

PHOTO 1 WATER WAS FOUND TRAPPING IN THE ROOFING SYSTEM AT ONE LOCATION OF ROOF AREA B NEAR A ROOF DRAIN. ALL RECENT LAYER OF BUILD UP ROOF IN AREA A AND AREA B MUST BE REMOVED DOWN TO THE ORIGINAL GRAVEL ROOF OVER LIGHT WEIGHT INSULATED CONCRETE. ALLOW THE SYSTEM TO DRY TO ACCEPTABLE LEVEL BY THE NEW ROOFING MANUFACTURER BEFORE INSTALLATION OF THE NEW SYSTEM.



PHOTO 2 ALL REMAINING SKYLIGHTS SHALL BE REMOVED FOR ROOF RECOVERING. PORTION OF EXISTING SKYLIGHT INDICATED BY YELLOW ON DRAWING BELOW HAS ALREADY BEEN REMOVED AND COVERED BY OTHERS.



PHOTO 3 EXAMPLE OF EXISTING METAL DECKING UNDER THE LIGHT WEIGHT CONCRETE. CONTRACTOR SHALL FIELD VERIFY CONDITION OF ALL AREAS WITHIN THE PROJECT SCOPE.



PHOTO 4 THE MAKEUP OF THE RECENT SECOND ROOF CONSISTS OF THE BUILD UP OF LAYERS OF CAPS SHEETS AND BASE SHEETS HOT MOPPED OVER FIBERBOARD THAT WAS MOPPED ONTO THE ORIGINAL PEA GRAVEL ROOF OVER LIGHT WEIGHT CONCRETE OVER ORIGINAL METAL DECKING

