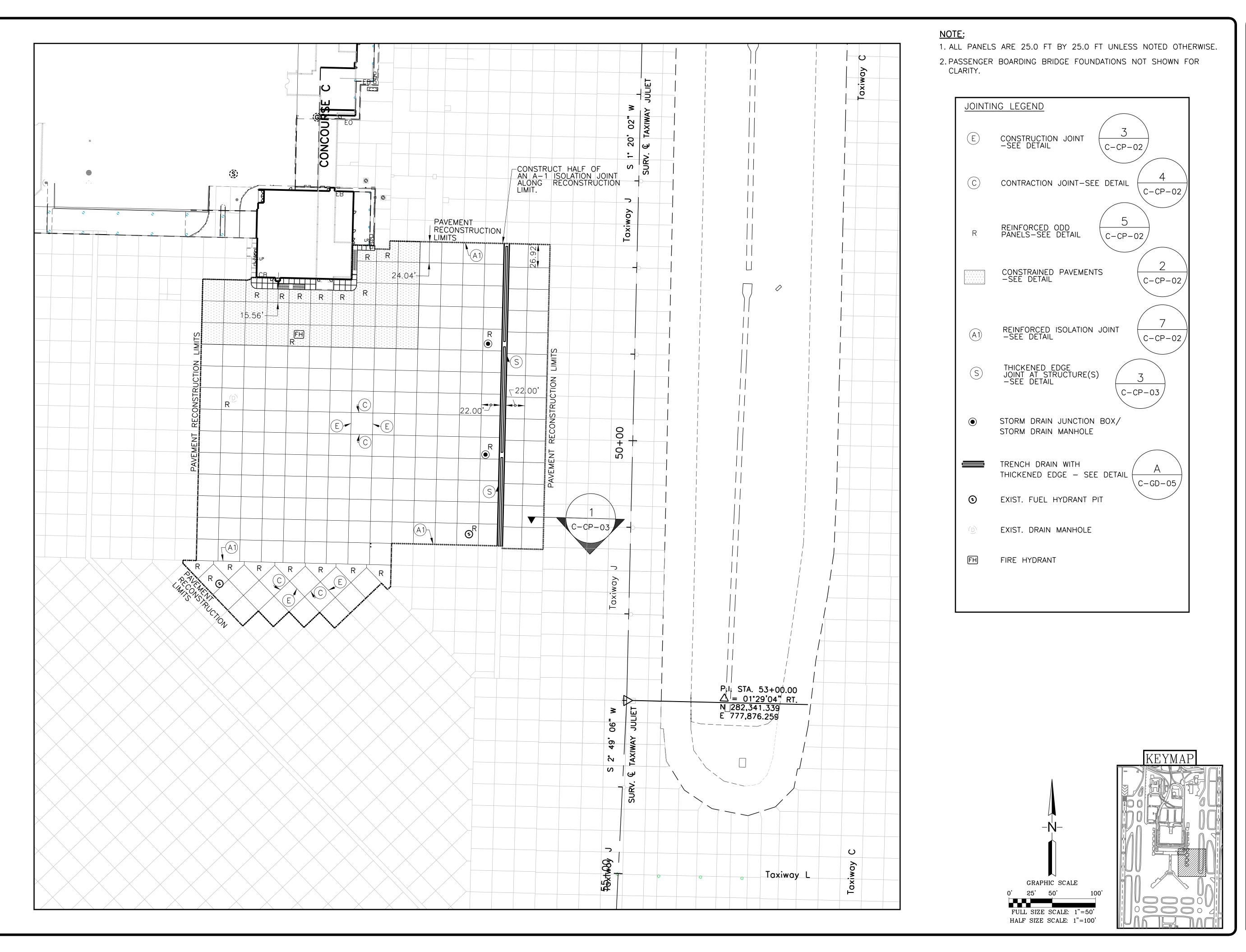
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NO. DESCRIPTION DATE BY REVISIONS PROJECT:
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
SHEET TITLE:
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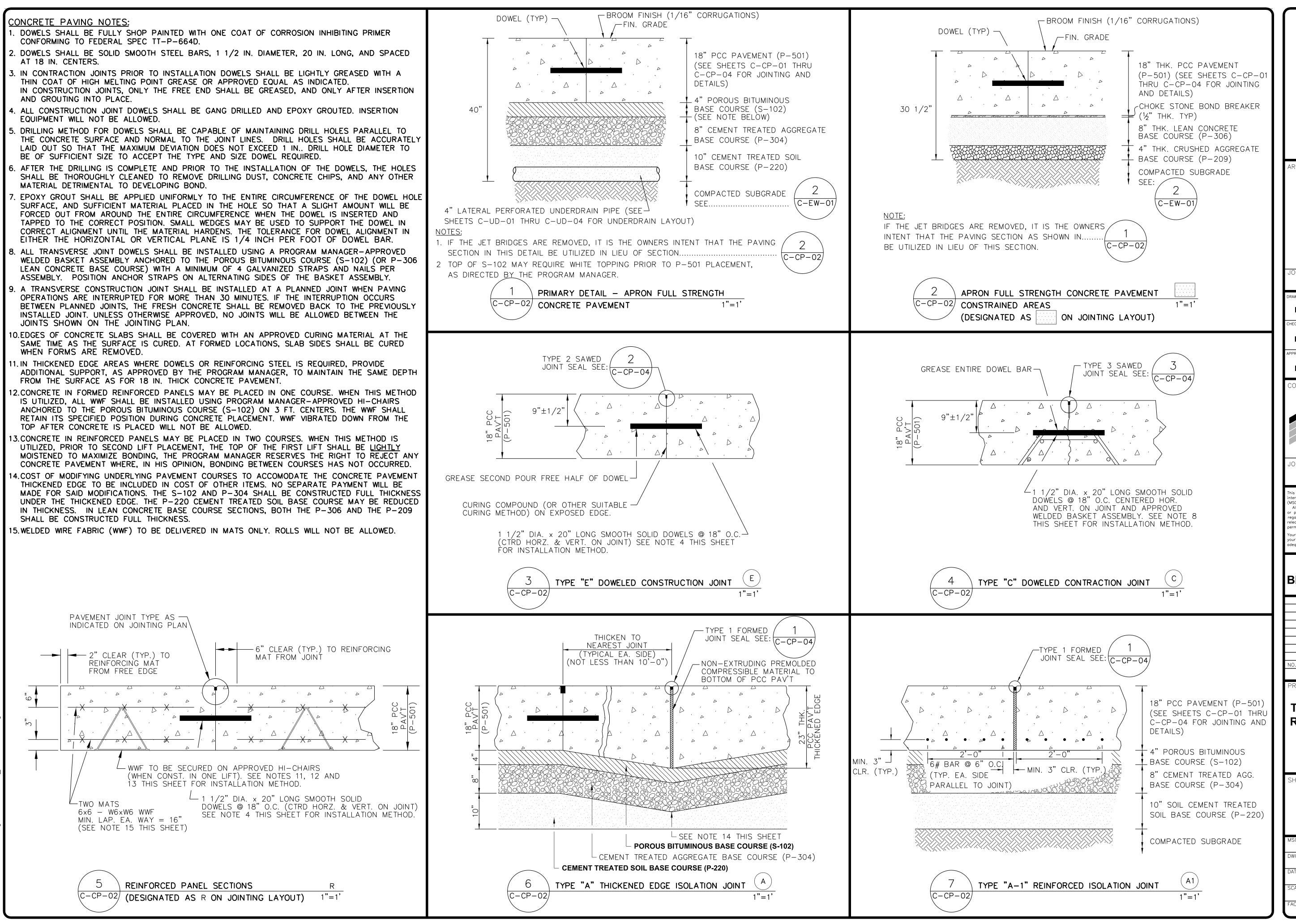


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NOTES 1. UNDERDRAIN PIPES AND 2. CLEANOUTS SHALL BE A MINIMUM 3. FEET AWAY FROM ANY CONSTRUCTION OR CONTRACTION JOINT. MEASURED FROM THE CENTER OF THE CLEAN OUT.	DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN CONSULTANT
AA. -PERFORATED 4" DIA. PERFORATED UNDERDRAIN PIPE	JOB NO. 22942.22
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	PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
	SHEET TITLE: PAVEMENT UNDERDRAIN DETAILS MSCAA NO. 13-1368-02
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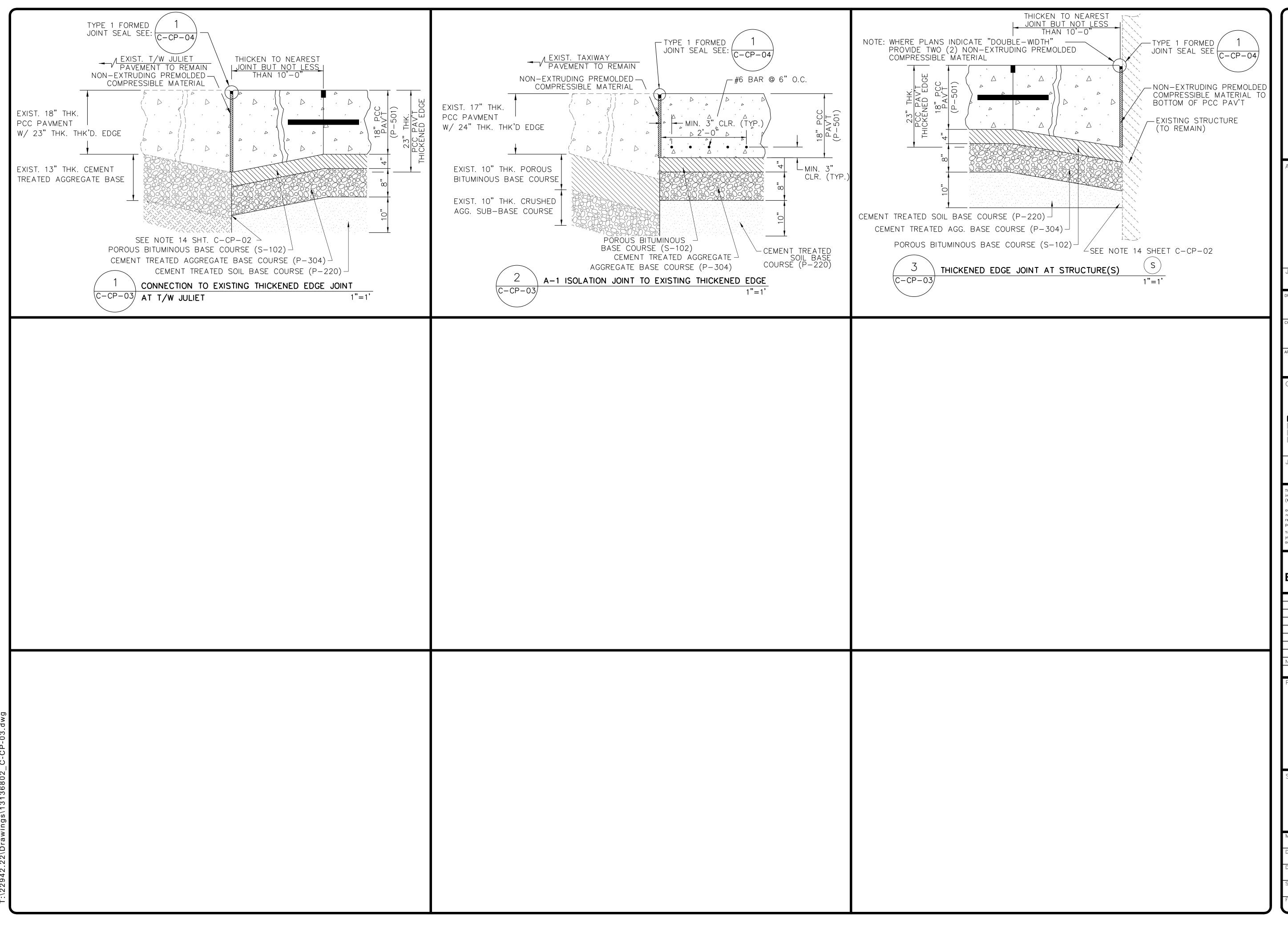
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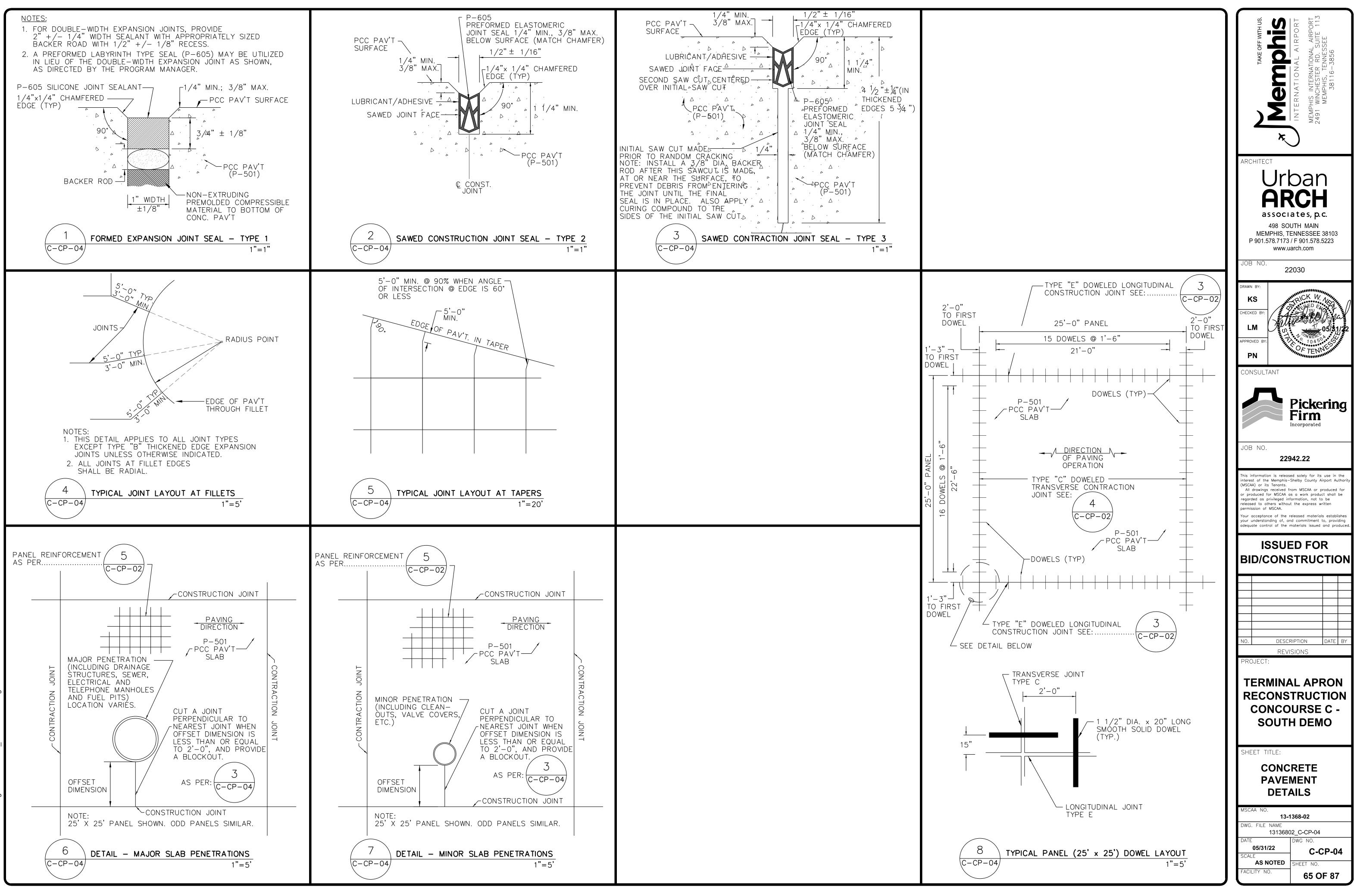
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ARCHITECT Urban Baran Associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
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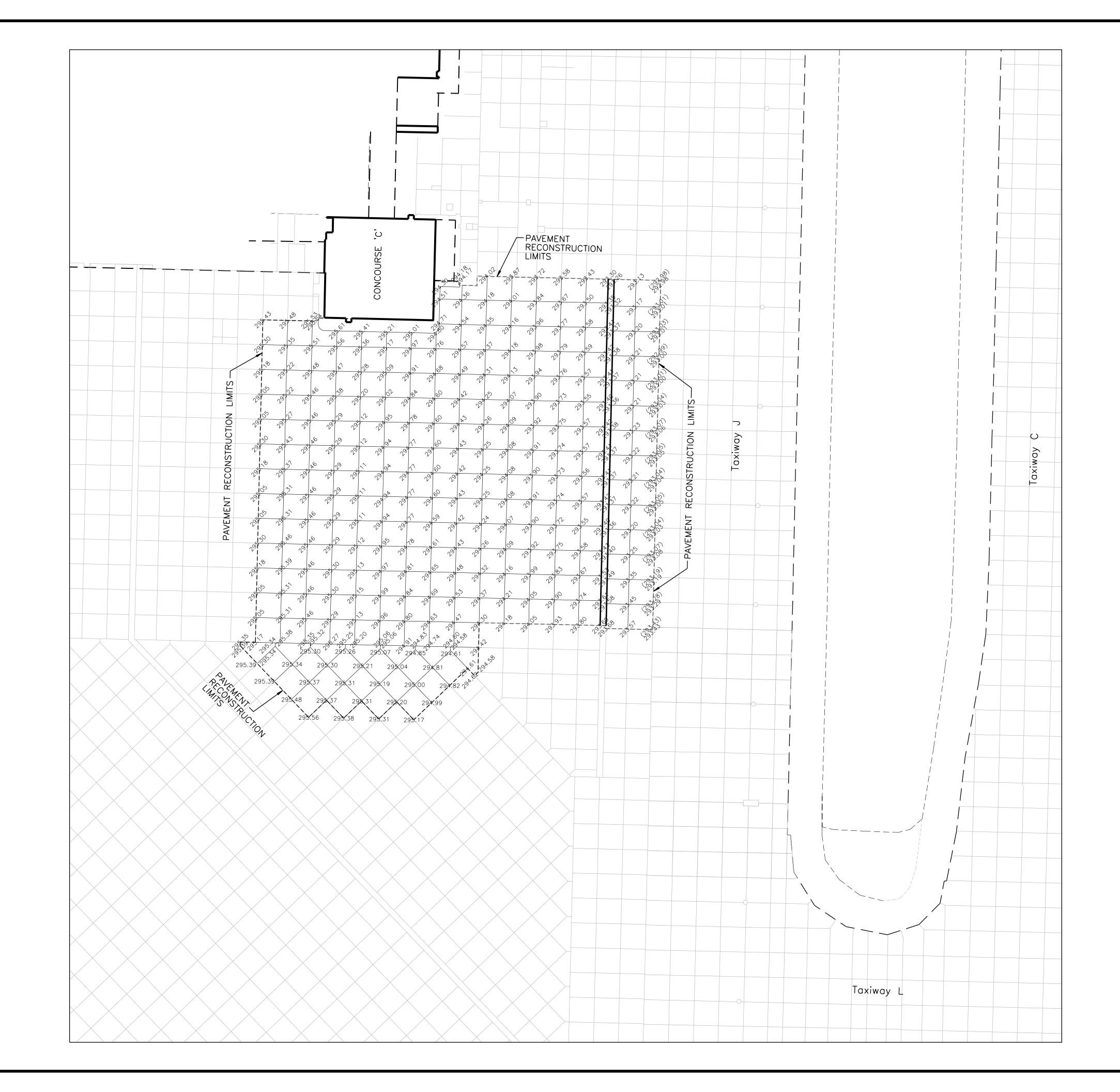


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# NOTES:

- <u>LEGEND</u>

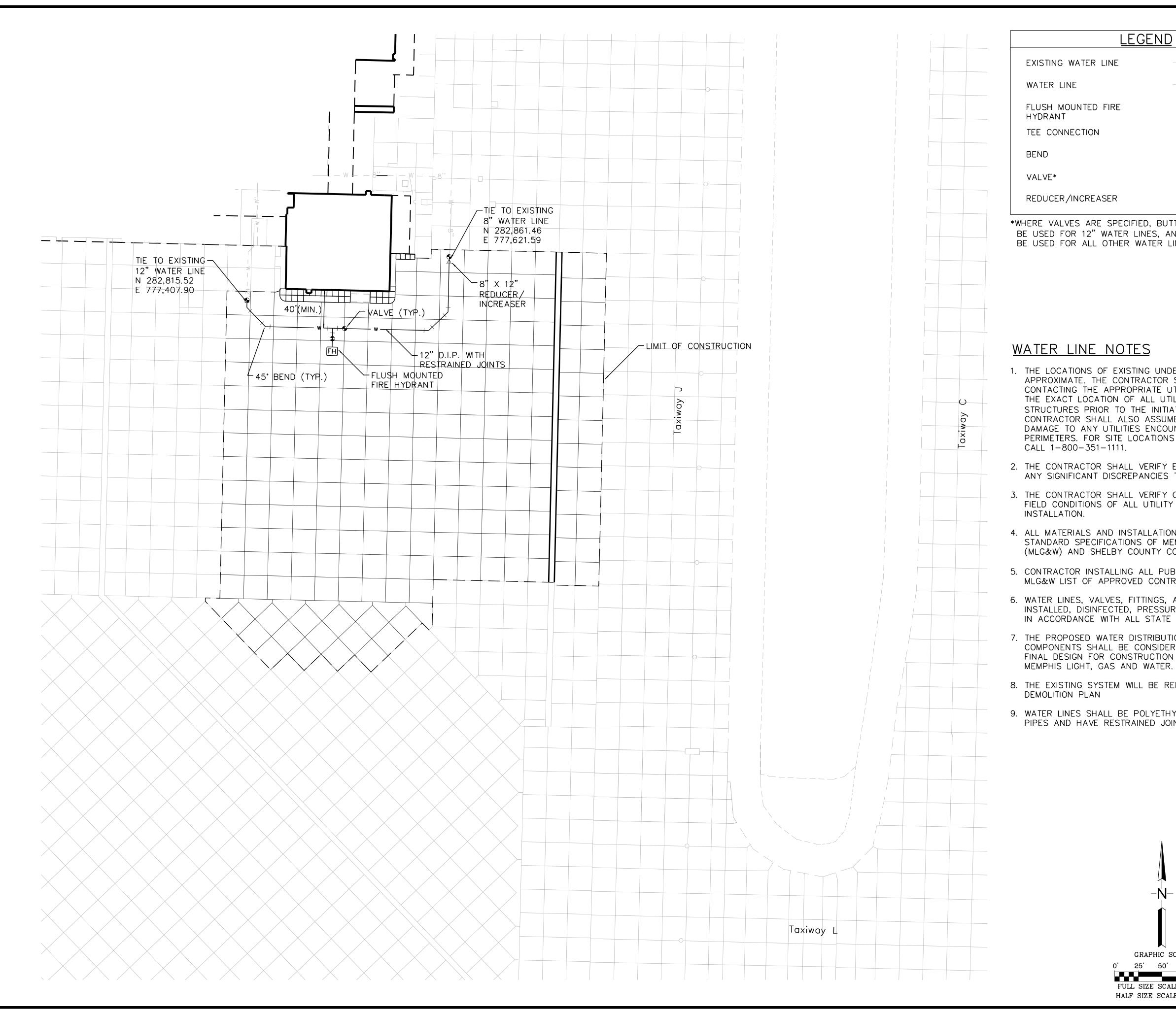
1. ALL ELEVATIONS SHOWN ARE AT TOP OF CONCRETE (FIN. GRADE) AT EACH PANEL CORNER. 2. SEE SHEET C-CF-01 FOR PROTECTIVE CONCRETE APRON ELEVATIONS AROUND CONCOURSE BUILDINGS.

