

FOTH PROJECT #: 0024M300.10

**EAST PERIMETER VEHICLE SERVICE ROAD REHABILITATION**  
MEMPHIS INTERNATIONAL AIRPORT



# EAST PERIMETER VEHICLE SERVICE ROAD REHABILITATION

## MEMPHIS INTERNATIONAL AIRPORT

MEMPHIS-SHELBY COUNTY AIRPORT AUTHORITY  
MEMPHIS, TN  
MSCAA PROJECT #: #23-1476-10

**IFB PLANS**  
**APRIL 8, 2025**

UTILITY AND EMERGENCY TELEPHONE NUMBERS	
MEMPHIS-SHELBY COUNTY AIRPORT AUTHORITY (MSCAA) - MEMPHIS, TENNESSEE	
EMERGENCIES	(901) 922-8333
AIRPORT COMMUNICATION CENTER	(901) 922-8298
MSCAA OPERATIONS	(901) 922-8117
FAA AIR TRAFFIC CONTROL TOWER	(901) 842-8400
TENNESSE CALL BEFORE YOU DIG (BUD)	811

OWNER CONTACT INFORMATION
BRIAN TENKHOFF, PE DIRECTOR OF DEVELOPMENT MEMPHIS-SHELBY COUNTY AIRPORT AUTHORITY (MSCAA) 2491 WINCHESTER ROAD, SUITE 113 MEMPHIS, TN 38116-3856 (901) 922-2297

ENGINEER CONTACT INFORMATION
JOSH MANN, PE - LEAD CIVIL ENGINEER FOTH INFRASTRUCTURE & ENVIRONMENT, LLC 5100 POPLAR AVENUE CLARK TOWER, SUITE 2709A MEMPHIS, TN 38137 (515) 322-0069