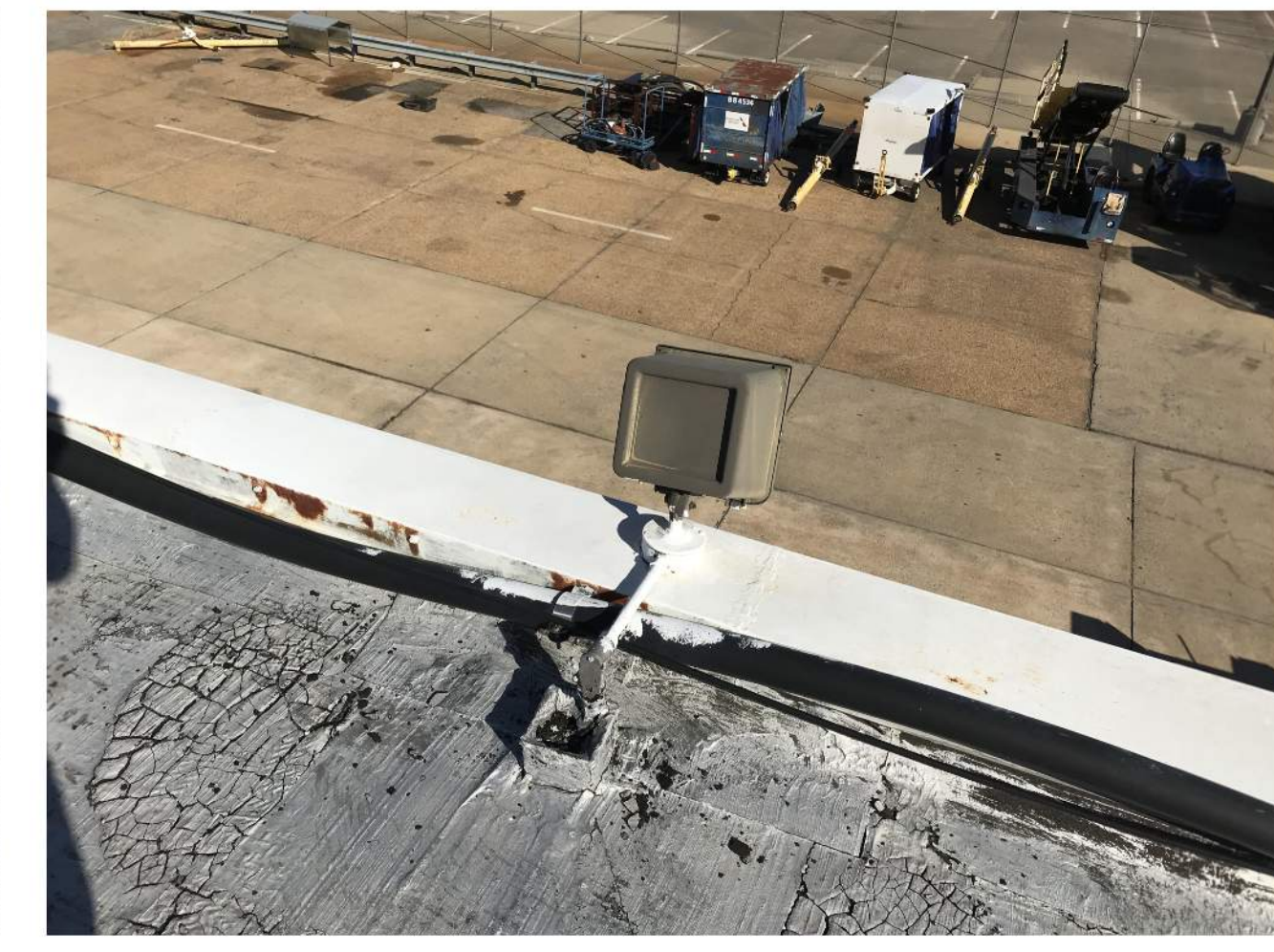


PHOTO 5 EXISTING AREA LIGHT SHALL BE REMOVED FOR REEROOFING AND REINSTALLED AFTER THE ROOFING IS COMPLETE. COORDINATE WITH AIRPORT AUTHORITY FOR ANY ELECTRICAL REPAIR WORK THAT MAY BE PERFORMED AT THE SAME TIME DURING OR BEFORE REEROOF



PHOTO 6 EXISTING DIMENSION OF EDGE TO EDGE OF THE DIMENSION OF THE SKYLIGHT CURB TO BE 51 1/4" BY 51 1/4"...THE INSIDE CLEARANCE IS APPROXIMATELY 48 1/2" X 48 1/2"



PHOTO 7 ABOVE THE JOIST IS 6" OF COMPOSITE CONSISTS OF LIGHT WEIGHT INSULATED CONCRETE ON CORRUGATED METAL DECKING. THE OVERALL HEIGHT OF THE SKYLIGHT ON THE CONCRETE IS ABOUT 9 INCHES. THE WIDTH OF FLANGES IS ABOUT 2 1/4 INCHES