3. SPOT ELEVATIONS DO NOT REFLECT "CROWNING" AS INDICATED ON THE FUELING PIT SURFACE FITTING DETAILS.

(XXX.XX) EXISTING PAVEMENT ELEVATIONS XXX.XX PROPOSED PAVEMENT ELEVATIONS

GRAPHIC SCALE 0' 25' 50' 100' FULL SIZE SCALE: 1"=50' HALF SIZE SCALE: 1"=100'
KEYMAP

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\*WHERE VALVES ARE SPECIFIED, BUTTERFLY VALVES SHALL BE USED FOR 12" WATER LINES, AND GATE VALVES SHALL BE USED FOR ALL OTHER WATER LINES.

1. THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE APPROPRIATE UTILITY COMPANY TO DETERMINE THE EXACT LOCATION OF ALL UTILITIES AND/OR UNDERGROUND STRUCTURES PRIOR TO THE INITIATION OF ANY CONSTRUCTION. CONTRACTOR SHALL ALSO ASSUME FULL RESPONSIBILITY FOR DAMAGE TO ANY UTILITIES ENCOUNTERED WITHIN CONSTRUCTION PERIMETERS. FOR SITE LOCATIONS OF EXISTING UTILITIES, PLEASE

2. THE CONTRACTOR SHALL VERIFY EXISTING DATA AND REPORT ANY SIGNIFICANT DISCREPANCIES TO THE ENGINEER.

3. THE CONTRACTOR SHALL VERIFY CONSTRUCTABILITY BASED ON FIELD CONDITIONS OF ALL UTILITY CONNECTIONS PRIOR TO

4. ALL MATERIALS AND INSTALLATIONS SHALL CONFORM TO THE STANDARD SPECIFICATIONS OF MEMPHIS LIGHT GAS AND WATER (MLG&W) AND SHELBY COUNTY CODE ENFORCEMENT.

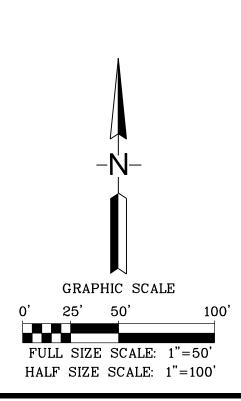
5. CONTRACTOR INSTALLING ALL PUBLIC WATER LINES MUST BE ON MLG&W LIST OF APPROVED CONTRACTORS.

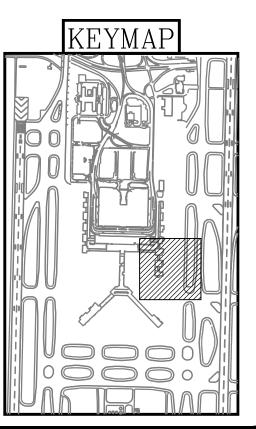
6. WATER LINES, VALVES, FITTINGS, AND HYDRANTS SHALL BE INSTALLED, DISINFECTED, PRESSURE TESTED AND LEAK TESTED IN ACCORDANCE WITH ALL STATE AND LOCAL REQUIREMENTS.

7. THE PROPOSED WATER DISTRIBUTION SYSTEM AND ALL COMPONENTS SHALL BE CONSIDERED SCHEMATIC DESIGN. THE FINAL DESIGN FOR CONSTRUCTION SHALL BE PERFORMED BY

8. THE EXISTING SYSTEM WILL BE REMOVED AS SHOWN IN THE

9. WATER LINES SHALL BE POLYETHYLENE WRAPPED DUCTILE IRON PIPES AND HAVE RESTRAINED JOINTS





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ARCHITECT Urban associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com		
DRAWN BY: KS CHECKED BY: LM APPROVED BY: PN		
CONSULTANT Pickering Firm Incorporated JOB NO.		
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ISSUED FOR BID/CONSTRUCTION		
NO. DESCRIPTION DATE BY REVISIONS PROJECT:		
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO		
SHEET TITLE: WATER LINE PLAN		
MSCAA NO. 13-1368-02 DWG. FILE NAME 13136802_C-WT-01 DATE 05/31/22 SCALE 1"=50' SHEET NO.		

# STRUCTURAL NOTES

THE STRUCTURAL NOTES DEFINE GENERAL DESIGN AND MATERIAL REQUIREMENTS AND ARE INTENDED TO SUPPLEMENT, BUT NOT REPLACE, THE PROJECT SPECIFICATIONS

### GENERAL

- REFERENCE TO STANDARDS OR SPECIFICATIONS OF TECHNICAL SOCIETIES, ORGANIZATIONS, OR ASSOCIATIONS, OR TO CODE OF LOCAL/STATE AUTHORITIES, MEANS THE LATEST STANDARD, SPECIFICATION, OR CODE ADOPTED BY THE DATA SHOWN ON THE DRAWINGS, UNLESS SPECIFICALLY NOTED OTHERWISE.
- MATERIAL, WORKMANSHIP, AND DESIGN SHALL CONFORM TO THE REFERENCE BUILDING CODE.
- 3. FOR DIMENSIONS NOT SHOWN ON THE STRUCTURAL DOCUMENTS, SEE THE ARCHITECTURAL DOCUMENTS.
- 4. CONTRACTOR RESPONSIBILITIES INCLUDE, BUT ARE NOT LIMITED TO, THE FOLLOWING: 4.1. COORDINATE THE STRUCTURAL DOCUMENTS WITH THE ARCHITECTURAL, MECHANICAL, ELECTRICAL, PLUMBING, AND CIVIL DOCUMENTS. ARCHITECTURAL/STRUCTURAL ENGINEER SHALL BE NOTIFIED OF ANY DISCREPANCY OR OMISSION.
  - 4.2. VERIFY EXISTING DIMENSIONS, ELEVATIONS, AND SITE CONDITIONS BEFORE STARTING WORK. ARCHITECT/STRUCTURAL ENGINEER SHALL BE NOTIFIED OF DISCREPANCY OR OMISSION.
  - 4.3. THE STRUCTURE IS DESIGNED TO BE SELF-SUPPORTING AND STABLE AFTER THE BUILDING IS FULLY COMPLETED. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO DETERMINE ERECTION PROCEDURES AND SEQUENCE AND TO INSURE THE SAFETY OF THE BUILDING AND ITS COMPONENT PARTS DURING ERECTION. THIS INCLUDES THE ADDITION OF SHORING, SHEETING, TEMPORARY BRACING, GUYS OR TIE DOWNS THAT MAY BE NECESSARY. REVIEW OF THE CONSTRUCTION BY THE ENGINEER IS FOR CONFORMANCE WITH THE DESIGN ASPECTS ONLY, NOT TO REVIEW THE CONTRACTOR'S CONSTRUCTION PROCEDURES. LACK OF COMMENT ON THE PART OF THE ENGINEER WITH REGARD TO CONSTRUCTION PROCEDURES IS NOT TO BE INTERPRETED AS APPROVAL OF THOSE PROCEDURES.
  - REFER TO THE ARCHITECTURAL DRAWINGS FOR ANGLES, CLIPS, BARS, PLATES, AND OTHER ITEMS 4.4 ATTACHED TO STRUCTURAL MEMBERS.
  - PROVIDE TEMPORARY BRACING AS REQUIRED TO MAINTAIN THE ALIGNMENT AND SECURITY OF 4.5
  - STRUCTURES DURING CONSTRUCTION. DO NO CUTTING, DRILLING, OR MODIFYING OF STRUCTURAL MEMBERS WITHOUT THE APPROVAL OF 4.6. THE ARCHITECT.
  - 4.7. JOB SITE SAFETY IS SOLELY THE RESPONSIBILITY OF THE CONTRACTOR. REVIEW OF THE CONSTRUCTION BY THE ENGINEER IS FOR CONFORMANCE WITH DESIGN ASPECTS ONLY, NOT TO REVIEW THE CONTRACTOR'S PROVISIONS FOR JOB SITE SAFETY. LACK OF COMMENT ON THE PART OF THE ENGINEER WITH REGARD TO JOB SITE SAFETY IS NOT TO BE INTERPRETED AS APPROVAL OF JOB SITE SAFETY ASPECTS.
  - 4.8. DURING CONSTRUCTION, THE GENERAL CONTRACTOR SHALL GIVE THE STRUCTURAL ENGINEER 48 HOURS NOTICE OF DIFFERENT POINTS DURING CONSTRUCTION WHEN A SITE VISIT WOULD BE APPROPRIATE. THESE TIMES ARE THE PLACING OF FOOTING AND SLAB CONCRETE, AND THE COMPLETION OF DIFFERENT AREAS OF FRAMING BEFORE THOSE AREAS ARE COVERED UP WITH SHEATHING, ROOFING, OR CEILINGS.
  - NO PORTION OF THE CONSTRUCTION IS TO PROCEED BEFORE SHOP DRAWINGS FOR THAT AREA 49 HAVE BEEN REVIEWED BY THE ARCHITECT AND STRUCTURAL ENGINEER.
  - 4.10. INDEPENDENT TESTING AGENCY/AGENCIES PROVIDED BY THE CONTRACTOR SHALL PERFORM TESTS IN ACCORDANCE WITH APPLICABLE CODES AND STANDARDS FOR SOIL COMPACTION, FOUNDATION BEARING CAPACITY, REBAR PLACEMENT, CONCRETE COMPRESSIVE STRENGTH, GROUT COMPRESSIVE STRENGTH, METAL DECK ATTACHMENTS AND HIGH STRENGTH BOLT TIGHTENING. WHERE SPECIFIED ELSEWHERE ON THE EITHER THE DRAWINGS OR THE SPECIFICATIONS TESTS ARE ALSO TO BE DONE FOR WELD TESTING. A COPY OF EACH OF THESE TEST REPORTS IS TO BE SENT BY FAX OR MAIL TO JAMNU H. TAHILIANI AND ASSOCIATES, INC., 474 PERKINS EXT., SUITE 211, MEMPHIS, TN 38117, FAX # (901)767-2510.

# FOUNDATIONS

- THE FOUNDATIONS ARE DESIGNED BASED ON THE GEOTECHNICAL INVESTIGATION REPORT PREPARED BY ARUN WAGH, INC., JOB NO. GEC-5501, DATED MARCH 2014. THE GEOTECHNICAL ENGINEER SHALL HAVE FULL RESPONSIBILITY FOR ALL EXCAVATIONS, BACK SHORING DESIGN, RECOMPACTION OF REMOVED SOILS, PAVEMENT DESIGN, AND ALL OTHER ISSUES NOT NOTED ON THESE STRUCTURAL DOCUMENTS.
- 1.1. FOUNDATIONS ARE DESIGNED FOR AN ALLOWABLE SOIL BEARING CAPACITY OF 2500 PSF. THE GEOTECHNICAL ENGINEER SHALL VERIFY ALL BEARING VALUES AND PROVIDE ALL LATERAL PRESSURES IMPOSED ON THE NEW AND EXISTING TUNNEL RETAINING WALLS. TO INCLUDE ANY POINT LOAD IMPOSED BY AIRCRAFT OR OTHER DEVICES.
- 1.2. CONTRACTOR SHALL REFERENCE THIS GEOTECHNICAL INVESTIGATION FOR ALL WORK NECESSARY TO COMPLETE THAT PORTION OF WORK.
- BOTTOM OF ALL FOOTINGS SHALL BEAR ON NATURAL UNDISTURBED SOIL OR ENGINEERED FILL A MINIMUM DEPTH BELOW FINISHED GRADE AS RECOMMENDED IN THE GEOTECHNICAL INVESTIGATION OR AS SHOWN IN THE CONSTRUCTION DOCUMENTS.
- ALL SITE PREPARATION, INCLUDING THE WORK OF STRIPPING, BACKFILLING, PROOFROLLING, ENGINEERED FILL PLACEMENT, AND APPROVAL OF FOUNDATION BEARING SURFACES, SHALL BE UNDER THE SUPERVISION OF A LICENSED GEOTECHNICAL ENGINEER PROVIDED BY THE CONTRACTOR. IF THE ALLOWABLE SOIL BEARING CAPACITIES ARE DETERMINED BY THE GEOTECHNICAL ENGINEER TO BE LESS THAN THE DESIGN BEARING PRESSURES, AS STATED HEREIN, THE STRUCTURAL ENGINEER OF RECORD SHALL BE CONTACTED FOR REDESIGN OF THE FOUNDATIONS.
- GENERAL CONTRACTOR SHALL NOTIFY THE GEOTECHNICAL ENGINEER, STRUCTURAL ENGINEER AND/OR ARCHITECT 48 HOURS PRIOR TO PLACEMENT OF CONCRETE IN THE FOOTINGS.
- ANY EXCAVATIONS UNDER FOOTINGS FOR THE PURPOSE OF RUNNING PIPES, ETC. SHALL BE BACKFILLED WITH COMPACTED FILL TESTED BY A GEOTECHNICAL ENGINEER OR WITH LEAN CONCRETE AS APPROVED BY THE STRUCTURAL ENGINEER OF RECORD. THIS SHALL BE DONE BEFORE THE EXCAVATION IS ALLOWED TO FILL WITH WATER.
- NO EXCAVATIONS SHALL BE MADE UNDER, OR ADJACENT TO, IN-PLACE SPREAD FOOTINGS, STRIP FOOTINGS, OR ANY OTHER FOUNDATIONS WITHOUT THE APPROVAL FROM THE STRUCTURAL ENGINEER OF RECORD.
- 7. IF THERE IS SEEPAGE IN EXCESS OF ONE INCH (1"), THE FOOTING EXCAVATION SHOULD BE PUMPED DRY.

## CONCRETE

- AIR ENTRAINMENT (AIR) AS SPECIFIED: CATEGORY FOOTINGS FOUNDATION WALLS BASEMENT AND RETAINING WALLS SLAB-ON-GRADE COLUMNS BEAMS AND FRAMED SLABS
  - EXPOSED FRAMED SLABS
- ACI 318 TABLE 4.2.1 FOR SEVERE EXPOSURE.

### 3. REINFORCEMENT:

J. K⊑IIV	
3.1.	REINFORCING SHALL BE A-615, GRADE
	THE LATEST A.S.T.M. SPECIFICATIONS.
3.2.	WELDED WIRE FABRIC (WWF) SHALL B
	SUPPLIED IN FLAT SHEETS - NOT ROLL
	THAT IT IS NOT DISPLACED BY FOOT T
3.3.	REINFORCING STEEL SHALL HAVE THE
3.4.	ALL REINFORCING STEEL AND ACCESS
	ACCORDANCE WITH THE LATEST EDITI
	*CONCRETE CAST AGAINST EARTH
	*FORMED CONCRETE EXPOSED TO
	#6 THROUGH #18 BARS
	#5 BARS AND SMALLER
	*CONCRETE NOT EXPOSED TO EAF
	WALLS BEAM STIRRUPS
	BEAM STIRRUPS
	COLUMN TIES
	SLAB:
	TOP
	BOTTOM
3.5.	REINFORCEMENT NOTED AS CONTINU
/	THE CONTRACTOR UNLESS SHOWN OF
3.5.1.	REINFORCEMENT SPLICES FOR (
0 5 0	EDITION OF ACI 318. MINIMUM OF
3.5.2.	WELDED REINFORCEMENT SPLIC
	WELDING IS APPROVED, IT SHALL
0 5 0	REINFORCING STEEL.
3.5.3.	
	MEMBERS IS NOT PERMITTED. W
	EQUIVALENT OR A-307 TREADED

	MEMBERS IS NOT PERMITTED. W
	EQUIVALENT OR A-307 TREADED
	THE STRUCTURAL STEEL MEMBE
8.6.	REINFORCING IN ALL CONCRETE STRIF
	CORNERS.
3.7.	DETAILING OF CONCRETE REINFORCE
	RECOMMENDATIONS OF THE A.C.I. DET

- FLY ASH AND CEMENT IN THE MIX SHALL NOT EXCEED 20 PERCENT.
- PLACE BEFORE CONCRETE IS POURED.
- REACHES ITS DESIGN STRENGTH.
- DESIGN STRENGTH.
- STRENGTH.
- SLAB CONSTRUCTION".
- DEPRESSIONS.

- 13. WATER FOR CONCRETE SHALL BE CLEAN, FRESH AND POTABLE.

CONCRETE SHALL HAVE NATURAL SAND FINE AGGREGATE AND NORMAL WEIGHT CRUSHED LIMESTONE COARSE AGGREGATES CONFORMING TO ASTM C-33, TYPE I PORTLAND CEMENT CONFORMING TO ASTM C-150 AND SHALL HAVE A MINIMUM 28-DAY COMPRESSIVE STRENGTH (F'c), WATER/CEMENT RATIO (W/C), AND

	<u>F'c</u> 4500 psi	<u>W/C</u> 0.45	<u>AIR</u> YES
	4500 psi	0.45	YES
.S	4500 psi	0.45	YES
	4500 psi	0.50	NO
	<u>4</u> 500 psi	0.40	YES
	<u>4</u> 500 psi	0.40	YES
	<u>4</u> 500 psi	0.45	YES

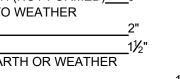
2. CONCRETE NOTED AS AIR ENTRAINED ABOVE SHALL AIR CONTENT CONFORMING TO THE THE STANDARDS IN

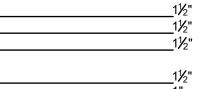
E 60, EXCEPT #3 BARS SHALL BE GRADE 40, IN ACCORDANCE WITH

BE ASTM A-185, 8" MINIMUM SIDE AND END LAPS. WWF SHALL BE LS. SUPPORT WWF WITH CHAIRS AND REINFORCING BARS SO TRAFFIC.

E FOLLOWING CONCRETE COVER UNLESS NOTED OTHERWISE: SORIES SHALL BE DETAILED. FABRICATED. AND PLACED IN TION OF THE A.C.I. DETAILING MANUAL.

H (NOT FORMED)\_\_\_3"





JOUS (CONT.) SHALL BY SPLICES AT LOCATIONS DETERMINED BY OR NOTED OTHERWISE

CONCRETE SHALL BE FULL TENSION SPLICES PER LATEST OF 62 BAR DIAMETERS.

ICES IS NOT PERMITTED WITHOUT ENGINEER APPROVAL. WHERE LL CONFORM TO AWS D-1.4, STRUCTURAL WELDING CODE -

LAIN CONCRETE REINFORCING BARS TO STEEL STRUCTURAL WELD "D-250 SERIES" COUPLERS BY BAR-LOCK OR AN ROD OR EQUAL DIAMETER WITH PROPER SPLICE LENGTH TO

IP FOOTINGS AND WALLS SHALL BE CONTINUOUS AROUND

EMENT BARS AND ACCESSORIES SHALL CONFORM TO THE ETAILING MANUAL (ACI 315 LATEST EDITION). 3.8. UPON REQUEST, SUPPLIER SHALL PROVIDE MILL REPORTS ON ALL REINFORCING.

4. FLY ASH MAY BE USED AS A POZZOLAN TO REPLACE A PORTION OF THE PORTLAND CEMENT IN A CONCRETE MIX, SUBJECT TO THE APPROVAL OF THE STRUCTURAL ENGINEER. FLY ASH, WHEN USED, SHALL CONFORM TO ASTM C-618. CONCRETE MIXES USING FLY ASH SHALL BE PROPORTIONED TO ACCOUNT FOR THE PROPERTIES OF THE SPECIFIC FLY ASH USED AND TO ACCOUNT FOR THE SPECIFIC PROPERTIES OF THE FLY ASH CONCRETE THUS RESULTING. THE RATIO OF THE AMOUNT OF THE FLY ASH TO THE TOTAL AMOUNT OF

5. ANCHOR BOLTS, DOWELS AND OTHER EMBEDDED ITEMS SHALL BE SECURED TO THE REINFORCEMENT IN

ALL CONCRETE WALLS RETAINING EARTH SHALL BE BRACED AND NO BACKFILLING SHALL COMMENCE UNTIL THE FRAMING, AND CONCRETE SLAB AT THE TOP OF THE WALLS ARE IN PLACE AND ALL CONCRETE

ALL CONCRETE CANTILEVERED RETAINING WALLS SHALL BE BRACED TO THE TOP AND BACKFILLING BEHIND THE CANTILEVERED RETAINING WALLS SHALL COMMENCE ONLY AFTER THE CONCRETE HAS ATTAINED ITS

ALL CONCRETE WALLS RETAINING EARTH SHALL BE BRACED AND NO BACKFILLING SHALL COMMENCE UNTIL SLAB DECK AT THE TOP OF THE WALLS ARE IN PLACE AND THE CONCRETE HAS ATTAINED ITS DESIGN

9. FLOOR SLABS SHALL BE PLACED IN ACCORDANCE WITH ACI 302.1R-04 "GUIDE FOR CONCRETE FLOOR AND

10. SPECIAL FINISHES: REFER TO ARCHITECTURAL DRAWINGS FOR MOLDS, GROVES, ORNAMENTS, CLIPS OR GROUNDS REQUIRED TO BE ENCASED IN CONCRETE AND FOR LOCATIONS OF FLOOR FINISHES AND SLAB

11. CONCRETE MIX DESIGNS AND SHOP DRAWINGS FOR CONCRETE REINFORCEMENT SHALL BE SUBMITTED TO THE ARCHITECT/STRUCTURAL ENGINEER FOR REVIEW BEFORE INSTALLATION OF CONCRETE.