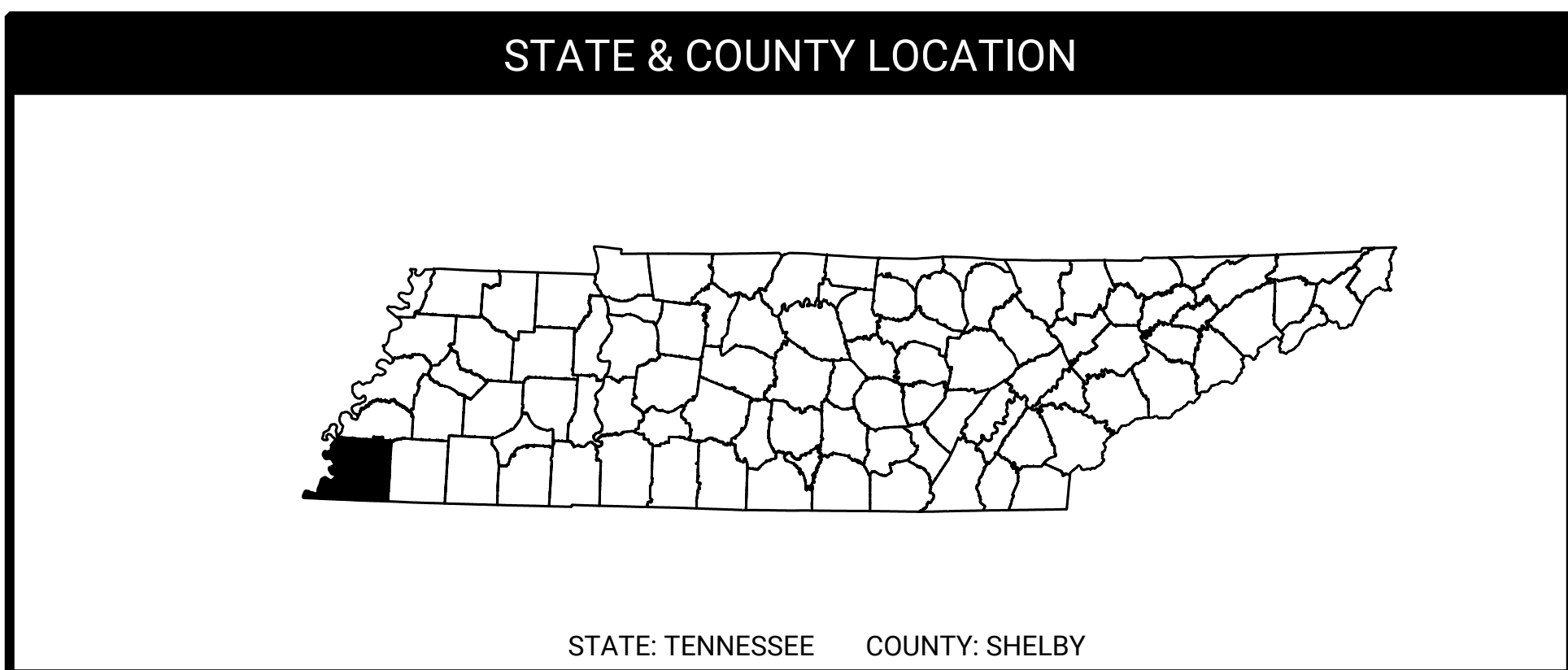
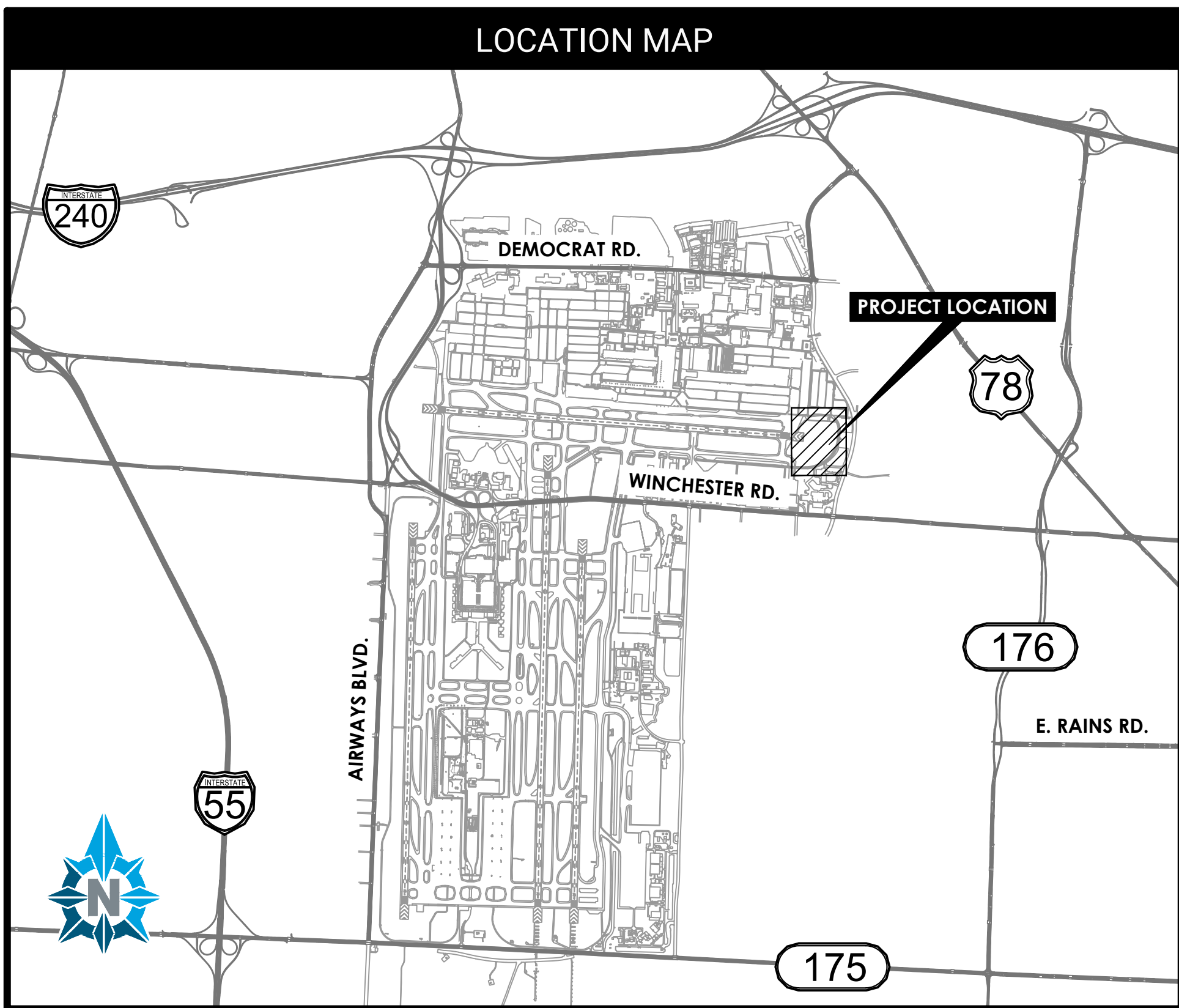
PROPERTY ADDRESS
MEMPHIS INTERNATIONAL AIRPORT 2491 WINCHESTER ROAD MEMPHIS, TENNESSEE 38116

**UTILITY NOTES**

WHERE PUBLIC UTILITY FIXTURES ARE SHOWN AS EXISTING ON THE PLANS OR ENCOUNTERED WITHIN THE CONSTRUCTION AREA, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE OWNERS OF THOSE UTILITIES PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. THE CONTRACTOR SHALL AFFORD ACCESS TO THESE FACILITIES FOR NECESSARY MODIFICATION OF SERVICES. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THERE MAY BE OTHERS, THE EXISTENCE OF WHICH PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATION AND TO AVOID DAMAGE THERETO. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK.