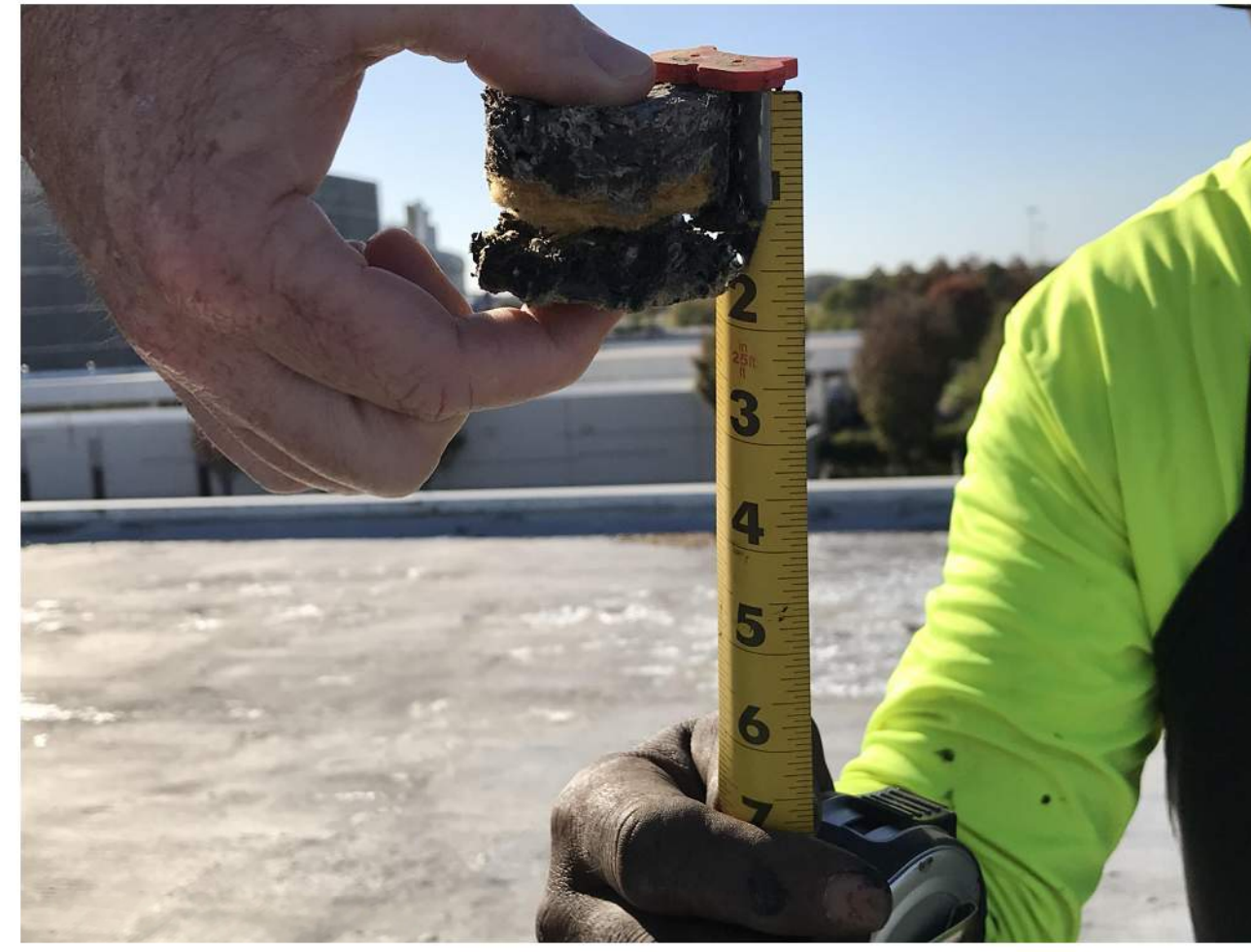


PHOTO 8 THE SECOND ROOF IS ABOUT 2'-3" THICK ON TOP OF 6" OF EXISTING LIGHT WEIGHT INSULATED CONCRETE OVER METAL DECKING. THE RECENT SECOND ROOF CONSISTS OF THE BUILD UP OF LAYERS OF CAPS SHEETS AND BASE SHEETS HOT MOPPED OVER FIBERBOARD THAT WAS MOPPED ONTO THE ORIGINAL PEA GRAVEL ROOF OVER LIGHT WEIGHT CONCRETE