12. MIXING, TRANSPORTING, AND PLACING OF CONCRETE SHALL CONFORM TO (ACI 301 LATEST EDITION).

### CONNECTIONS

- 1. STEEL STUDS SHALL BE NELSON <sup>3</sup>/<sub>4</sub>" DIAMETER, HEADED, ASTM A-108 TYPICALLY, UNLESS NOTES OTHERWISE. STUD LENGTH AT COMPOSITE BEAMS SHALL BE 1½" LESS THAN THE SLAB THICKNESS, UNLESS NOTED OTHERWISE.
- 2. ADHESIVE ANCHORS SHALL BE SIMPSON STRONG-TIE "SET HIGH STRENGTH EPOXY" OR AN EQUIVALENT.
- POWDER ACTUATED FASTENERS SHALL BE SIMPSON STRONG-TIE "PDP FASTENERS" OR AND EQUIVALENT.

### STRUCTURAL STEEL

STRUCTURAL STEEL SHALL CONFORM TO THE FOLLOWING: STEEL ANGLES, CHANNELS, PLATES A.S.T.M. SPECIFICATION A-36 (Fy=36KSI) 1.1.

- 2. ALL STEEL EXPOSED TO WEATHERING AND/OR CORROSION SHALL BE HOT-DIPPED GALVANIZED AT THE DIRECTION OF THE ARCHITECT AND/OR OWNER, PER SPECIFICATIONS.
- STRUCTURAL STEEL SHALL BE DETAILED IN ACCORDANCE WITH THE STANDARD PRACTICES OF A.I.S.C.
- ALL WELDS SHALL BE MADE IN ACCORDANCE WITH AWS D1.1 STRUCTURAL WELDING CODE STEEL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ADEQUACY OF ALL CONNECTIONS, WHICH SHALL BE 5. DESIGNED BY A PROFESSIONAL ENGINEER, LICENSED IN THE PROJECT STATE, IN ACCORDANCE WITH THE STANDARD PRACTICES OF A.I.S.C. SHOP DRAWING REVIEW SHALL BE ONLY FOR GENERAL COMPLIANCE WITH THE INFORMATION CONTAINED IN THE CONTRACT DRAWINGS AND SPECIFICATIONS.
- 6. BEFORE FABRICATION SHOP DRAWINGS SHALL BE SUBMITTED TO THE ARCHITECT FOR REVIEW.

### **DESIGN LOADS**

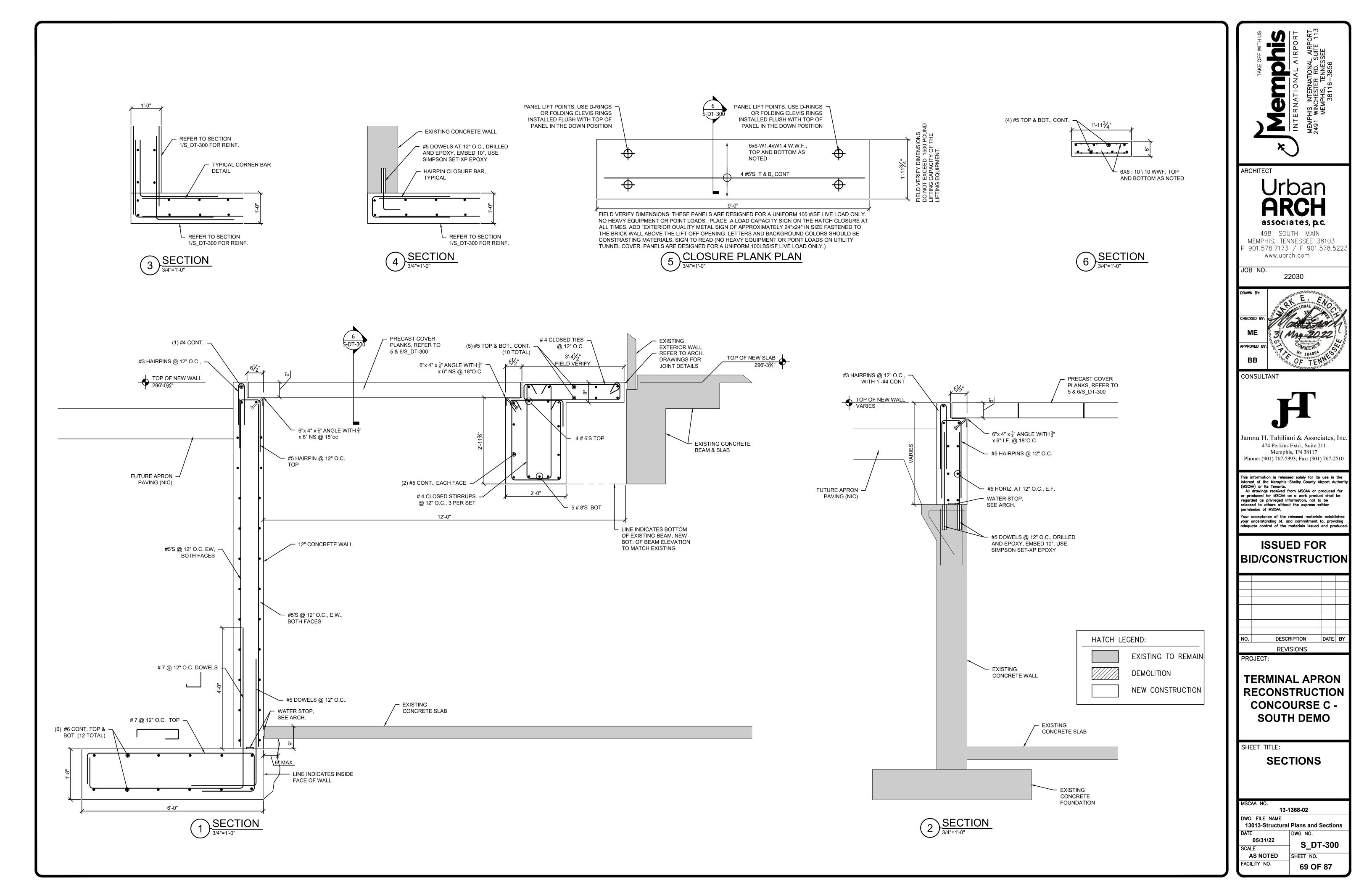
THE STRUCTURE IS DESIGNED FOR THE FOLLOWING LOADS:

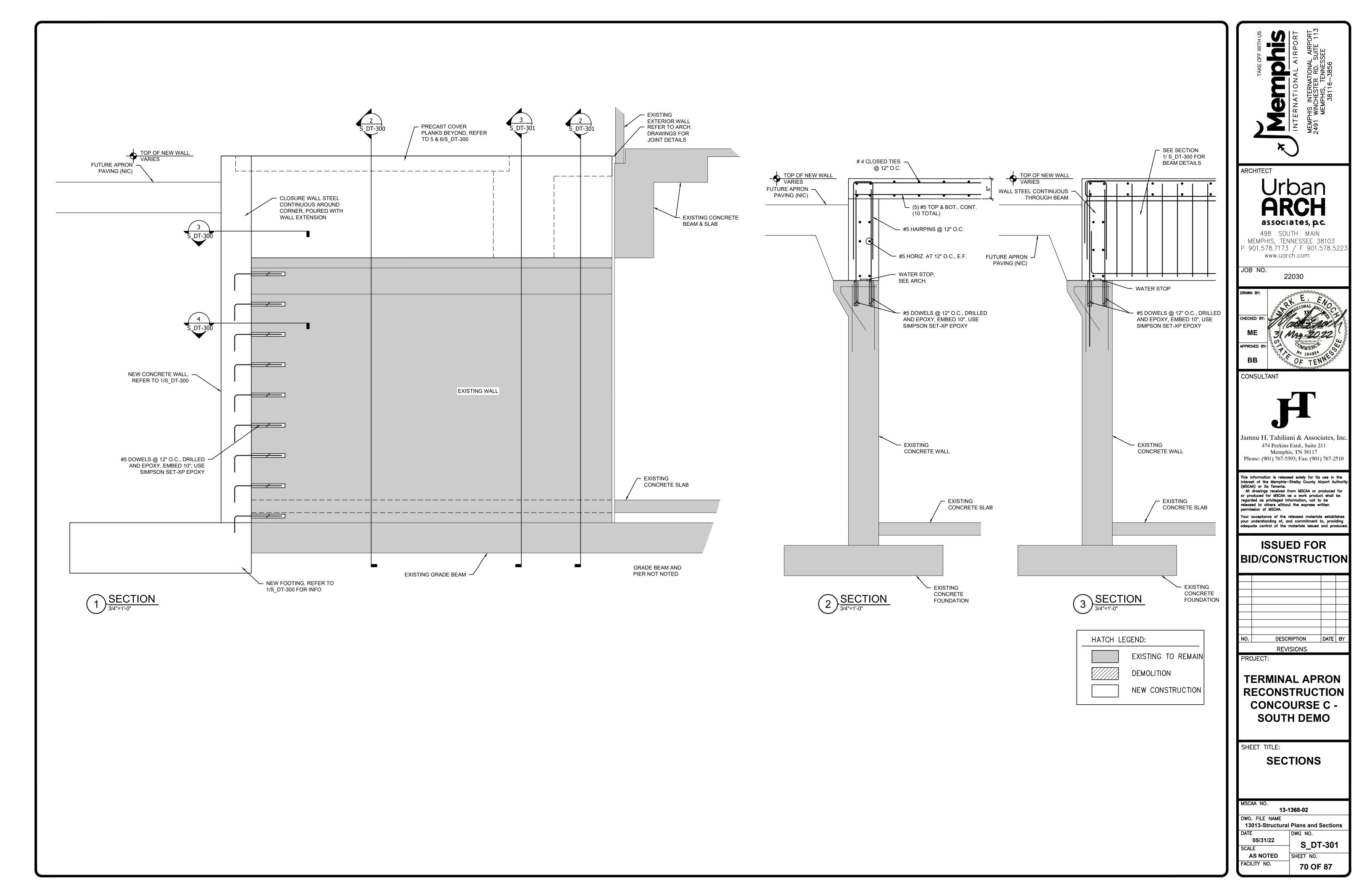
A.	LIVE LOADS: TUNNEL ROOF	100 PSF	(NO POINT LOADS)
В.	WIND PER 2009 INTERNATIONAL BUILDING CODE:		

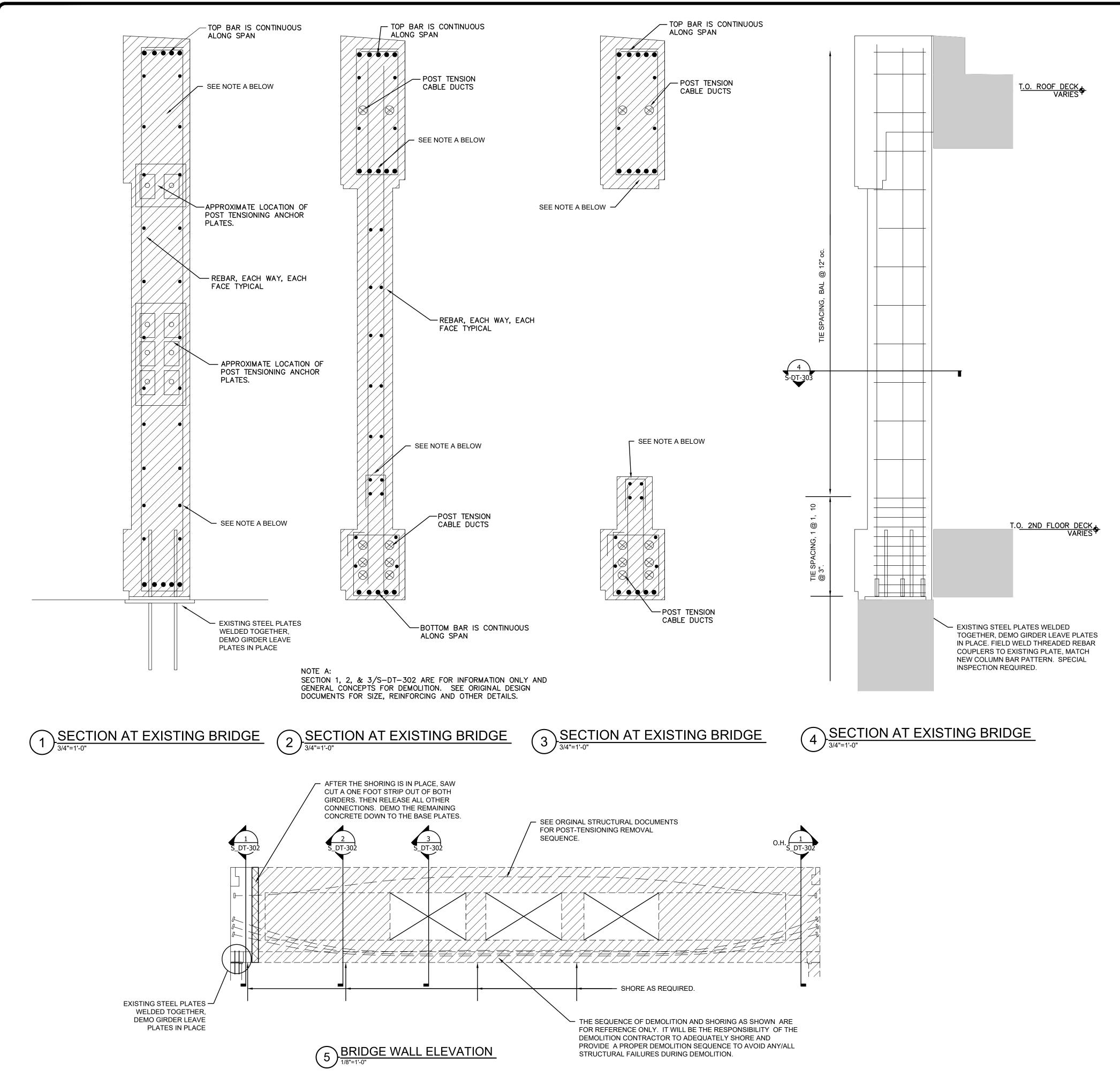
- 1. BASIC WIND SPEED 90mph (3 second gust) 2. WIND IMPORTANCE FACTOR l = 1.00 3. WIND EXPOSURE
  - 4. INTERNAL PRESSURE COEFFICIENT
- SEISMIC C.
- 1. THE MODIFICATIONS ARE ATTACHED TO THE ORIGINAL STRUCTURE. THE LOADS IMPOSE ARE LESS THAN THE ORIGINAL DEAD LOADS IMPARTED BY THE BRIDGE. NO CONSIDERATION HAS BEEN GIVEN TO SEISMIC.

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Jamnu H. 7 474	Tahiliani & Perkins Extd Memphis, TN	2 Assoc I., Suite 2 N 38117	11		
interest of the (MSCAA) or its All drawings or or produced for regarded as pri- released to othe	Phone: (901) 767-5393; Fax: (901) 767-2510 This information is released solely for its use in the interest of the Memphis-Shelby County Airport Authority (MSCAA) or its Tenants. All drawings received from MSCAA or produced for or produced for MSCAA as a work product shall be regarded as privileged information, not to be released to others without the express written permission of MSCAA. Your acceptance of the released materials establishes your understanding of, and commitment to, providing adequate control of the materials issued and produced. <b>ISSUEDFOR</b>				
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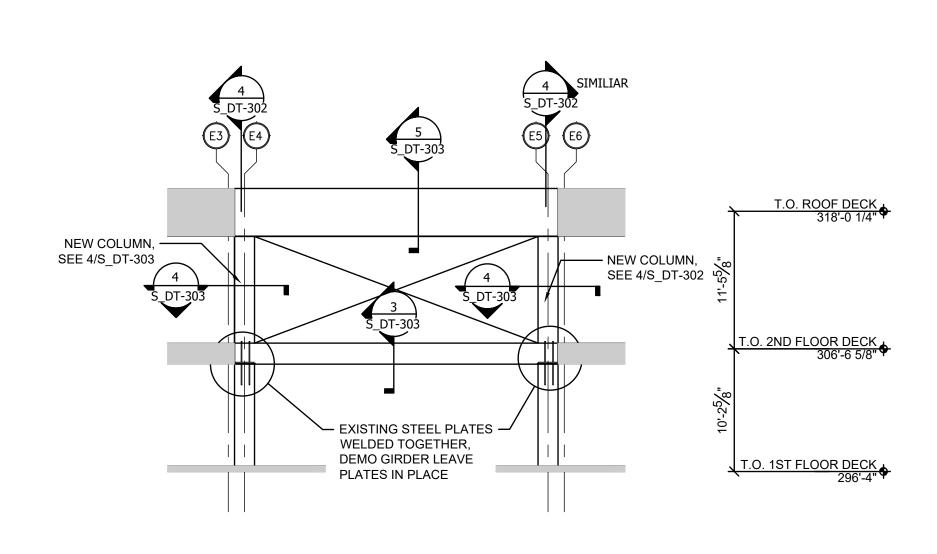


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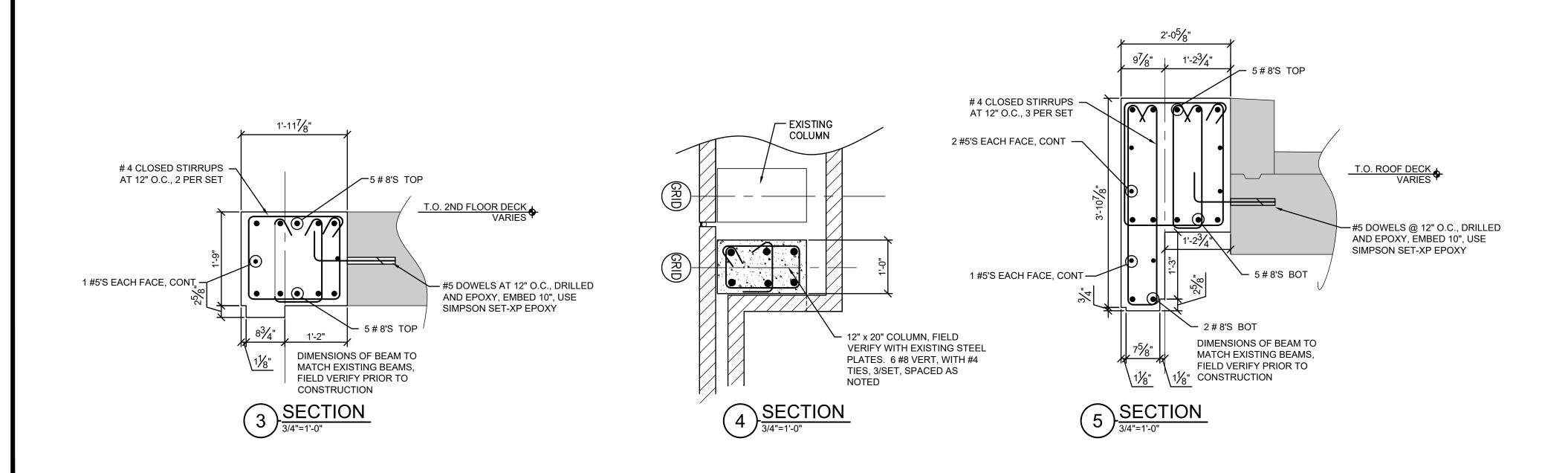
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PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO				
SHEET TITLE: BRIDGE DEMO DETAILS				

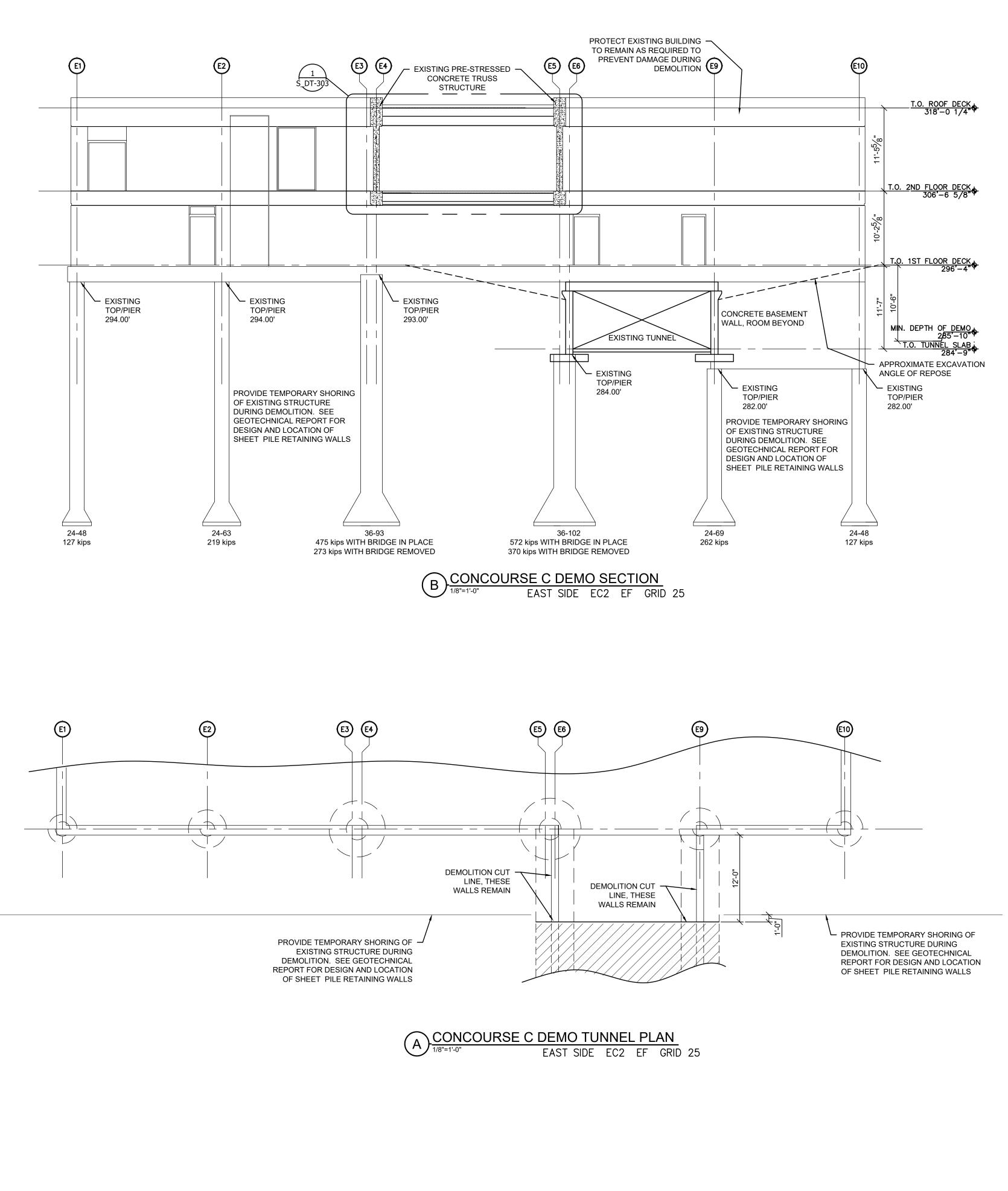


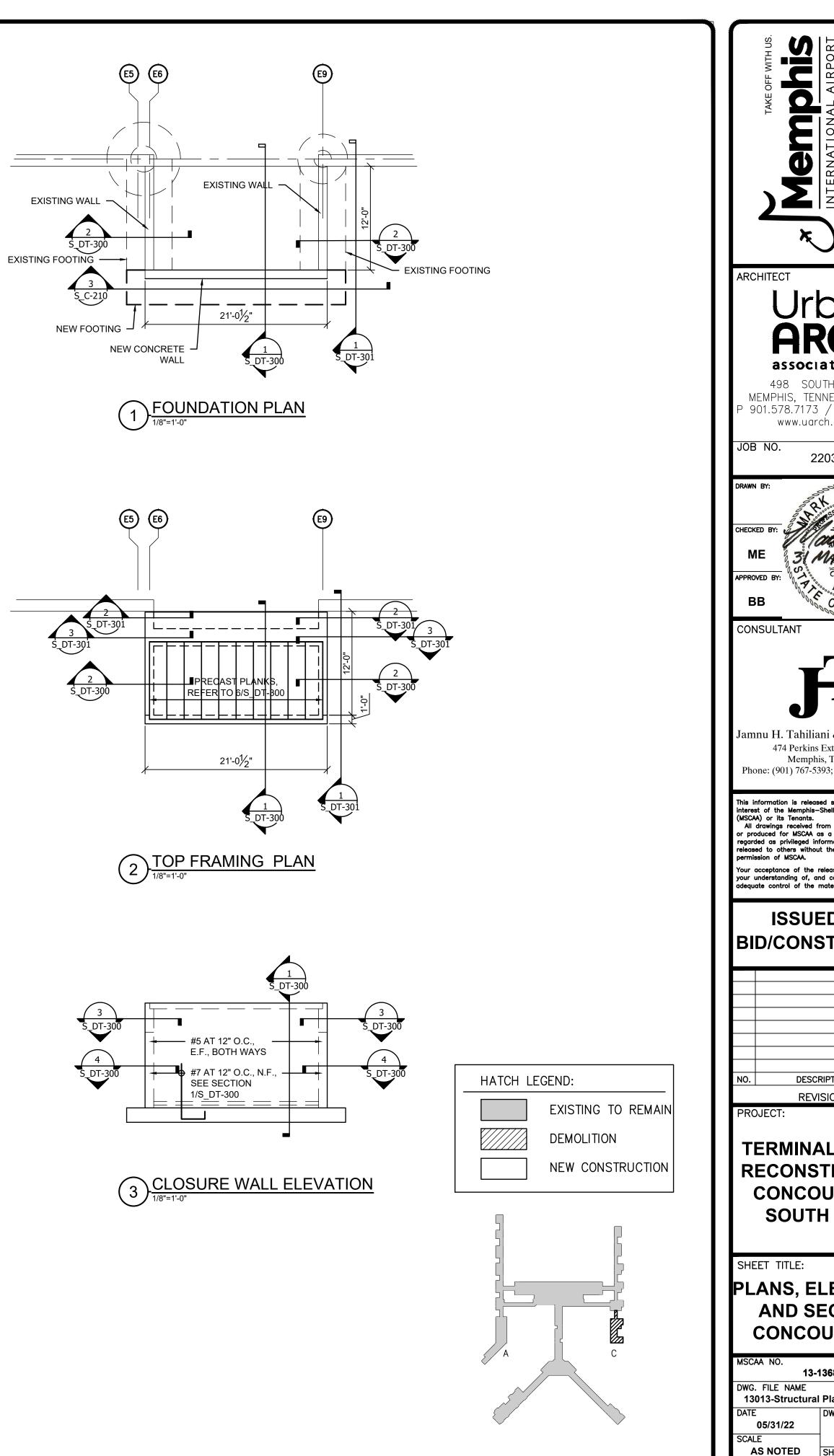


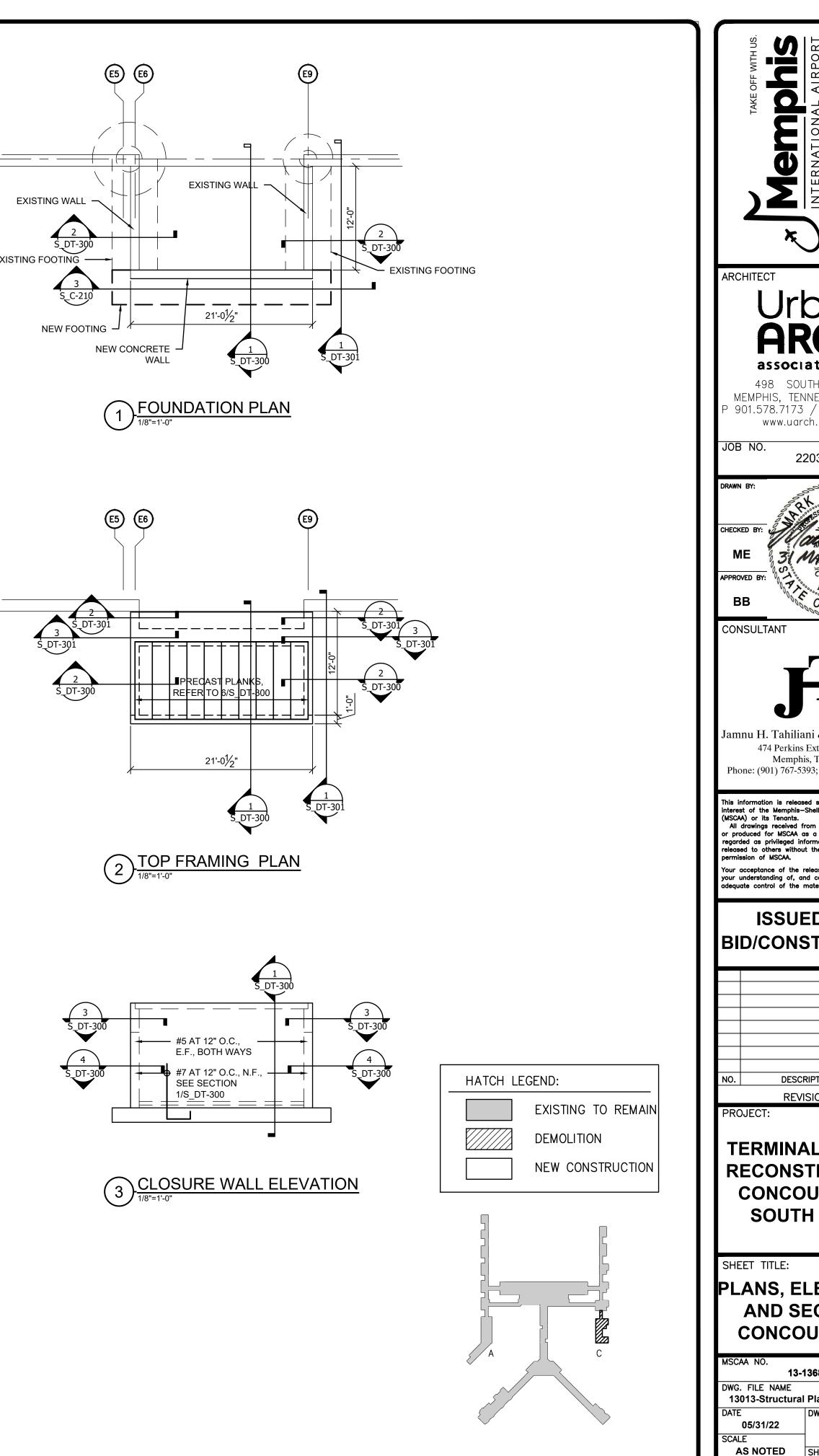


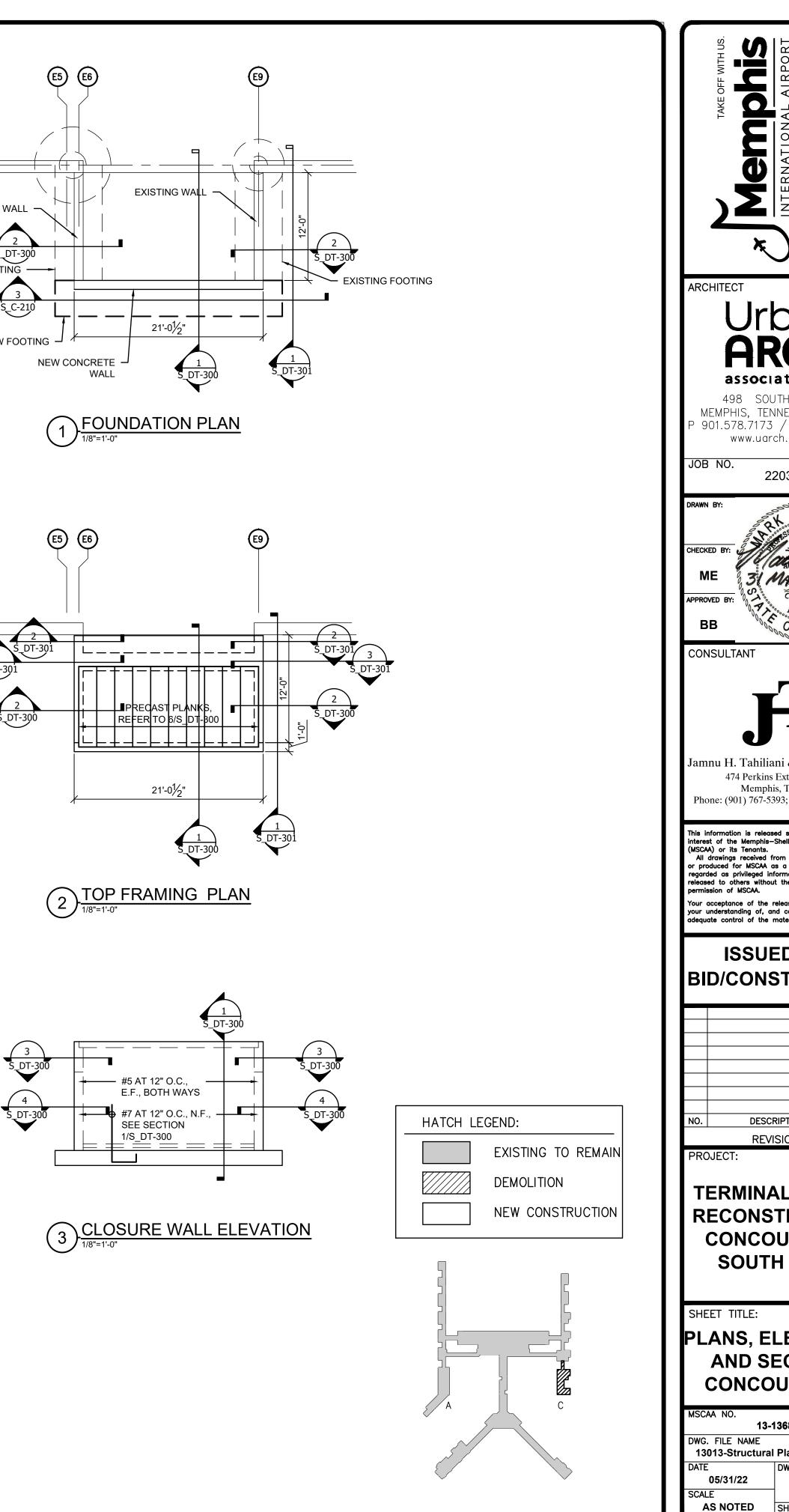
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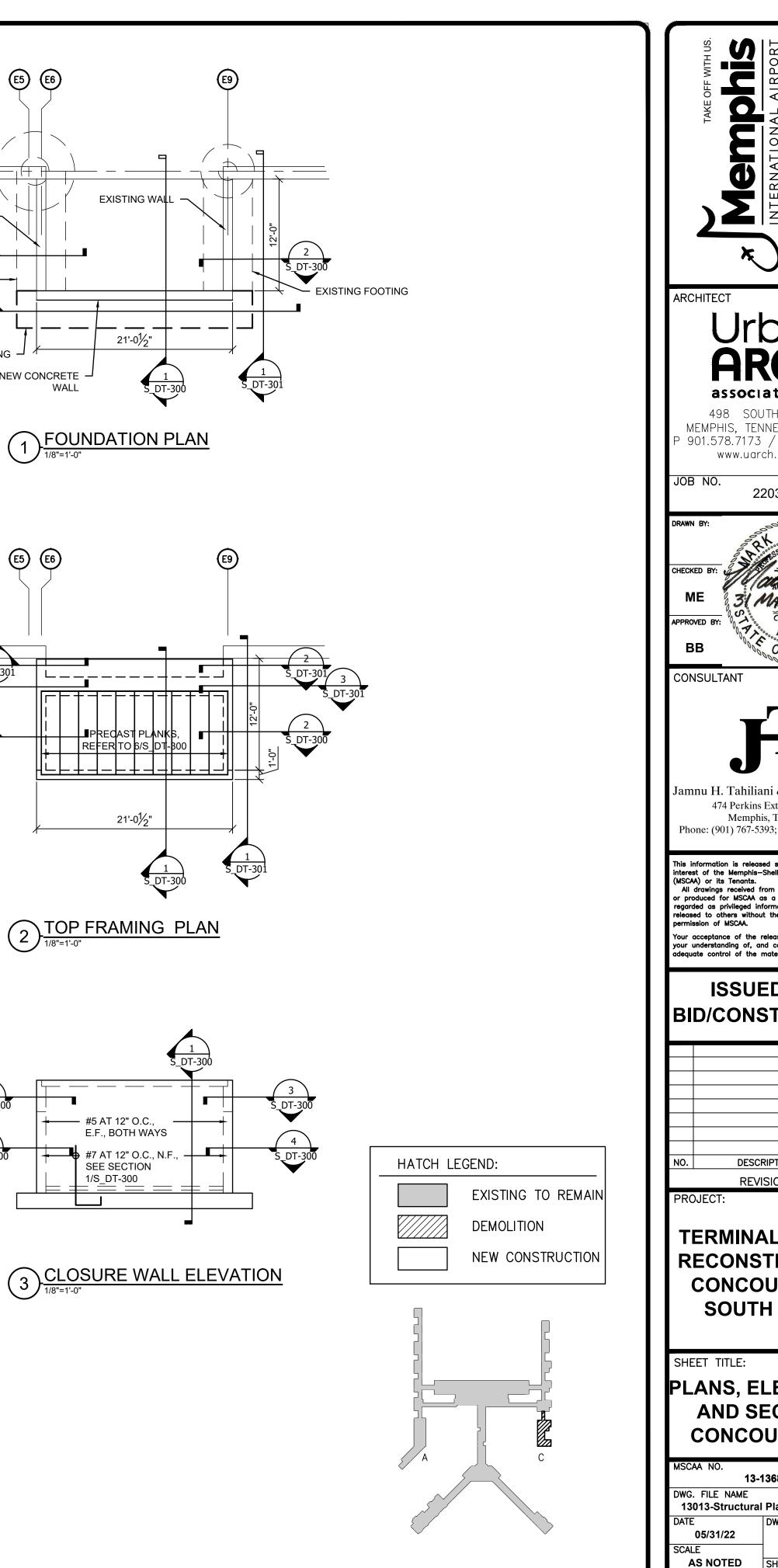
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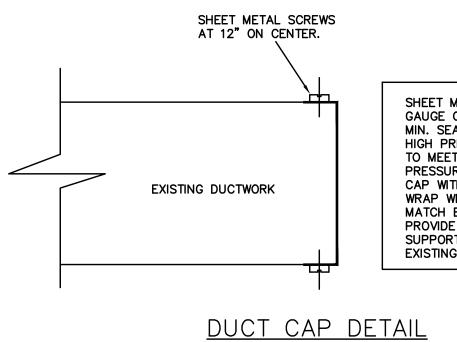








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ARCHITECT Urban associates, pc. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
DRAWN BY: CHECKED BY: APPROVED BY: BB CONSULTANT
Jamnu H. Tahiliani & Associates, Inc. 474 Perkins Extd., Suite 211 Memphis, TN 38117 Phone: (901) 767-5393; Fax: (901) 767-2510
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NOT TO SCALE

SHEET METAL CAP TO MATCH GAUGE OF EXISTING DUCT AT MIN. SEAL CAP TO DUCT WITH HIGH PRESSURE DUCT SEALANT TO MEET SMACNA 10" W.G. PRESSURE STANDARDS. COVER CAP WITH 2" 1.0 PCF DUCT WRAP WITH SILVER COVER TO MATCH EXISTING DUCT WRAP. PROVIDE NEW DUCT HANGER SUPPORT AT CAP TO MATCH EXISTING TRAPEZE DUCT HANGERS. MECHANICAL GENERAL DEMOLITION NOTES: 1. DEMOLITION OF HVAC DUCTS AND PIPING SHALL BE BACK TO MAIN SUPPLY DUCT/PIPE OR REMOVED COMPLETELY UNLESS OTHERWISE NOTED

NOTED. 2. PROVIDE TEMPORARY SUPPORT FOR DUCTWORK AND PIPING AS REQUIRED.