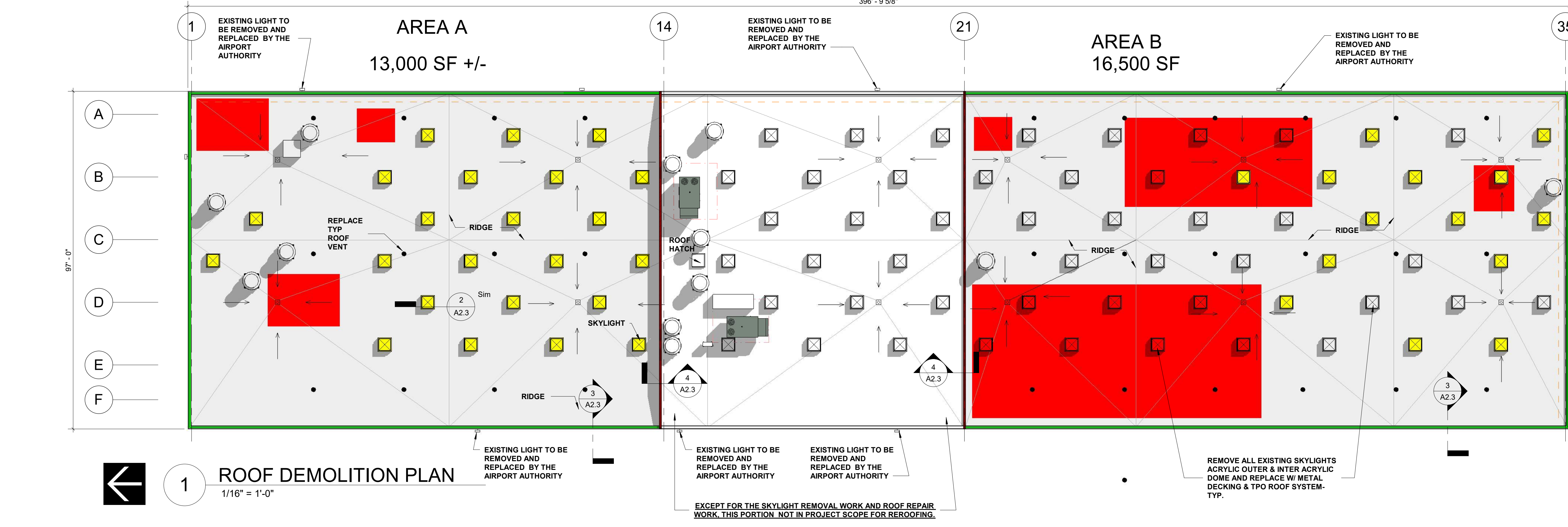


PHOTO 9 TPO ROOFING TO REMAIN AS IS IN THE MIDDLE PORTION OF THE BUILDING THAT WAS RECENTLY INSTALLED. ALL SKYLIGHT AT THESE AREA TO BE REMOVED ALSO AND BE COVERED WITH INSULATION OVER METAL DECKING TO SPAN OVER THE EXISTING CURB. SLOPE TOP TO DRAIN SO THAT WATER PONDING WOULD NOT BE POSSIBLE.

PROJECT NOTES:

- CONTRACTOR SHALL VISIT THE JOBSITE, INSPECT EXISTING CONDITIONS, AND SHALL NOTIFY MSCAA OF ANY DISCREPANCIES THAT MAY AFFECT SCOPE OR PRICE OF WORK. NOT ALL ROOF EQUIPMENT AND DRAIN ARE SHOWN ON THIS SCOPE DRAWING. NOT ALL PENETRATIONS WERE SHOWN ON THE ROOF PLAN. CONTRACTOR TO FIELD VERIFY THE EXACT QUANTITY.
- ALL ROOF AREAS SHOWN ARE APPROXIMATE ONLY. CONTRACTOR SHALL FIELD VERIFY THE ACTUAL EXISTING CONDITIONS FOR BIDDING. CONTRACTOR SHALL OBTAIN ALL NECESSARY APPROVALS AND PERMITS.
- CONTRACTOR SHALL CONDUCT A PRE-CONSTRUCTION MEETING WITH OWNER TO COORDINATE WORK SCHEDULE, ANY INTERRUPTION, MATERIAL STAGING, SECURITY, EMPLOYEE PARKING, ETC. PROTECT OWNER PROPERTY. ANY DAMAGE TO THE OWNER'S PROPERTY DUE TO THE ROOF WORK OR MATERIAL HANDLING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND DAMAGE SHALL BE REPAIRED IMMEDIATELY WHEREVER POSSIBLE SO AS NOT TO AFFECT OWNERS OR TENANTS OPERATION AT THE FACILITY.
- CONTRACTOR SHALL REMOVE ALL DEMOLITION MATERIALS AND CONSTRUCTION DEBRIS FROM THE SITE AND DISPOSE OF IN AN APPROVED MANNER. THE FACILITY WILL REMAIN IN OPERATION AND OCCUPIED DURING REEROOFING. SECURITY AND LIFE SAFETY MEASURE, AND WATER TIGHTNESS SHALL NOT BE COMPROMISED AT ALL TIME.
- SITE IS LOCATED ON AIRPORT PROPERTY. CONTRACTOR SHALL COMPLY WITH ALL WORK RULES AND SECURITY REQUIREMENTS OF AIRPORT AUTHORITY.
- FOR WARRANTY PURPOSES, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL ROOFING DETAILS REQUIRED BY MANUFACTURER FOR PROPER INSTALLATION OF THE ROOFING SYSTEM. SUBMIT MANUFACTURER DETAILS OR SHOP DRAWING PACKAGE TO OWNER FOR RECORD.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE THE DAILY SCHEDULE OF THE ROOF WORK BASED ON WEATHER FORECAST. ANY ROOF AREA OPEN FOR REWORK MUST BE COMPLETED WITHIN THE SAME DAY. THE CONTRACTOR WILL BE FULLY RESPONSIBLE FOR DAMAGE DUE TO INCOMPLETE WORK.
- IT IS THE INTENTION OF THE OWNER TO AWARD A CONTRACT BASED UPON THE LOWEST AND BEST RESPONSIVE BID. BIDDER UNDERSTANDS AND AGREES THAT AFTER A REVIEW OF ALL THE BIDS, THE OWNER WILL SELECT THE BID THAT BEST SUITS THE OWNER'S NEEDS WITHIN THE SOLE DISCRETION OF THE OWNER.
- PROTECT ALL ROOF TOP UNITS AND EQUIPMENT FROM ROOF WORK. DO NOT DISCONNECT OR RAISE ANY ROOF TOP UNITS UNLESS APPROVED AND COORDINATED WITH THE OWNER.
- EXERCISE CARE SO AS NOT TO CUT ANY WIRES OR CONDUIT UNDER THE METAL ROOF DECK. USE MINIMUM LENGTH FASTENER WHEREVER POSSIBLE.
- VERIFY AND COORDINATE WITH AIRPORT AUTHORITY REGARDING THE CONDITION OF WIRE AND CABLE ON THE ROOF. IT IS THE AIRPORT'S RESPONSIBILITY TO CONFIRM IF THE WIRES OR CONDUITS IN QUESTION SHALL BE REMOVED OR REROUTED PROPERLY BY A LICENCED INSTALLER.

REV	DESCRIPTION	DATE



LEGEND

- REMOVE EXISTING BUILT-UP ROOF SYSTEM AND FIBER BOARD DOWN TO ORIGINAL PEA GRAVEL AND LIGHT WEIGHT INSULATED CONCRETE ROOF.
- REMOVE EXISTING COPING AT THE ROOF PERIMETER. REPLACE UNSUITABLE WOOD NAILER UNDER THE COPING
- REMOVE EXISTING COPING OVER THE BUILDING EXPANSION JOINT. INSTALL NEW TPO EXPANSION JOINTS WITH BACKER RODS.
- SHADED AREA INDICATE PROBLEMATIC WATER PONDING AREA SHALL BE CORRECTED TO ALLOW POSITIVE DRAINAGE TOWARD THE ROOF DRAINS
- YELLOW SQUARE INDICATES THE LOCATION WHERE THE EXISTING SKYLIGHTS WERE REMOVED AND TEMPORARY COVERED BY A FLAT PIECE OF PLYWOOD AND TPO.
- EXISTING RELIEF VENT TO BE REMOVED AND REPLACED WITH NEW