3. HVAC DUCTWORK PATCHING, REROUTING, ETC. TO MATCH MATERIAL AND CONSTRUCTION OF EXISTING.

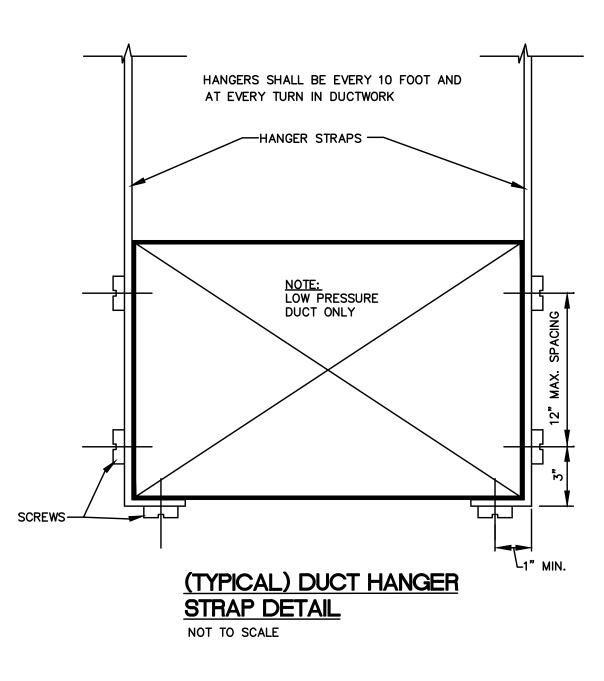
4. CONTRACTOR TO MAINTAIN OPERATION OF THE REMAINING HVAC SYSTEMS (AIR HANDLERS, CHILLED WATER SYSTEM, HOT WATER SYSTEM, ETC.) THROUGHOUT DEMOLITION PROCESS. COORDINATE ANY PLANNED OUTAGES WITH THE AIRPORT AUTHORITY BEFORE BEGINNING

ANY WORK. 5. CONTRACTOR TO INSPECT THE INSIDE OF ALL DUCTWORK TO BE REMOVED FOR MOLD BEFORE DEMOLITION BEGINS. IF MOLD IS PRESENT, THE CONTRACTOR IS TO FOLLOW ALL LOCAL CODE AND OSHA STANDARDS FOR PERSONEL PROTECTIVE GEAR AND REMOVAL AND DISPOSAL OF MOLD CONTAMINATED

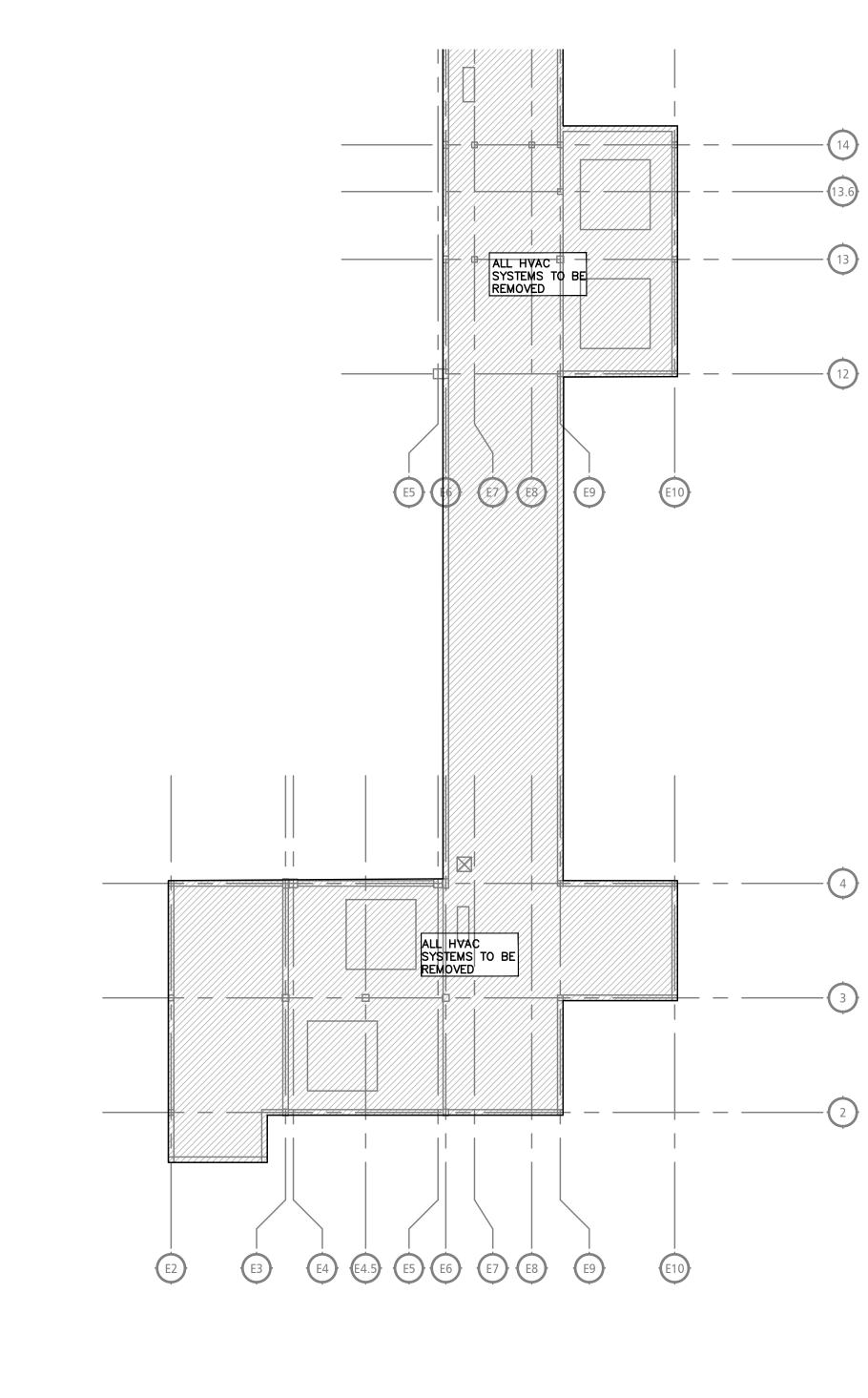
DUCTWORK. 6. CONTRACTOR TO PROPERLY DECOMISSION ALL EQUIPMENT TO BE REMOVED. COLLECT AND DISPOSE OF ANY OILS OR OTHER LIQUIDS AS REQUIRED BY

OF ANY OLS OR OTHER LIQUIDS AS REQUIRED BY LOCAL CODES.
7. CONTRACTOR TO MATCH EXISTING MATERIALS FOR DUCT CAPS, PIPING CAPS, DUCT INSULATION, AND PIPE INSULATION.
8. CONTRACTOR TO COMPLETELY TEST AND BALANCE ALL REMAINING AFFECTED HVAC SYSTEMS AT THE COMPLETION OF THE DEMOLITION WORK.
9. TACS, CORPESSIOND, TO NUMPERS, LOCATED, AT, OR, NO

9. TAGS CORRESPOND TO NUMBERS LOCATED AT OR NEAR THE CUT LINE ON EACH PIPE, DUCT, ETC. THIS IS REPRESENTATIVE OF CONDITIONS AT TIME OF INSPECTIONS. CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS PRIOR TO BEGINNING WORK AS CHANGES MAY HAVE BEEN MADE IN THE INTERIM BETWEEN DESIGN INSPECTION AND BEGINNING OF WORK.



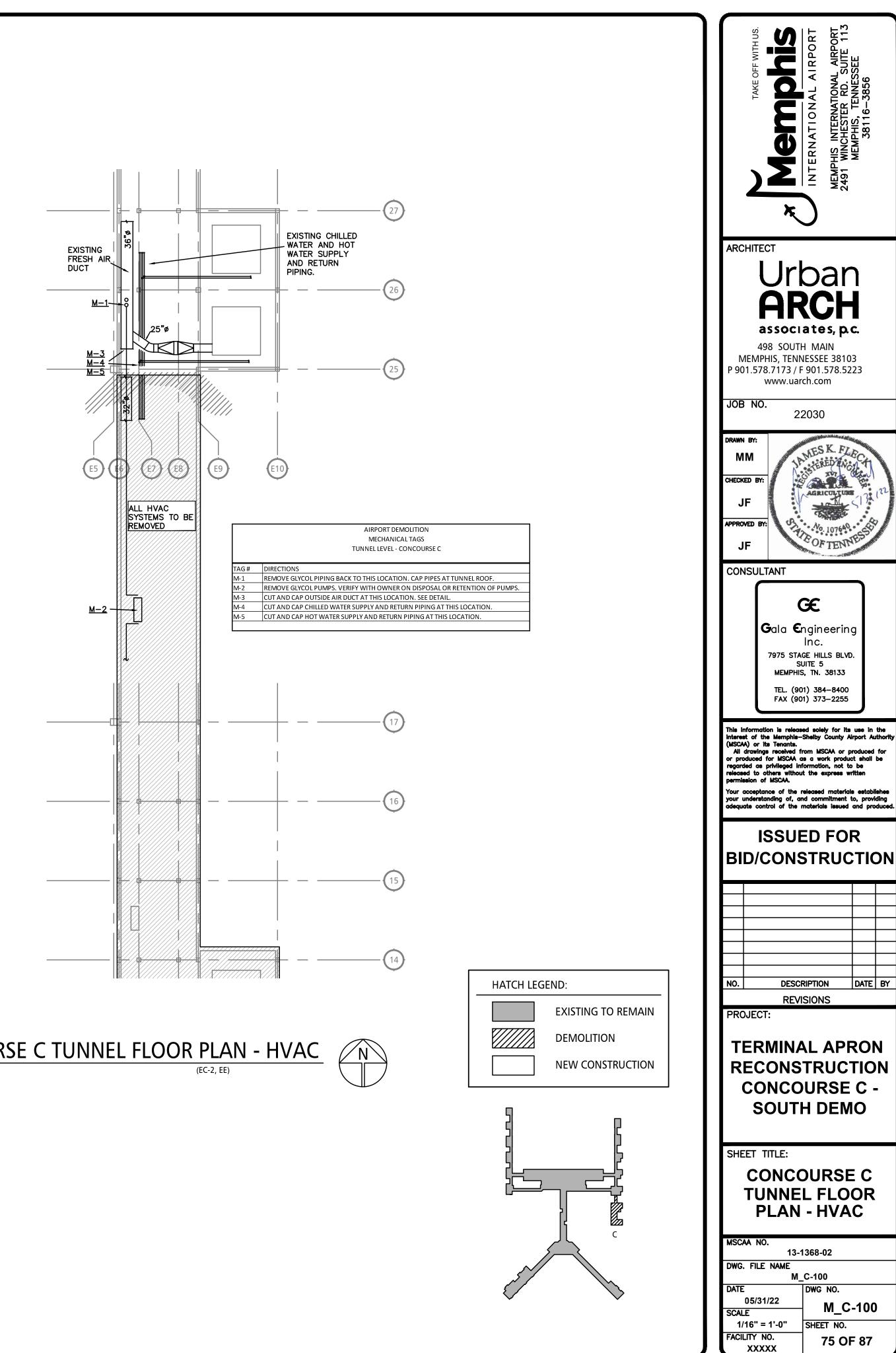
TAKE OFF WITH US.	A MEMPHIS INTERNATIONAL AIRPORT	MEMPHIS INTERNATIONAL AIRPORT 2491 WINCHESTER RD. SUITE 113	MEMPHIS, TENNESSEE 38116–3856	
ARCHITECT Urban associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com				
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interest of the (MSCAA) or its All drawings of or produced for regarded as pri- released to othe permission of M Your acceptance your understand adequate control	This information is released solely for its use in the interest of the Memphis–Shelby County Airport Authority (MSCAA) or its Tenants. All drawings received from MSCAA or produced for or produced for MSCAA as a work product shall be regarded as privileged information, not to be released to others without the express written permission of MSCAA. Your acceptance of the released materials establishes your understanding of, and commitment to, providing adequate control of the materials issued and produced.			for be shes ding
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### CONCOURSE C TUNNEL FLOOR PLAN - HVAC SCALE: 1/16" = 1' - 0" (EE, EC-1, ED)

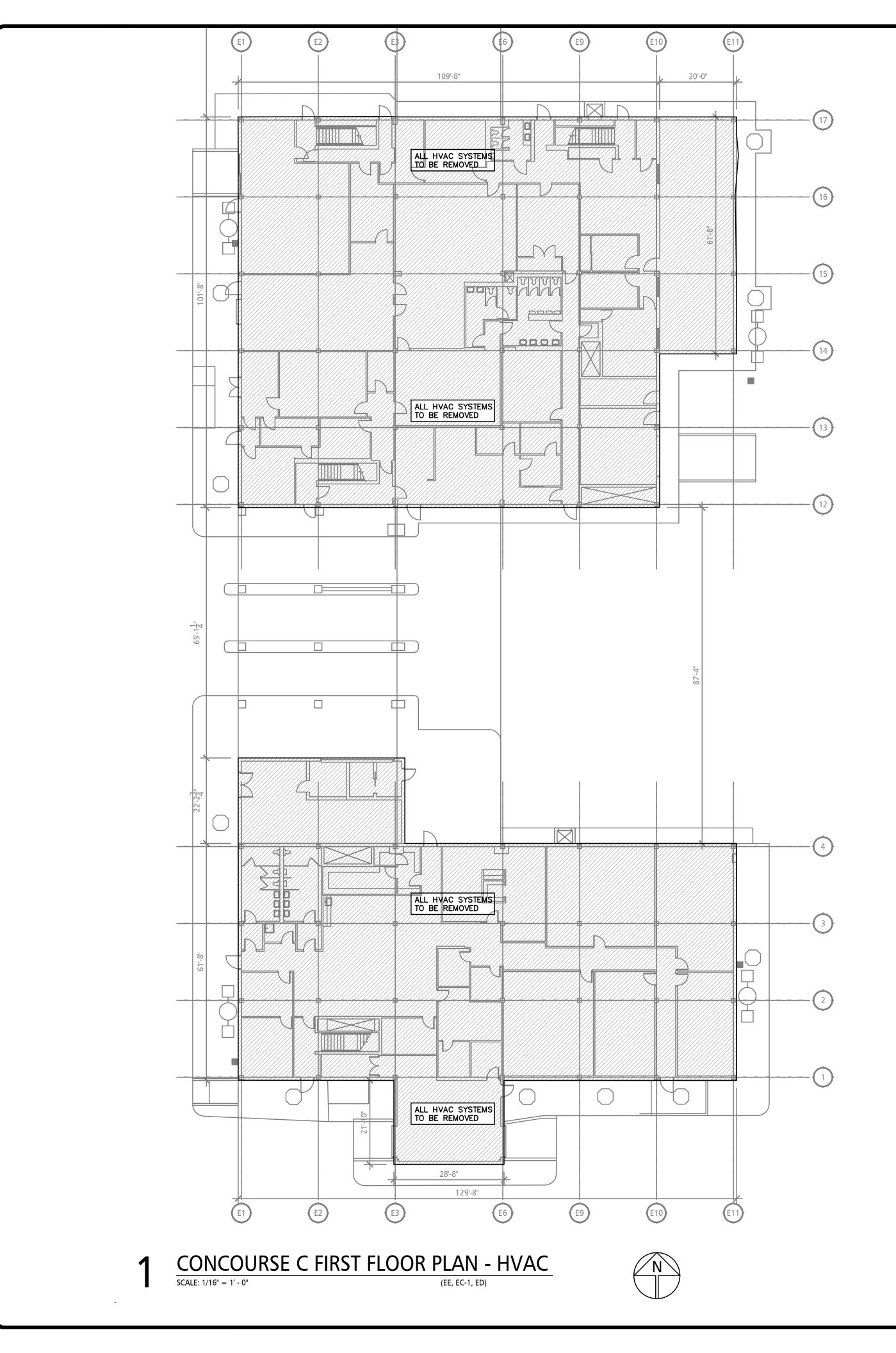
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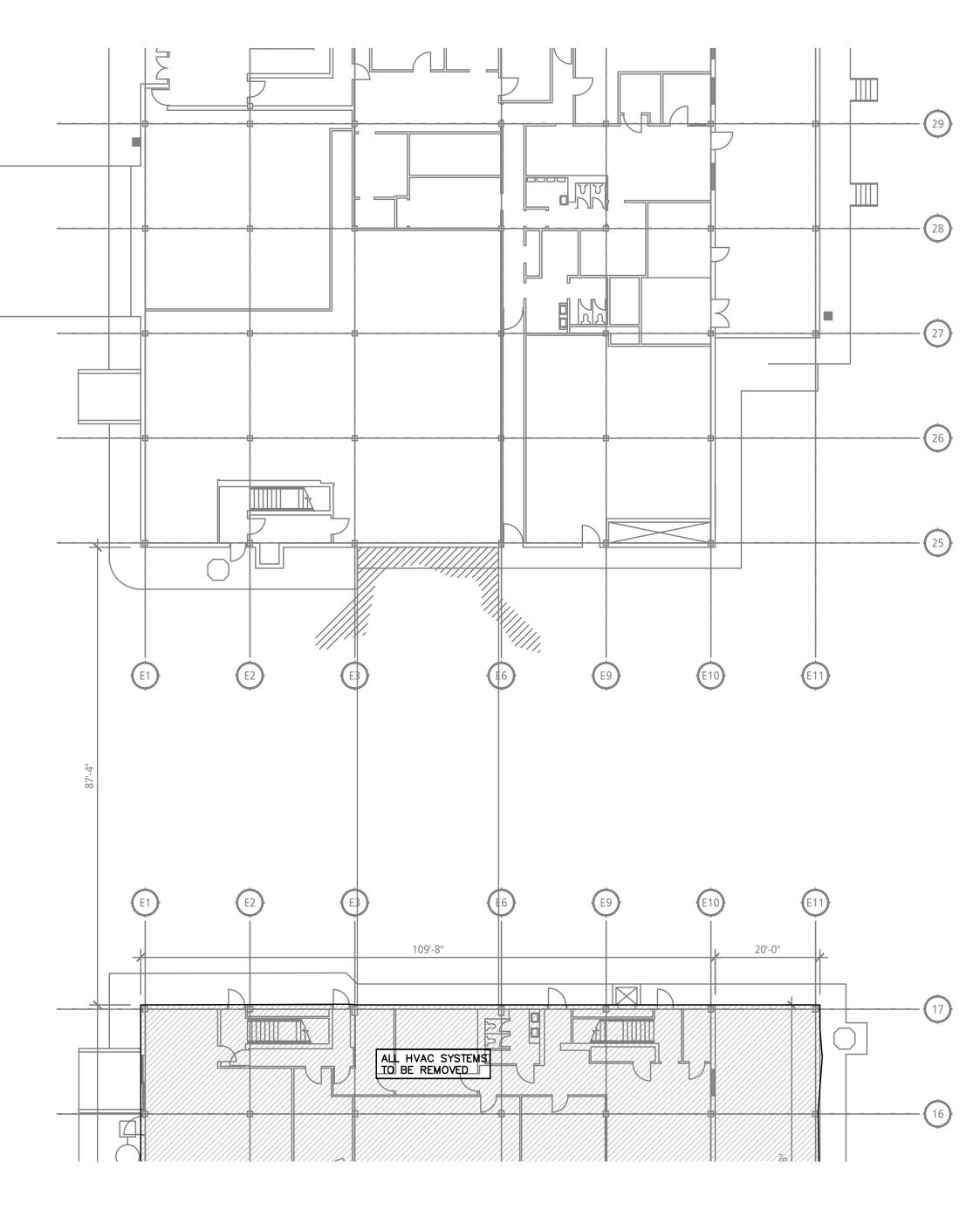






CONCOURSE C TUNNEL FLOOR PLAN - HVAC



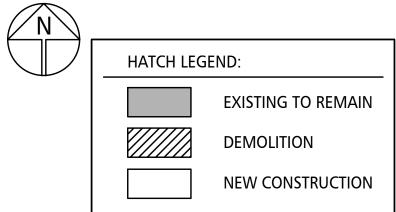


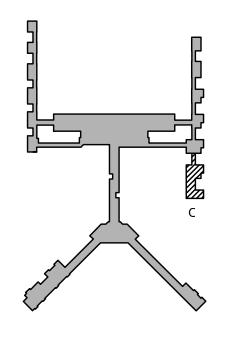


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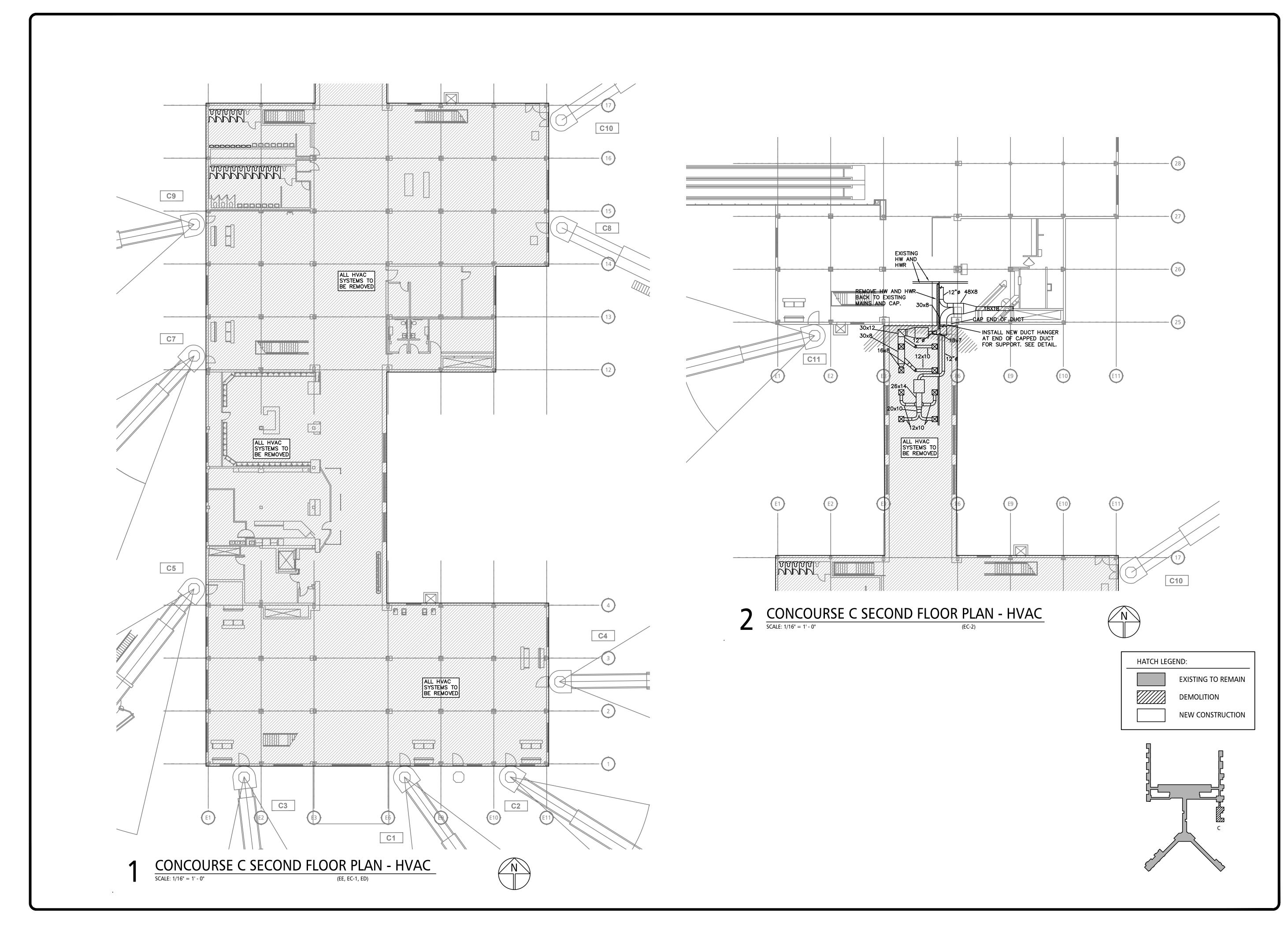
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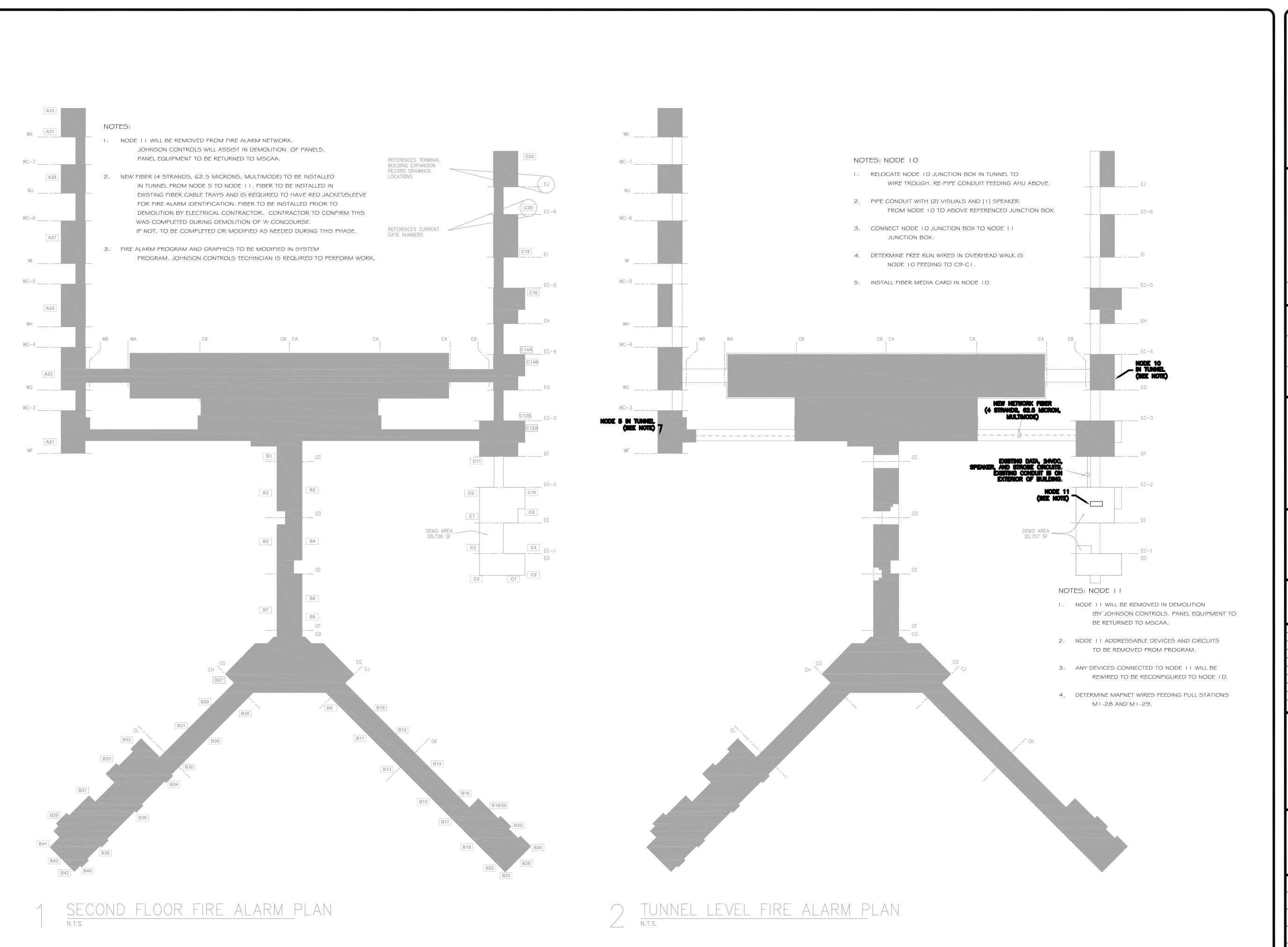