SCOPE OF DEMOLITION WORK

- ALL EXISTING BUILDUP ROOF TO BE REMOVED DOWN TO ORIGINAL PEA GRAVEL ROOF OVER THE ORIGINAL LIGHT WEIGHT INSULATED CONCRETE AND METAL DECK
- ATTACHED EXISTING ENVIRONMENT REPORT DATED 09-05-2023 BY TIOGA FURNISHED BY THE AIRPORT AUTHORITY INDICATES PRESENCE OF ASBESTOS CONTAINING MATERIAL IN THE EXISTING ROOFING MATERIAL. CONTRACTOR SHALL BE FULLY RESPONSIBLE TO PROPERLY REMOVE AND DISPOSE OF THE MATERIAL PER GOVERNMENT REGULATION AND OSHA STANDARD. IF THE CONTRACTOR'S ACTIONS CAUSE POTENTIAL EXPOSURE INTO THE INTERIOR WORKSPACES, PLEASE SEE GUIDANCE LETTER FOR ASBESTOS CONTAINING ROOFING MATERIAL WITH REFERENCES.
- REMOVE EXISTING ROOF METAL COPING AND REPLACE WITH NEW.
- REMOVE EXISTING FLASHING FOR REEROOF. REFER TO THE ATTACHED GUIDANCE DOCUMENTS REGARDING THE HANDLING OF THE REMOVED MATERIAL.
- INSPECT EXISTING ROOF DECKING FOR DAMAGE. REPAIR AS NEEDED TO MATCH EXISTING CONDITION.
- REMOVE ALL EXISTING DOUBLE SHELL ACRYLIC SKYLIGHTS. ALL CURBS AND FLANGES REMAIN AS IS.
- INSTALL CRICKETS AND TAPERED INSULATION AT ROOF EQUIPMENT AS NEEDED FOR POSITIVE DRAINAGE. CORRECT LOW AREA TO ELIMINATE WATER PONDING.
- REMOVE FLASHING AND REPLACE WITH NEW ONE IN ACCORDANCE WITH N.R.C.A. STANDARDS.
- CONDITION OR INTEGRITY OF EXISTING ROOF AREA DRAINS UNKNOWN. REMOVE ALL EXISTING ROOF DRAIN IN AREA A AND AREA B AND REPLACE WITH NEW DRAIN OF THE SAME SIZE AND SAME MATERIAL. RECONNECT AND TEST TO ENSURE DRAIN CONNECTION ARE SECURED.
- IF EXCESSIVE MOISTURE EXISTING IN THE ROOF SYSTEM THAT WILL INTERFERE WITH THE SUCCESSFUL INSTALLATION OF THE NEW SYSTEM; REMOVE THE UNACCEPTABLE MOISTURE TO THE FULLEST EXTENT POSSIBLE BEFORE REEROOFING.
- CONTRACTOR TO PROVIDE AN ALLOWANCE AS PART OF THE BASE BID FOR THE FOLLOWING: \$8,000 FOR UNFORESEEN REPAIR

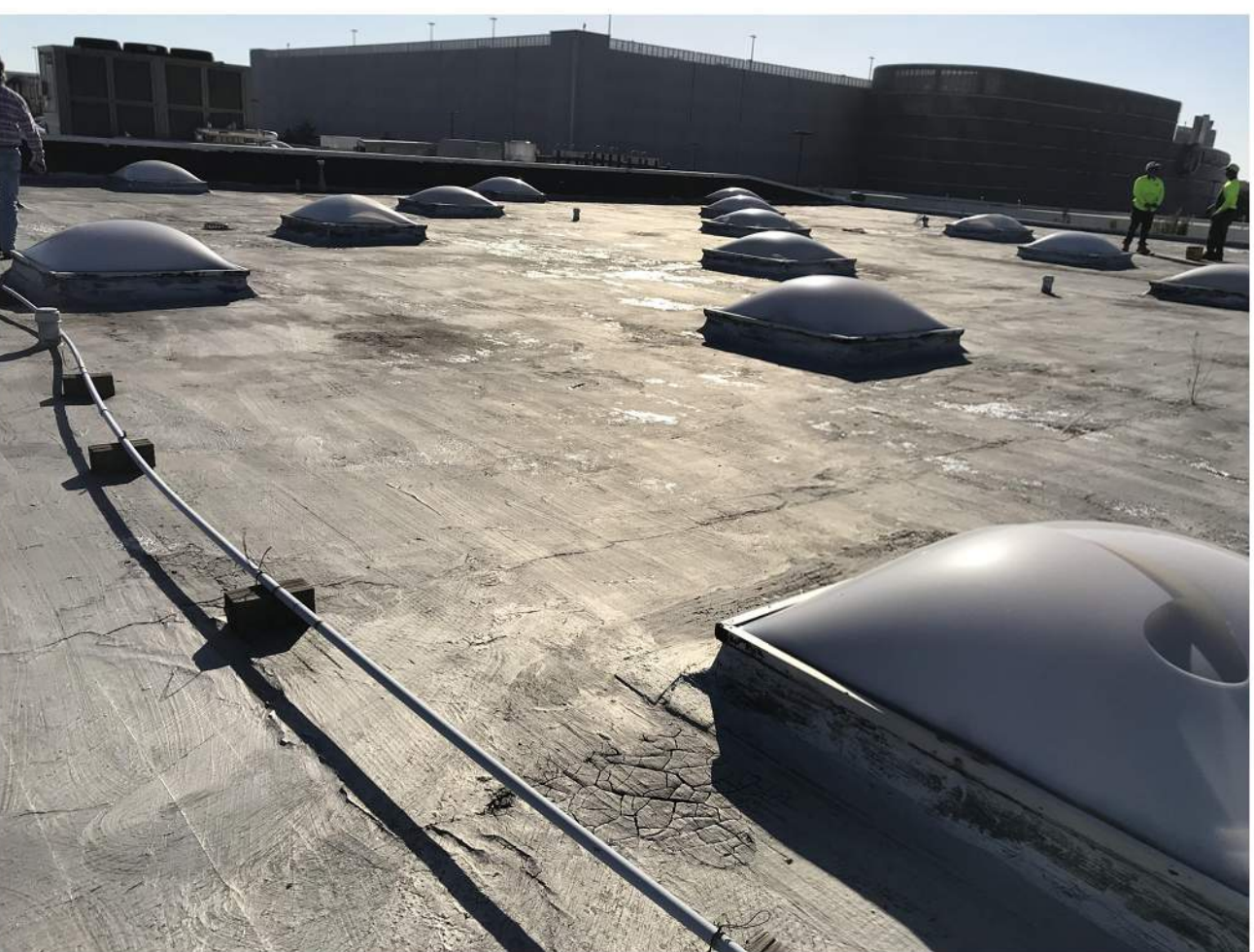


PHOTO 10 ROOF VENT TO REMAIN TO ALLOW THE MOISTURE IN LIGHT WEIGHT CONCRETE SYTEM TO ESCAPE. EXISTING ROOF VENT TO REMAIN FOR RELIEF OF ANY POTENTIAL MOISTURE REMAIN IN THE ROOF SYSTEM. VERIFY WITH AIRPORT AUTHORITY TO CONFIRM IF THE CONDUITS OR PIPES IS STILL IN SERVICE AND BE REMOVED OR RELOCATED BY THE AIRPORT.



PHOTO 11 COORDINATE WITH AIRPORT AUTHORITY ON PROPER REINSTALLATION OR REMOVAL OF THE CABLING LEFT ON THE ROOF TOP.



PHOTO 12 SAFETY HARNESS ANCHOR POINTS SHALL REMAIN AS IS. PROVIDE SEALANT POCKET TO ENSURE THE AREA IS WATER TIGHT



PHOTO 13 AREA OF PONDING SHALL BE CORRECTED TO ALLOW PROPER DRAINAGE FLOW. CLEAN THE DRAIN LINE AS NEEDED TO ENSURE PROPER FLOW. ALL EXISTING AREA DRAIN SHALL BE REPLACED WITH NEW DRAIN OF SAME SIZE AND RECONNECT SECURELY FOR PROPER FLOW.

Sheet Title
DEMOLITION ROOF PLAN

Project No
20006-137

Drawn by
RGI

Date
10-15-2024