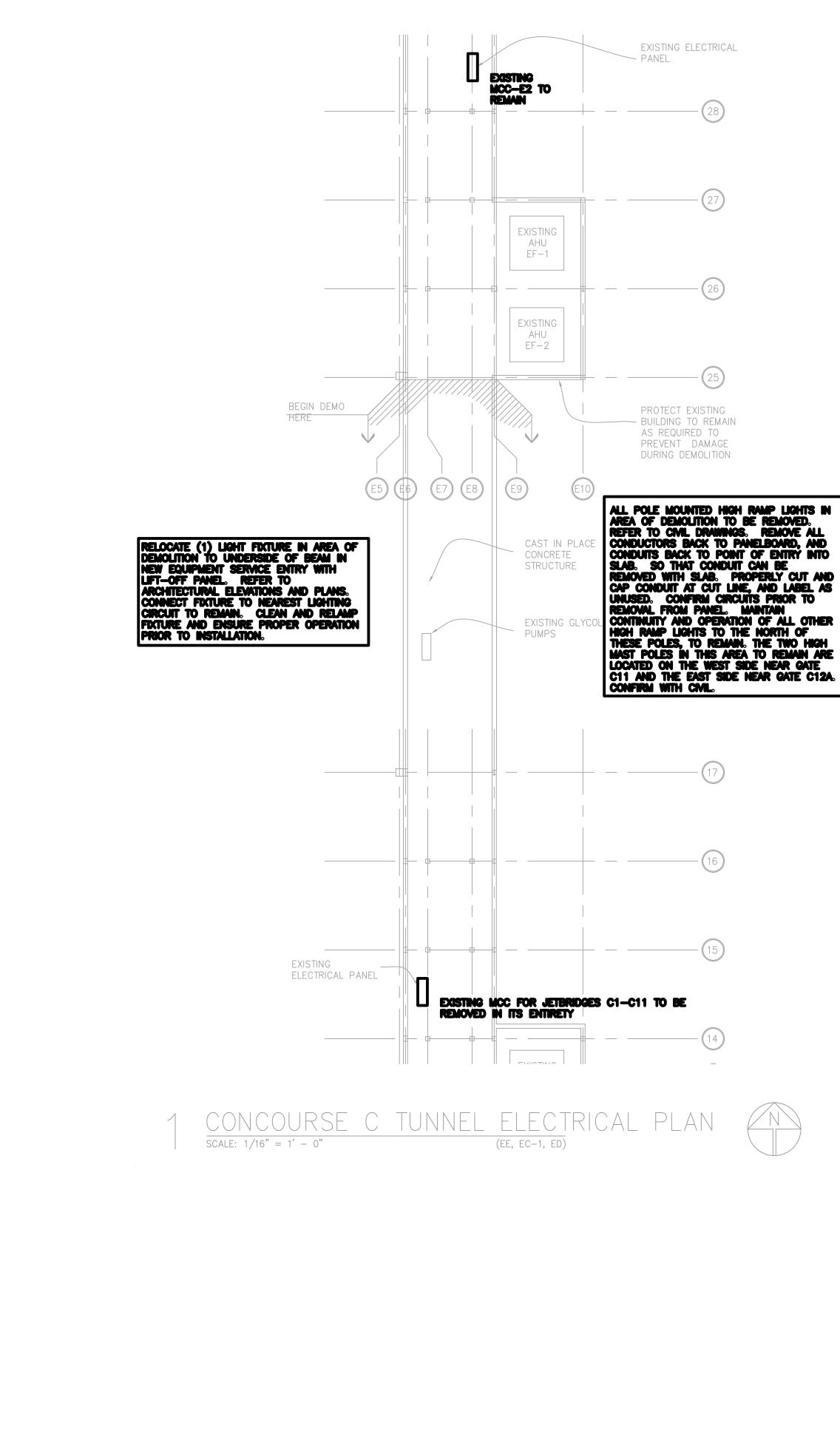
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ARCHITECT Urban associates, p.c. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com		
CHECKED BY: CHECKED BY: APPROVED BY: BB CHECKED BY: CHECKED BY:		
CONSULTANT CONSULTANT CONSULTANT Constance Hickey Scott P.E. Engineering Design and Consulting 7846 Wood Oak Cove Cordova, Tennessee (901) 830-5300 chscott@chsengineering.com		
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<b>BID/CONSTRUCTION</b>		
NO. DESCRIPTION DATE BY		
REVISIONS PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO		
SHEET TITLE: FIRE ALARM PLAN		
MSCAA NO.         13-1368-02         DWG. FILE NAME         FIRE ALARM         DATE       DWG NO.         05/31/22       FA-100         SCALE       SHEET NO.         FACILITY NO.       TO OF 07		



### AIRPORT DEMOLITION ELECTRICAL TAGS **TUNNEL LEVEL - CONCOURSE C**

TAG #	DIRECTIONS	NEW PANEL/CKT	SHEET
1	Relocate all electrical feeding varsity grill to new location in area to remain		
2	Card Key System - door D6007 - relocate power and comm equipment to north		
3	remove 1" conduit and conductors from Panel EHX to north of cut line	MCCE2 SEC 2	FUEL V
4	Remove CCTV ground north to junction box closest to cut line		
5	Remove 1/2" EMT and conductors back to Panel EFX		
E	Remove back to the north to tee		
7	Remove 1/2" EMT and conductors back to junction box to the north		
8	Remove back to MCC-E2		
ç	1" EMT with fire alarm - remove to the nearest junction box north of cut line.		FA-100
10	Remove 1" rigid conduit and conductors back to substation #4		
11	Remove 3/4" EMT and conductors back to the nearest junction box north of cut line		
12	Remove 2" EMT and conductors back to substation #4		
13	Remove 2" EMT and conductors back to substation #4		
14	Remove 2" EMT and conductors back to substation #4		
15	Remove 3" EMTand conductors back to substation #4		
16	Remove 2" rigid conduitand conductors back to substation #4		
17	Remove 1" conduit and conductors - unused		
	Remove feed to (3) American Airlines chargers, from Panel DPEJW1, located in		
18	demolition area		
19	Remove back to Substation #4 MCC-E2		
20	Remove 1-1/4" conduit back to Substation #4 MCC-E2		
21	Remove 1-1/4" conduit back to Substation #4 MCC-E2		
22	Remove 1-1/4" conduit back to Substation #4 MCC-E2		
23	Remove 3/4" conduit back to Substation #4 MCC-E2		
24	Remove 1" conduit back to Substation #4 MCC-E2		
25	Remove 1" rigid conduit back to Panel DPEJW1		
26	Remove 2" rigid conduit and condcutors feeding jetway C11		
27	Remove back to nearest junction box north of cut line		FA-100
28	Remove to substation/mcc #4		
29	Remove to substation/mcc #4		
30	Remove to substation/mcc #4		
31	Remove to substation/mcc #4		
32	Remove empty 2" EMT entirely		
33	Relocate ACAMS going up if in use		
34	Sluice gate	MCCE2 2-1	E-C302

## ELECTRICAL NOTES:

ALL WORK SHALL COMPLY WITH THE REQUIREMENTS OF THE LATEST ADOPTED EDITION OF THE NEC, INCLUDING ALL MEMPHIS AND SHELBY COUNTY AMENDMENTS, AND ALL MSCAA REQUIREMENTS.

2. CONTRACTOR SHALL REMOVE ALL ELECTRICAL LOCATED TO THE SOUTH OF THE DEMO LINE INDICATED.

3. CONTRACTOR SHALL VERIFY ALL EXISTING ELECTRICAL IN THIS AREA PRIOR TO THE START OF WORK. LIST OF EXISTING EQUIPMENT, CIRCUITS, UTILITIES IS NOT NECESSARILY ALL-INCLUSIVE, ALTHOUGH IT IS INTENDED TO BE USED AS A GUIDE DURING RE-ROUTING AND PUTBACK OF ALL BUILDING UTILITIES. CONTRACTOR SHALL COMPLETE ALL WORK REQUIRED TO OBTAIN A COMPLETE, FULLY OPERATIONAL, ELECTRICAL SYSTEM FOR THE PORTION OF THE BUILDING TO REMAIN.

4. CONTRACTOR SHALL PROVIDE AS-BUILT DRAWINGS INDICATING FINAL CIRCUITING OF MCC-E2. CONTRACTOR SHALL ALSO PROVIDE AS-BUILT DOCUMENTATION OF FINAL CIRCUITING OF MCC #4.

5. FOR CIRCUITS REMOVED IN THEIR ENTIRETY, CONTRACTOR SHALL REMOVE ALL CONDUIT, CONDUCTORS, AND SUPPORTS BACK TO PANELBOARD TO REMAIN. LABEL AS 'SPARE'.

6. FOR PANELBOARDS REMOVED IN THEIR ENTIRETY, CONTRACTOR SHALL REMOVE ALL CONDUIT, CONDUCTORS, SUPPORTS AND ACCESSORIES BACK TO SWITCHBOARD FROM WHICH PANELBOARD WAS FED. RELABEL SWITCHBOARD AS PER #5 ABOVE.

7. THIS DRAWING DOES NOT INDICATE ALL ELECTRICAL IN THE DEMOLITION AREA, ONLY THAT WHICH IS TO REMAIN AND WILL BE AFFECTED BY THE DEMOLITION.

8. REFER TO TABLE ABOVE FOR RELOCATION OF ELECTRICAL LINE AND LOW VOLTAGE THAT MUST BE RE-FED. TAGS CORRESPONDING TO THESE NUMBERS ARE LOCATED AT OR NEAR THE CUT LINE ON EACH CONDUIT OR CABLE. THIS IS REPRESENTATIVE OF CONDITIONS AT TIME OF INSPECTION. CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO BEGINNING OF WORK, AS CHANGES MAY HAVE BEEN MADE IN THE INTERIM BETWEEN DESIGN INSPECTION AND BEGINNING OF WORK.

10. ELECTRICAL MUST BE RE-ROUTED AND ALL CIRCUITS RE-ENERGIZED, WITH PROPER OPERATION CONFIRMED, PRIOR TO ANY DEMOLITION.

11. JOHNSON CONTROLS/SIMPLEX FIRE ALARM CONTROL PANEL IS LOCATED IN THE AREA OF DEMOLITION. REFER TO SHEET FA-100 AND COORDINATE ALL FIRE ALARM RELATED WORK WITH JOHNSON CONTROLS.

# LOW VOLTAGE SYSTEM NOTES

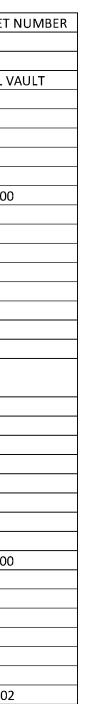
THE FOLLOWING WILL BE REMOVED BY OTHERS PRIOR TO THE **BEGINNING OF CONSTRUCTION:** 

- 1. BY ATT: • All equipment that will no longer be in service. Cabling to be removed by contractor.
- 4. BY OTHERS:
- All airport clocks. Cabling shall be removed by this contractor back to last device remaining.
- Johnson Controls Energy Management System. ONE SINGLE FLEXIBLE ORANGE CONDUIT FOR THIS SYSTEM SHALL REMAIN. THAT CONDUIT WILL BE IDENTIFIED PRIOR TO BEGINNING WORK. All other associated cabling and conduit shall be removed.

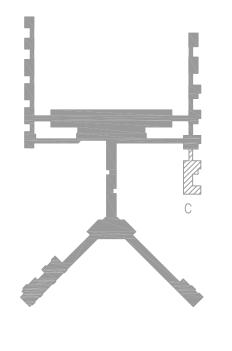
THE FOLLOWING SHALL BE REMOVED BY THIS CONTRACTOR: 1. Cabling related to Johnson Controls/Simplex indicated above. 2. For the Boingo Wireless system contractor shall remove all obsolete access

- points(located above ceiling, concourse level) and associated cabling. 3. All obsolete CCTV, security, and access control equipment and cabling. All
- equipment shall be removed by MSCAA. 4. All other low voltage cabling not mentioned herein. Coordinate with MSCAA
- IT Department. All equipment in any data room to be demolished shall be removed by MSCAA.

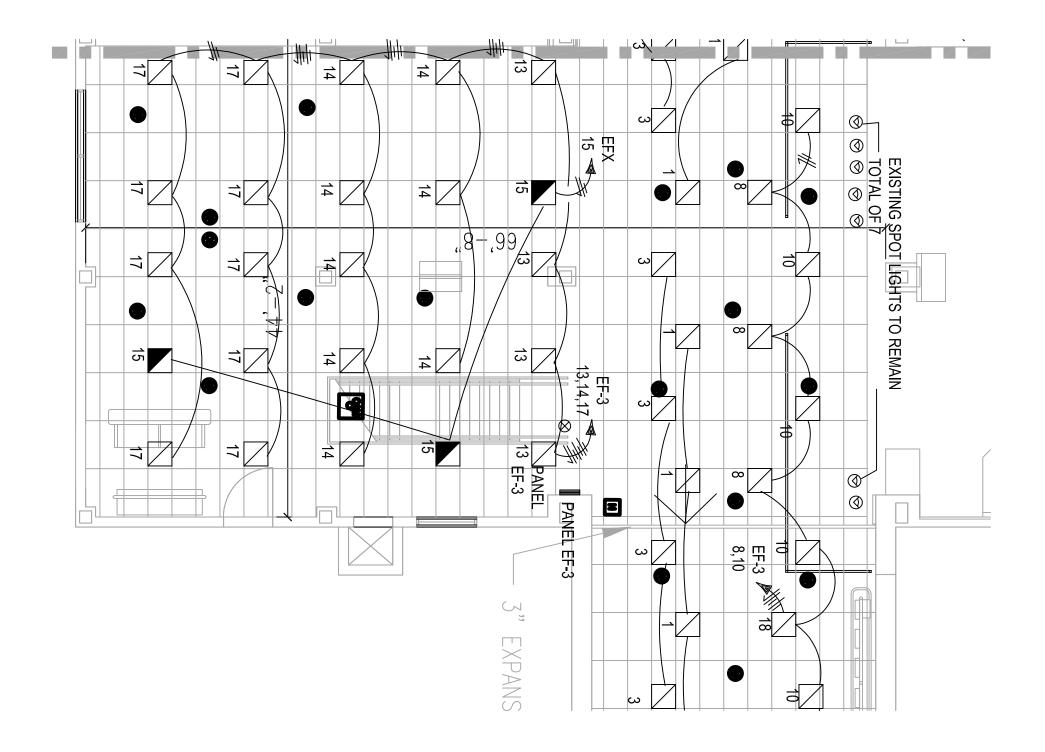
REMOVAL OF THE EXISTING CABLING AND CABLE TRAY PAST THE CUT LINE. ALL CABLING IN THE TRAY THAT IS NOT REMOVED BY OTHER AIRPORT VENDORS TO BE TESTED AND TONED. ANY ABANDONED CABLING SHOULD BE REMOVED BACK TO THE CLOSEST SOURCE OR TERMINATION.

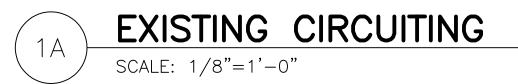


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ARCHITECT Urban ARCHITECT ARCHI ASSOCIATES, P.C. 498 SOUTH MAIN MEMPHIS, TENNESSEE 38103 P 901.578.7173 / F 901.578.5223 www.uarch.com
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NO. DESCRIPTION DATE BY REVISIONS PROJECT:
TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
MSCAA NO. 13-1368-02 DWG. FILE NAME 13034_MSCAA COVER SHEET DATE DWG NO.





### **ELECTRICAL NOTES:**

1. ALL WORK SHALL COMPLY WITH THE REQUIREMENTS OF THE LATEST ADOPTED EDITION OF THE NEC, INCLUDING ALL MEMPHIS AND SHELBY COUNTY AMENDMENTS, AND ALL MSCAA REQUIREMENTS.

2. CONTRACTOR SHALL REMOVE ALL ELECTRICAL LOCATED TO THE SOUTH OF THE DEMO LINE INDICATED.

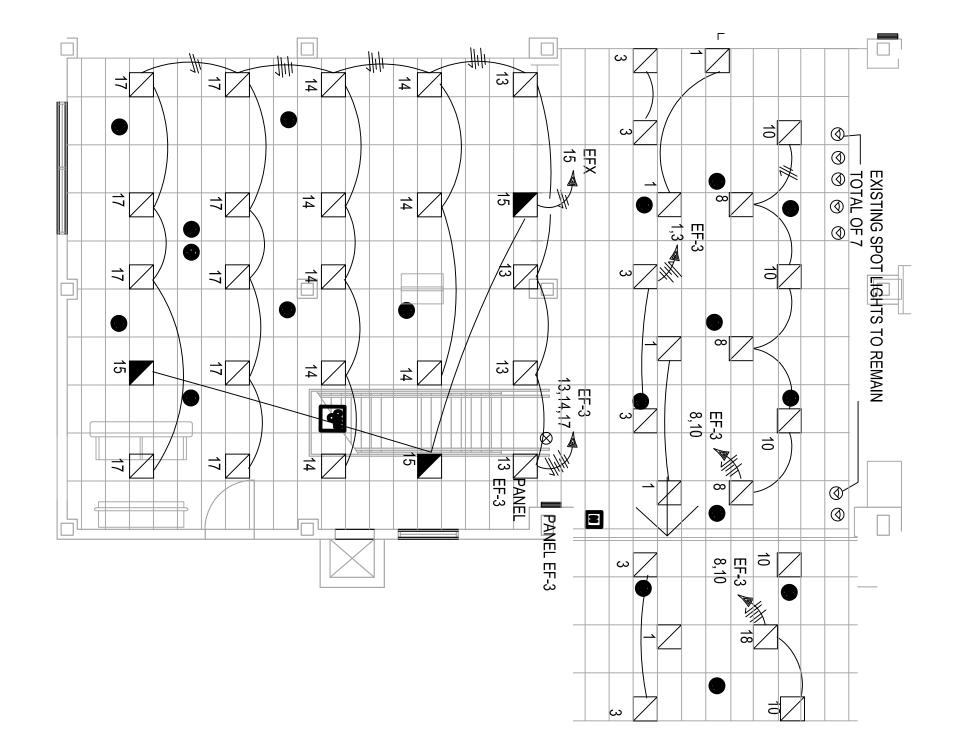
3. AT LINE, REFEED TEN(10) LIGHTING FIXTURES IN PANEL 'EF-3' AS INDICATED.

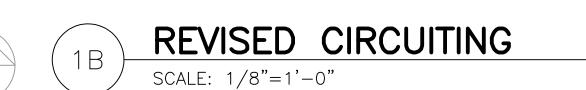
4. EMERGENCY FIXTURES TO REMAIN THIS AREA(TOTAL OF THREE) SHALL BE CIRCUITED TO WEX-15, AS PER CURRENT.

5. CONTRACTOR SHALL PROVIDE NEW PANEL DIRECTORY FOR PANEL WE-3 AND WEX INDICATING REVISED CIRCUITS AND LABELING THOSE NO LONGER USED AS 'SPARE'. DIRECTORIES SHALL BE TYPEWRITTEN. CONTRACTOR SHALL PROVIDE COPY OF REVISED DIRECTORY CARDS WITH CLOSEOUT RECORD DOCUMENTS.

6. FOR CIRCUITS REMOVED IN THEIR ENTIRETY, CONTRACTOR SHALL REMOVE ALL CONDUIT, CONDUCTORS, AND SUPPORTS BACK TO PANELBOARD TO REMAIN.

7. FOR PANELBOARDS REMOVED IN THEIR ENTIRETY, CONTRACTOR SHALL REMOVE ALL CONDUIT, CONDUCTORS, SUPPORTS AND ACCESSORIES BACK TO SWITCHBOARD FROM WHICH PANELBOARD WAS FED. RELABEL SWITCHBOARD AS PER #5 ABOVE.





THE FOLLOWING WILL BE REMOVED BY OTHERS PRIOR TO THE BEGINNING OF CONSTRUCTION:

- 1. BY ATT:
- 4. BY OTHERS: remaining.
- removed.

THE FOLLOWING SHALL BE REMOVED BY THIS CONTRACTOR:

MSCAA.

## LOW VOLTAGE SYSTEM NOTES

• All equipment that will no longer be in service. Cabling to be removed by contractor.

All airport clocks. Cabling shall be removed by this contractor back to last device

• Johnson Controls Energy Management System. ONE SINGLE FLEXIBLE ORANGE CONDUIT FOR THIS SYSTEM SHALL REMAIN. THAT CONDUIT WILL BE IDENTIFIED PRIOR TO BEGINNING WORK. All other associated cabling and conduit shall be

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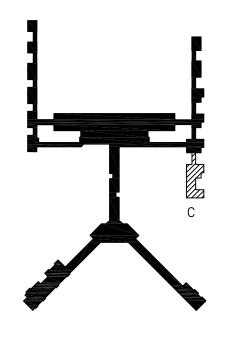
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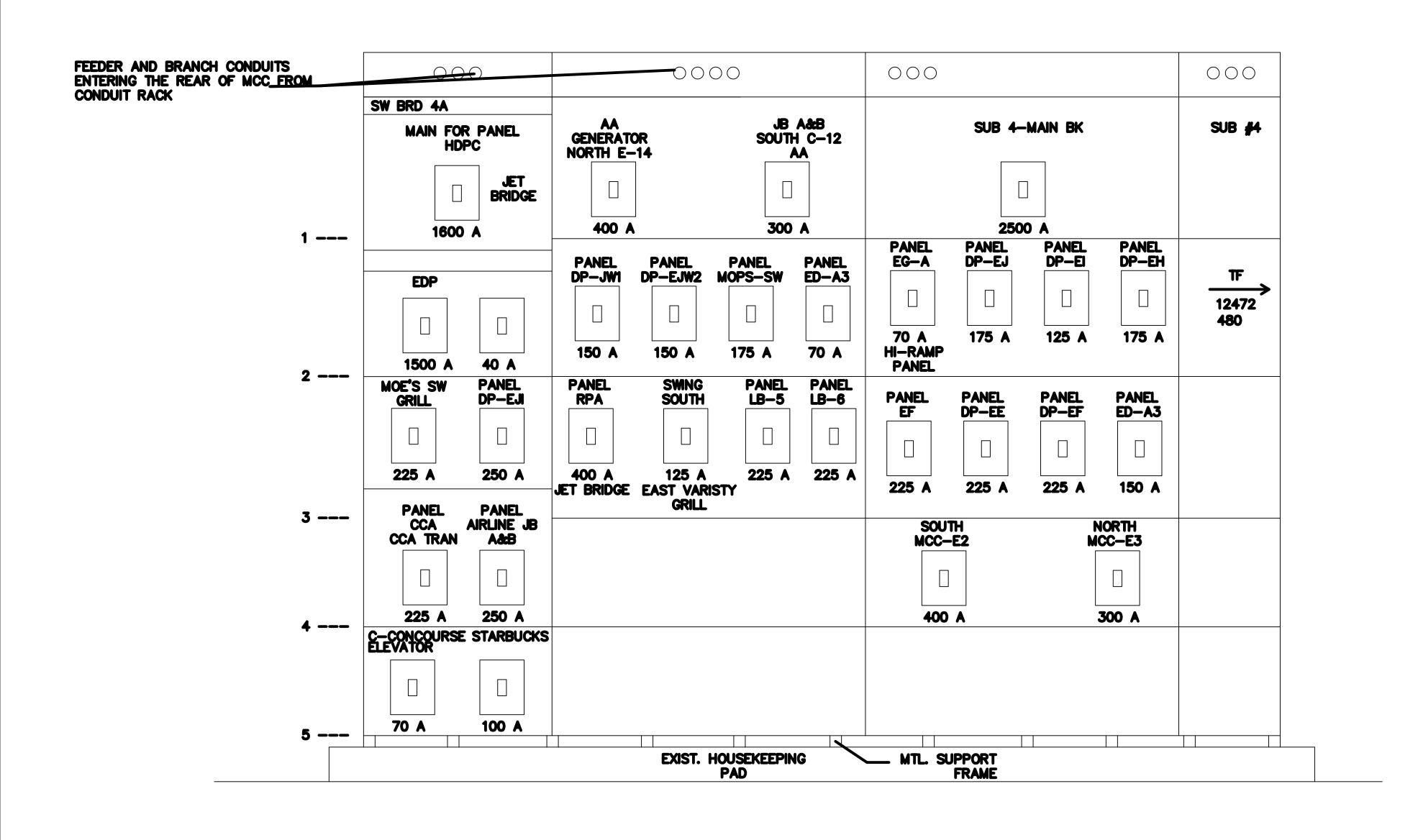
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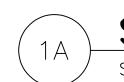
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MSCAA NO.       13-1368-02         DWG. FILE NAME       DWG NO.         DATE       DWG NO.         05/31/22       DWG NO.         SCALE       DWG NO.         1/8" = 1'-0"       SHEET NO.         FACILITY NO.       80 OF 87		





1. ALL WORK SHALL COMPLY WITH THE REQUIREMENTS OF THE LATEST ADOPTED EDITION OF THE NEC, INCLUDING ALL MEMPHIS AND SHELBY COUNTY AMENDMENTS, AND ALL MSCAA REQUIREMENTS.

2. AFFECTED SECTIONS OF SWITCHBOARD #4A ONLY INDICATED HEREIN.

DEMOLITION.

4. CONTRACTOR SHALL REMOVE THOSE DEVICES IN THEIR ENTIRETY, INCLUDING TRANSFORMERS, PANELBOARDS, CIRCUITS AND ALL DOWNSTREAM DEVICES. DEVICES INDICATED AS 'SPARE' IN FINAL CONFIGURATION ARE SOUTH OF THE DEMOLITION LINE.

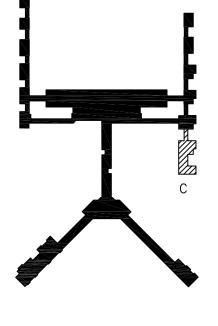
5. CONTRACTOR SHALL PROPERLY RELABEL SWITCHBORAD FOR FINAL CONFIGURATION. BREAKERS LABELED AS 'SPARE' SHALL BE LOCKED IN THE 'OFF' POSITION.

SCALE: N.T.S.

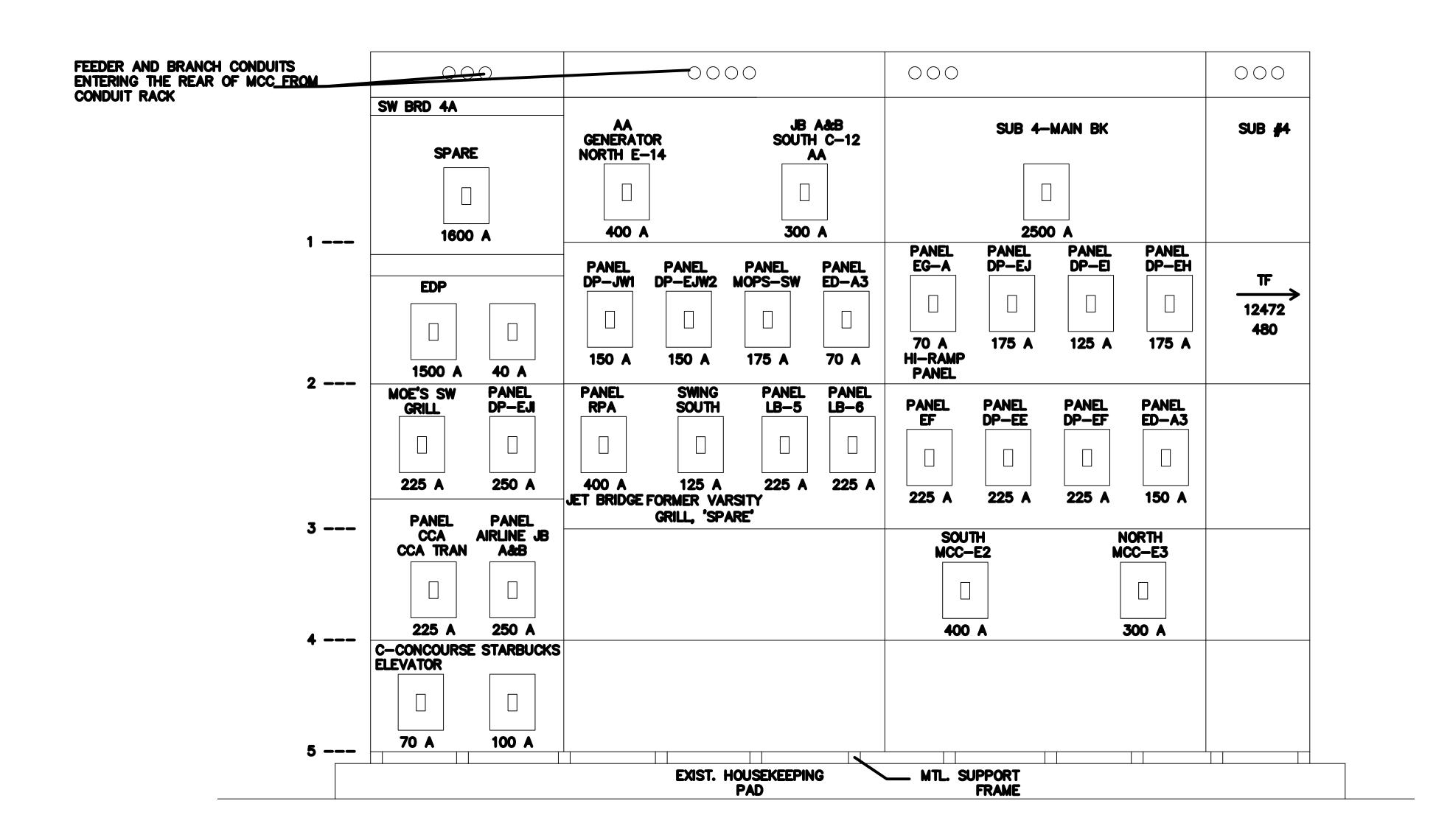
## **ELECTRICAL NOTES:**

3. CONTRACTOR SHALL REMOVE ALL CONDUIT, CONDUCTORS, SUPPORTS, AND ACCESSORIES TO THE (6) SIX DOWNSTREAM DEVICES LABELED AS 'SPARE' AFTER

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2. AFFECTED SECTIONS OF SWITCHBOARD #4A ONLY INDICATED HEREIN.

3. CONTRACTOR SHALL REMOVE ALL CONDUIT, CONDUCTORS, SUPPORTS, AND ACCESSORIES TO THE (6) SIX DOWNSTREAM DEVICES LABELED AS 'SPARE' AFTER DEMOLITION.

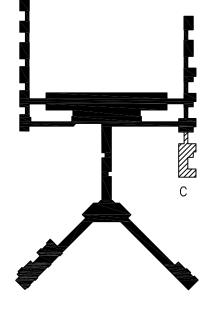
4. CONTRACTOR SHALL REMOVE THOSE DEVICES IN THEIR ENTIRETY, INCLUDING TRANSFORMERS, PANELBOARDS, CIRCUITS AND ALL DOWNSTREAM DEVICES. DEVICES INDICATED AS 'SPARE' IN FINAL CONFIGURATION ARE SOUTH OF THE DEMOLITION LINE.

5. CONTRACTOR SHALL PROPERLY RELABEL SWITCHBORAD FOR FINAL CONFIGURATION. BREAKERS LABELED AS 'SPARE' SHALL BE LOCKED IN THE 'OFF' POSITION.

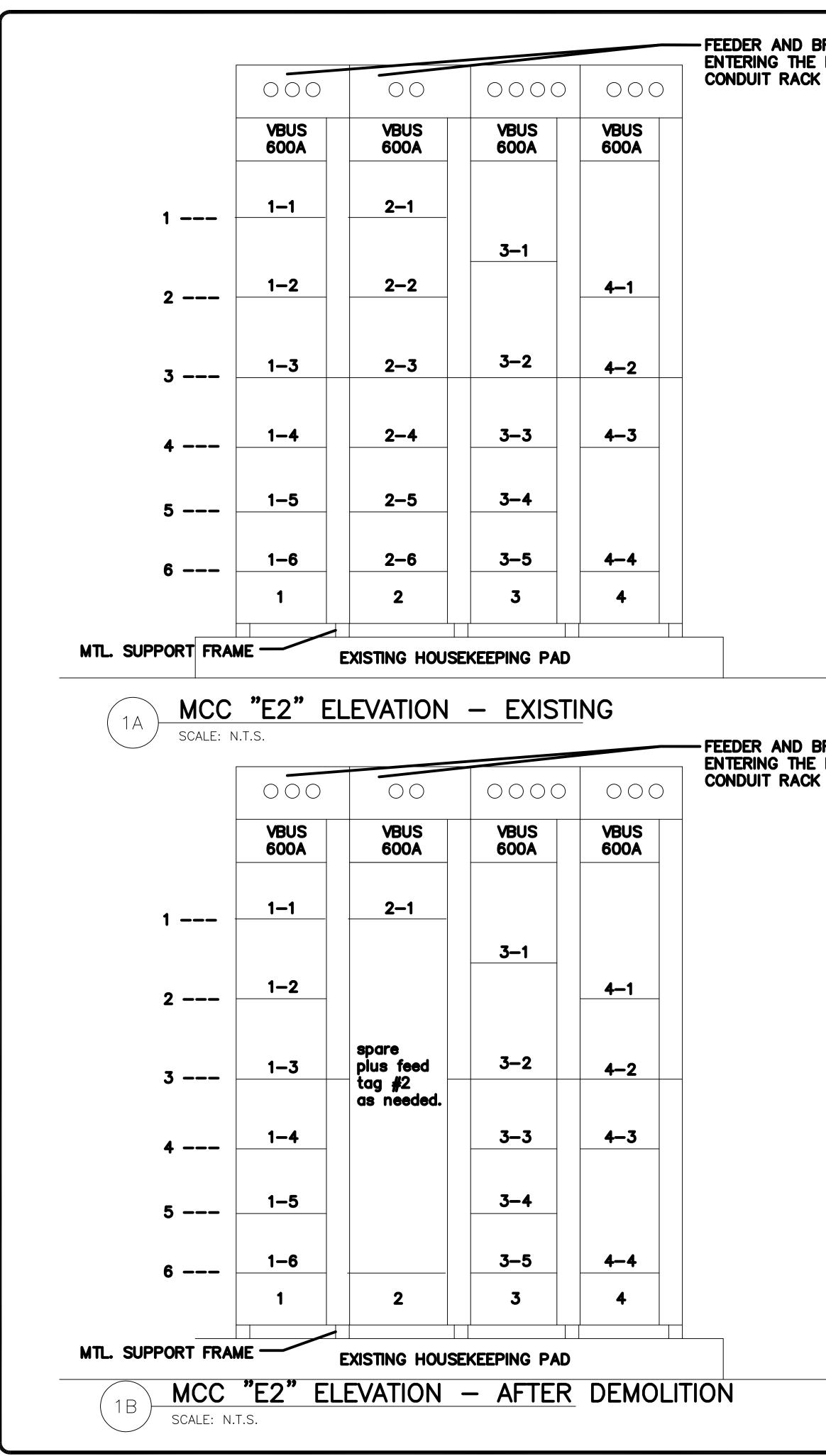
# SWITCHBOARD #4A - AFTER DEMOLITION

## **ELECTRICAL NOTES:**

HATCH LE	GEND:
	EXISTING TO REM
	DEMOLITION
	NEW CONSTRUCTI



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NO. DESCRIPTION DATE BY REVISIONS					
MSCAA NO. 13-1368-02 DWG. FILE NAME DATE 05/31/22 SCALE 1/8" = 1'-0" FACILITY NO. DUG NO. E_C-301 SHEET NO. CONTROL					



### FEEDER AND BRANCH CONDUIS ENTERING THE REAR OF MCC FROM CONDUIT RACK

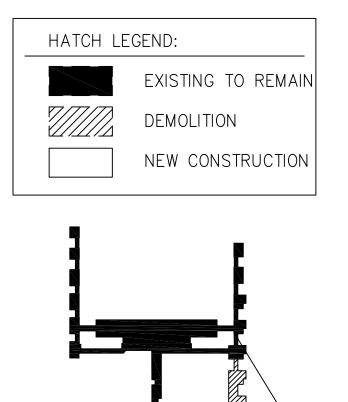
MC	C E2				Fed From S	ubstation #4		
Section	Device	Name of Load	NEMA Starter Size	BREAKER	HOA Equip.	Load In Season	Disposition	Comments
1	1	AHU EF-1	2	50/3	Yes	365	To Remain	
1	2	CP 4-1 South	1	20/3	Yes	365	To Remain	
1	3	Phase Monitor	None	20/3	No	365	To Remain	
1	4	Pump EF-1	1	15/3	Yes	Winter	To Remain	
1	5	R.F. EG-2	None	20/3	No	N/A	Nothing	Not In Service
1	6	Pump EG-1	1	15/3	Yes	Winter	To Remain	
2	1	AHU ED-1	1	30/3	Yes	365	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR FROM PANELBOARD TO EQUIPMENT
2	2	Pump ED-1	1	15/3	Yes	Winter	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR FROM PANELBOARD TO EQUIPMENT
2	3	R.F. ED-2	1	20/3	Yes	N/A	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR FROM PANELBOARD TO EQUIPMENT
2	4	AHU EE-1	2	50/3	Yes	365	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR FROM PANELBOARD TO EQUIPMENT
2	5	R.F. EE-2	1	20/3	Yes	N/A	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR
2	6	Pump EE-1	1	15/3	Yes	Winter	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR FROM PANELBOARD TO EQUIPMENT
3	1	AHU ED-2	3	100/3	Yes	365	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR FROM PANELBOARD TO EQUIPMENT
3	2	AHU EG-2	3	100/3	Yes	365	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR FROM PANELBOARD TO EQUIPMENT
3	3	Domestic HW Pump @ SS #4	1	20/3	Yes	365	To Remain	
3	4	EF EG-1	1	20/3	Yes	365	Same	Not Running
3	5	EF EG-2	1	20/3	Yes	365	Same	Not Running
4	1	AHU EF-2	4	150/3	Yes	365	To Remain	
4	2	EF EG-3	1	20/3	Yes	365	Same	Not Running
4	3	CP 4-2 North	1	20/3	Yes	N/A	Nothing	Not In Service
4	4	AHU EE-2	4	150/3	Yes	365	SPARE	REMOVE ALL CONDUIT AND CONDUCTOR FROM PANELBOARD TO EQUIPMENT

2-1, CURRENTLY FEEDING AHU ED-1 SHALL BE RE-USED TO FEED EXISTING SLUICE GATE. CONFIRM CIRCUIT BREAKER SIZE AND ADJUST IF NEEDED. FEED WITH 3#10, #10G, 2"C.

USE A SECOND SPARE IN SECTION 2 TO FEED TAG #3. MATCH EXISTING.



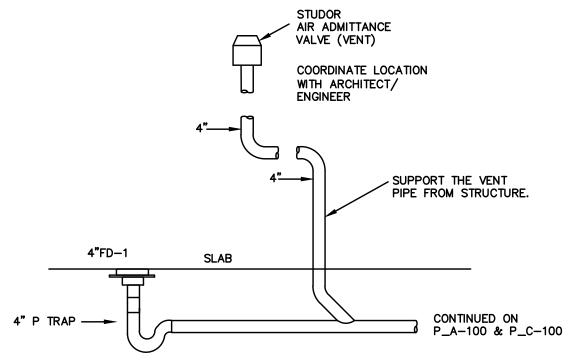
-FEEDER AND BRANCH CONDUIS ENTERING THE REAR OF MCC FROM CONDUIT RACK

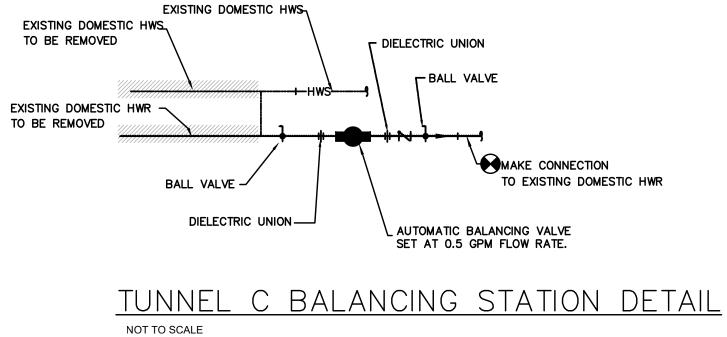


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22030 DRAWN BY: CHECKED BY: APPROVED BY: BB BB CHECKED BY: CHECKED
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<b>BID/CONSTRUCTION</b>
NO. DESCRIPTION DATE BY
REVISIONS PROJECT: TERMINAL APRON RECONSTRUCTION CONCOURSE C - SOUTH DEMO
SHEET TITLE: CONCOURSE C MCC-E2 DETAILS
MSCAA NO.       13-1368-02         DWG. FILE NAME       DWG NO.         DATE       DWG NO.         05/31/22       E_C-310         SCALE       SHEET NO.         FACILITY NO.       DOUT OF DOT

### PLUMBING DETAILS SCALE: NONE







### AIRPORT DEMOLITION PLUMBING TAGS

TAG #	DIRECTIONS
	Cap CW at this point. CW piping north of cap to remain. Refer to general
P1	demolition notes on plans for pipe removal instructions.
	Cap HW at this point. HW piping north of cap to remain. Refer to general
P2	demolition notes on plans for pipe removal instructions.
	Disconnect HWR at this point & install balancing valve. Make new connection
	to existing HWHWR north of balancing valve to remain. See detail. Refer to
Р3	general demolition notes for pipe removal instructions.
	Disconnect existing parallel RPZ remove all associated CW piping. Refer to
P4	general demolition notes for pipe removal instructions
P5	piping tagged to be removed.
P6	Provide permanent and temporary pipe supports as needed.
	All domestic water piping extentions from existing shall be made with sam
P7	pipe material as existing pipe material.
P8	Refer to general demolition notes on plans for gas pipe removal instruction
P9-P18	
P20-P23	Refer to General demolition notes for special requirements tagged at site.
P19	Natural gas supply piping tagged P19 to remain in service.

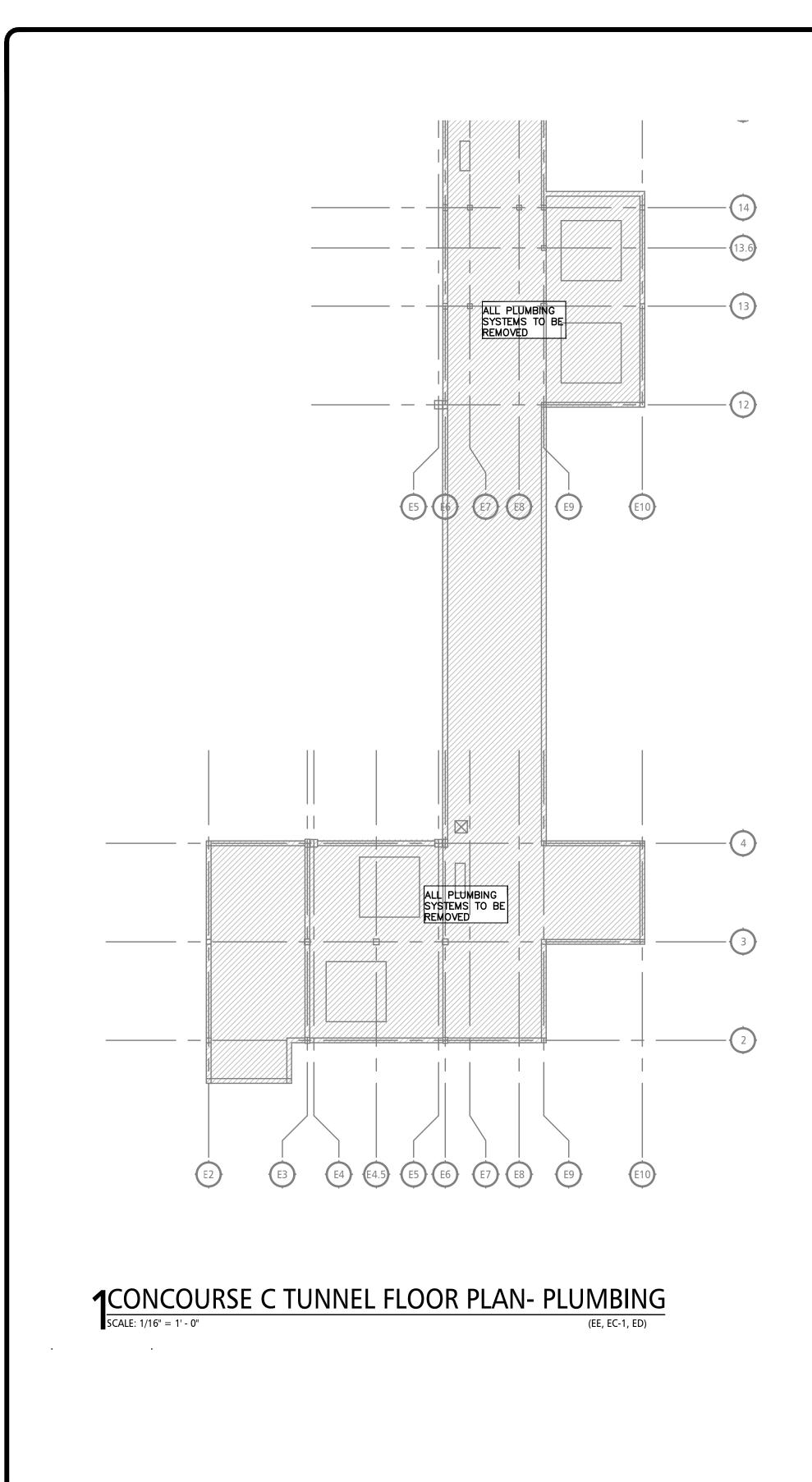
## PLUMBING SCHEDULE

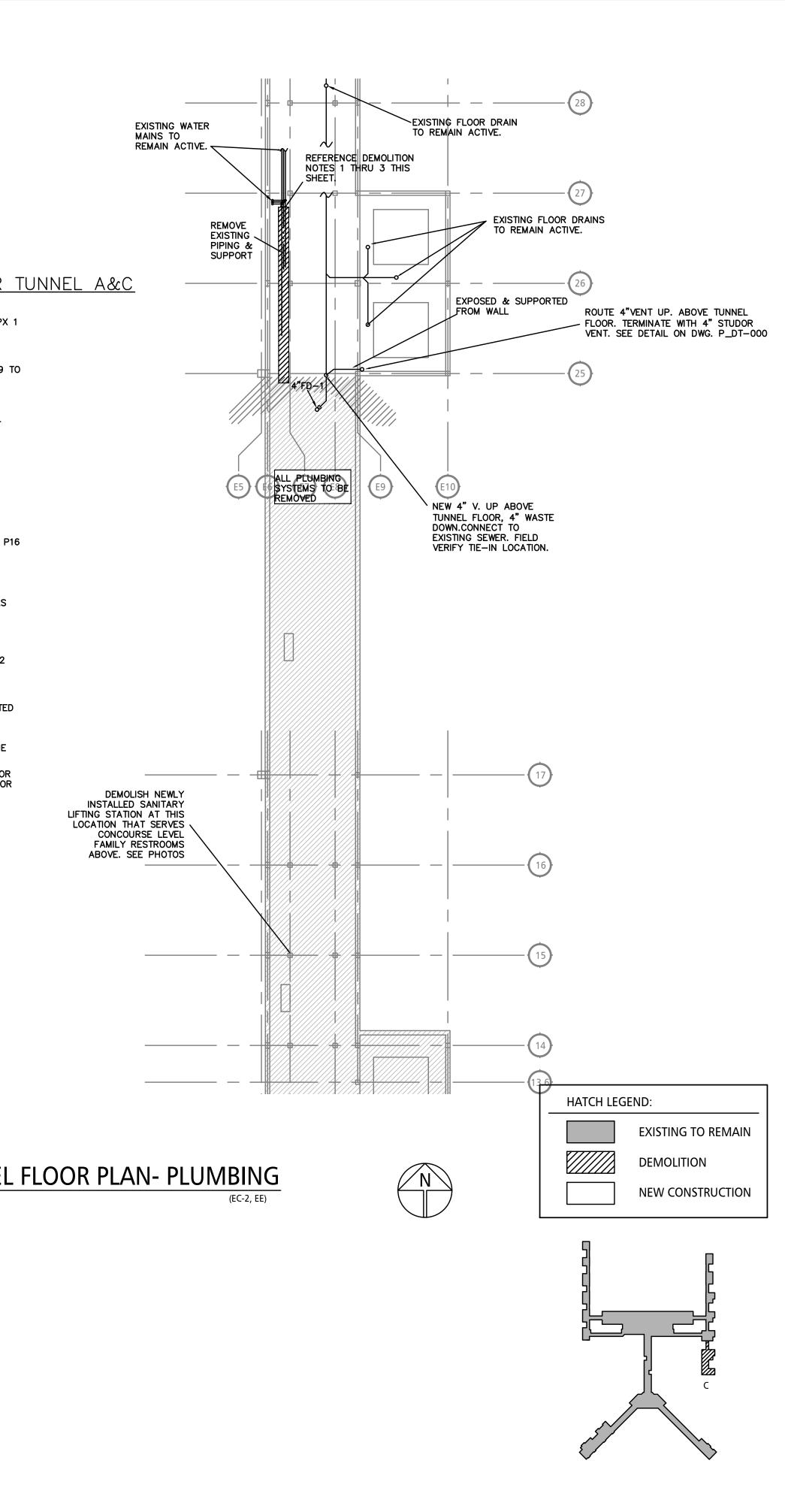
### NOTE:

REFER TO INVENTORY TAG TABLE FOR RELOCATION, RECONFIGURATION, REPLACEMENT, ETC OF HVAC AND PLUMBING UTILITIES. TAGS CORRESPONDING TO THESE NUMBERS ARE LOCATED AT OR NEAR THE CUT LINE ON EACH PIPE, DUCT, ETC. THIS IS REPRESENTATIVE OF CONDITIONS AT THE TIME OF INSPECTIONS. CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO BEGINNING OF WORK, AS CHANGES MAY HAVE BEEN MADE IN THE INTERIM BETWEEN DESIGN INSPECTION AND BEGINNING OF WORK.

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### GENERAL DEMOLITION NOTES FOR TUNNEL A&C

1. NATURAL GAS PIPING TAGGED WITH P8 TO BE REMOVED. (APPX 1 INCH 80PSI SUPPLY.)

2. 1 INCH DOMESTIC HW AND 3/4 INCH DOMESTIC HWR TAGGED P9 TO BE REMOVED BACK TO DOMESTIC HOT WATER HEAT EXCHANGER LOCATED ON NORTH SIDE OF ELECTRIC SUB-STATION 2.

3. DOMESTIC CW PIPING TAGGED P10 DOWNSTREAM OF PARALLEL RPZ TO BE REOMOVED.

4. DOMESTIC CW/HW/HWR TAGGED P12 TO BE REMOVED IN ITS ENTIRETY.

5. DOMESTIC HWR/HW/CW TAGGED P13, P14, AND P15 TO BE REMOVED

6. HIGH PRESSURE 80PSI NATURAL GAS SUPPLY PIPING TAGGED P16 TO BE REMOVED.

7. NATURAL GAS PIPING TAGGED P17 TO BE REMOVED IN ITS ENTIRETY.

8. LOW PRESSURE GAS PIPING TAGGED P18 DOWNSTREAM OF GAS METER TO BE REMOVED IN ITS ENTIRETY.

9. NATURAL GAS REGULATOR AND GAS REGULATOR VENT PIPING TAGGED P20 TO BE REMOVED IN ITS ENTIRETY.

10. ABANDONED INSULATED DOMESTIC WATER PIPING TAGGED P22 AND P23 TO BE REMOVED.

11. REFER TO INVENTORY TAG TABLE FOR RELOCATION, RECONFIGURATION, REPLACEMENT, ETC OF HVAC AND PLUMBING UTILITIES. TAGS CORRESPONDING TO THESE NUMBERS ARE LOCATED AT OR NEAR THE CUT LINE ON EACH PIPE, DUCT, ETC. THIS IS REPRESENTATIVE OF CONDITIONS AT THE TIME OF INSPECTIONS. CONTRACTOR SHALL VERIFY EXISTING CONDITIONS PRIOR TO BEGINNING OF WORK, AS CHANGES MAY HAVE BEEN MADE IN THE INTERIM BETWEEN DESIGN INSPECTION AND BEGINNING OF WORK.

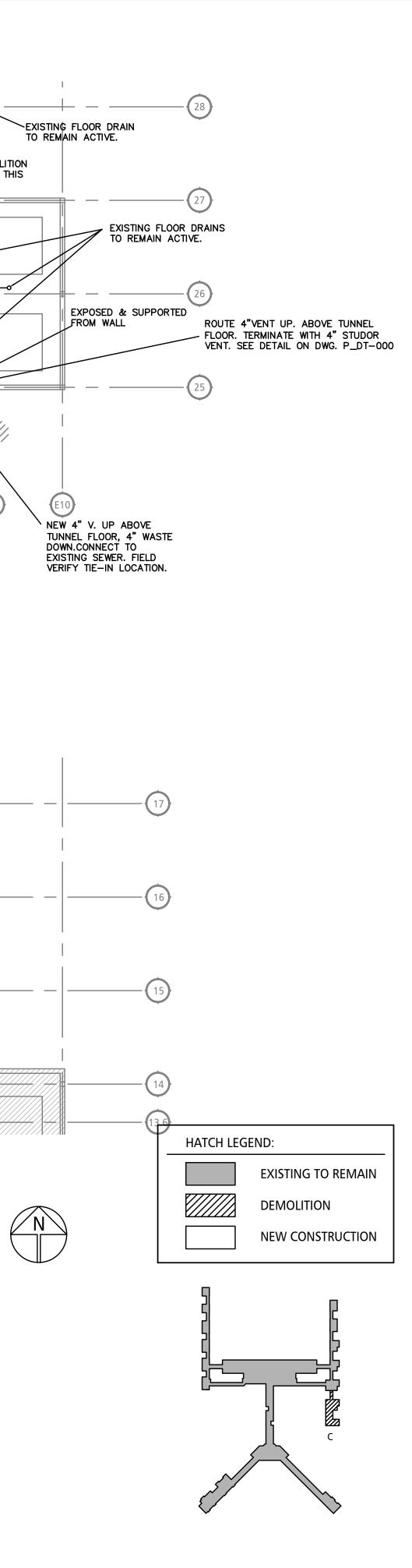
12. AFTER THE NEW FLOOR DRAIN IS INSTALLED, THE CONTRACTOR IS TO CAMERA SCOPE THE FLOOR DRAIN LINE TO THE NEXT FLOOR DRAIN. CONTACT MSCAA FOR COORDINATION AND SCHEDULING.

DRAIN, CLEANOUT AND HYDRANT SCHEDULE					
SYMBOL	FIXTURE	MANUF.	DESCRIPTION		
FD-1	FLOOR DRAIN		2142-Y04-M HEAVY DUTY FLOOR DRAIN WITH 12"TOP AND SEDIMENT BUCKET WITH DUCTILE IRON GRATE.		

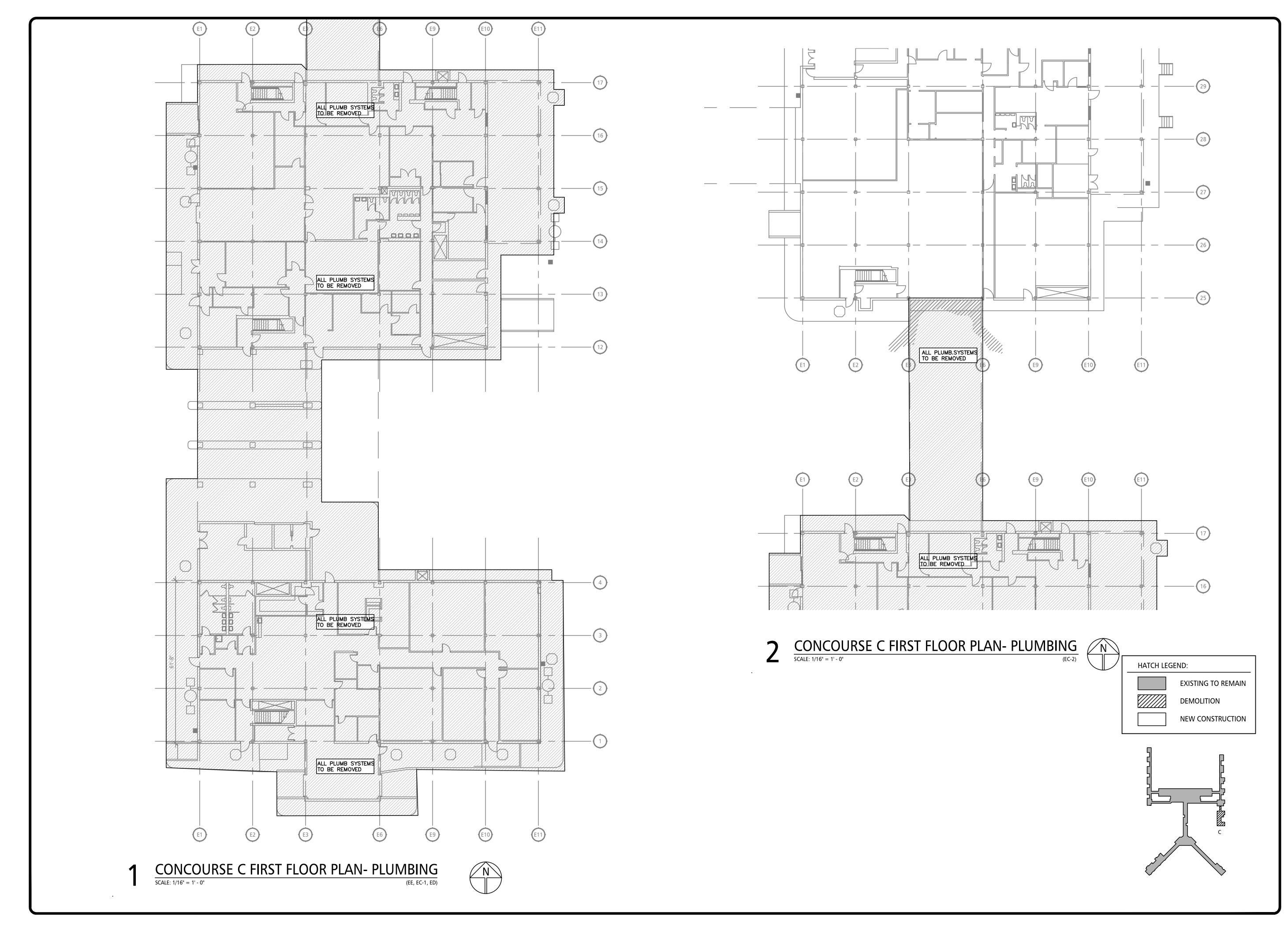


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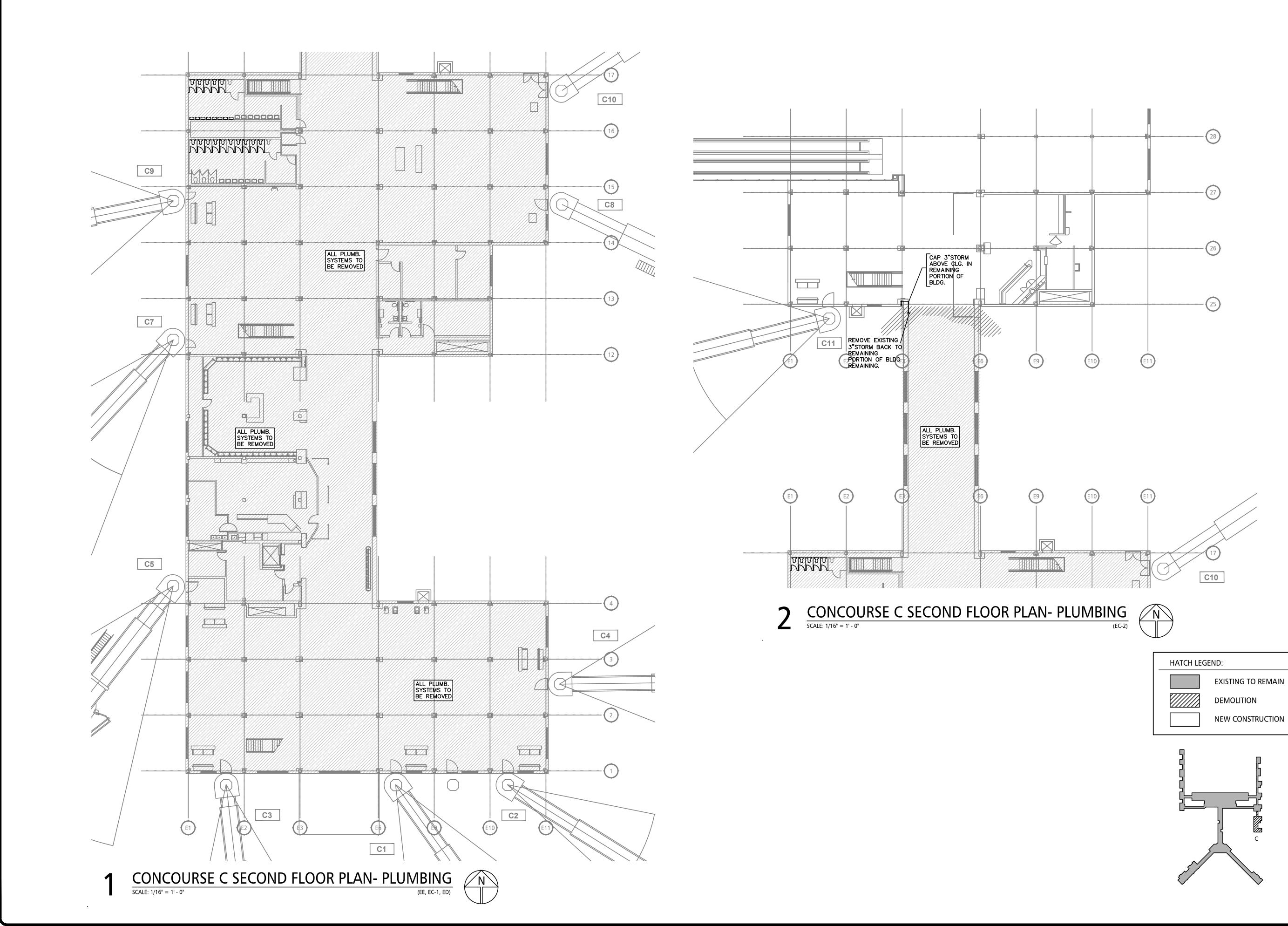
# CONCOURSE C TUNNEL FLOOR PLAN- PLUMBING SCALE: 1/16" = 1' - 0"



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Gala Engineering Inc. 7975 STAGE HILLS BLVD. SUITE 5 MEMPHIS, TN. 38133 TEL. (901) 384–8400 FAX (901) 373–2255 This information is released solely for its use in the interest of the Memphis–Shelby County Airport Authority (MSCAA) or its Tenants. All drawings received from MSCAA or produced for or produced for MSCAA as a work product shall be regarded as privileged information, not to be released to others without the express written permission of MSCAA. Your acceptance of the released materials establishes your understanding of, and commitment to, providing adequate control of the materials issued and produced.							
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