THE CONTRACTOR IS REQUIRED TO UTILIZE THE UTILITY ONE-CALL SERVICE AT (800) 351-1111 AT LEAST 48 HOURS PRIOR TO EXCAVATING ANYWHERE ON THE PROJECT. THE CONTRACTOR MUST ALSO CONTACT THE AIRPORT AND THE FAA AT LEAST 48 HOURS IN ADVANCE FOR LOCATES.

UTILITY CONFLICTS DISCOVERED DURING CONSTRUCTION WILL BE ADDRESSED AT THE TIME OF DISCOVERY.



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G12.00	PROJECT OVERVIEW & SURVEY CONTROL PLAN
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GC2.00	CONSTRUCTION SAFETY & PHASING - OVERVIEW
GC2.01	CONSTRUCTION SAFETY & PHASING - PHASE 1
GC2.02	CONSTRUCTION SAFETY & PHASING - PHASE 2
GC2.03	CONSTRUCTION SAFETY & PHASING - PHASE 3
GC2.04	CONSTRUCTION SAFETY & PHASING - PHASE 4
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	I hereby certify that this engineering document was prepared by me or under my direct personal supervision and that I am a duly licensed Professional Engineer under the laws of the State of Tennessee.
	JOSH MANN, PE License Number: 123332
	Pages or sheets covered by this seal: All sheets

SHEET #: G11.00 MEMPHIS INTERNATIONAL AIRPORT MEMPHIS-SHELBY COUNTY AIRPORT AUTHORITY FOTH INFRASTRUCTURE & ENVIRONMENT, LLC

SUMMARY OF QUANTITIES - BASE BID

ITEM NO.	SPEC. NO.	ITEM	UNIT	QUANTITY	AS-BUILT QUANTITY
1	A-102-1.1	CONSTRUCTION ACCESS, PROJECT SECURITY, AND STAGING AREA	LS	1	
2	A-102-1.2	MAINTENANCE OF TRAFFIC	LS	1	
3	C-102-5.1	CATCH BASIN PROTECTION (TYPE D)	EA	1	
4	C-105-6.1	MOBILIZATION	LS	1	
5	P-605-5.1	JOINT AND CRACK REPAIR	LF	5,400	
6	P-605-5.2	SEVERE JOINT AND CRACK REPAIR	LF	280	
7	P-620-5.1a	TEMPORARY PAVEMENT MARKING - 6" DOUBLE SOLID YELLOW LINE	LF	1,550	
8	P-620-5.1b	TEMPORARY PAVEMENT MARKING - 6" SINGLE SOLID WHITE LINE	LF	3,098	
9	P-620-5.1c	TEMPORARY PAVEMENT MARKING - 6" SINGLE DASHED WHITE LINE	LF	3,093	
10	P-620-5.2a	PAINTED PAVEMENT MARKING - 6" DOUBLE SOLID YELLOW LINE	LF	1,550	
11	P-620-5.2b	PAINTED PAVEMENT MARKING - 6" SINGLE SOLID WHITE LINE	LF	3,098	
12	P-620-5.2c	PAINTED PAVEMENT MARKING - 6" SINGLE DASHED WHITE LINE	LF	3,093	
13	P-620-5.3a	PAINTED PAVEMENT MARKING - VEHICLE STOP BAR (WHITE)	EA	2	
14	P-620-5.3b	PAINTED PAVEMENT MARKING - SURFACE PAINTED "STOP" SIGN (RED BACKGROUND WITH WHITE OUTLINE AND LETTERS)	EA	1	
15	TDOT-203.01	ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	CY	50	
16	TDOT-203.02	UNDERCUTTING	CY	350	
17	TDOT-304.01	SELECT MATERIAL (SOIL CEMENT BASE 8" THICK FOR FULL DEPTH REPAIR)	SY	1,045	
18	TDOT-309.01	CEMENT TREATED BASE (8" THICK)	SY	1,045	
19	TDOT-402.01	PRIME COAT	GAL	345	
20	TDOT-403.01	TACK COAT	GAL	125	
21	TDOT-407.01	HOT MIX ASPHALT (HMA) PAVEMENT - BASE COURSE (4.5" THICK) (TDOT 307 Grading B-M PG 70-22)	TN	280	
22	TDOT-411.01	HOT MIX ASPHALT (HMA) PAVEMENT - SURFACE COURSE (2" THICK) (TDOT 411 Grading D PG 70-22)	TN	1,080	
23	TDOT-415.01	2" COLD MILLING	SY	8,990	
24	TDOT-716.01	RAISED PAVEMENT MARKER (TWO SIDED REFLECTOR)	EA	185	

SUMMARY OF QUANTITIES - ADDITIVE ALTERNATE 1

ITEM NO.	SPEC. NO.	ITEM	UNIT	QUANTITY	AS-BUILT QUANTITY
1	A-102-1.1	CONSTRUCTION ACCESS, PROJECT SECURITY, AND STAGING AREA	LS	1	
2	A-102-1.2	MAINTENANCE OF TRAFFIC	LS	1	
3	C-105-6.1	MOBILIZATION	LS	1	
4	P-605-5.1	JOINT AND CRACK REPAIR	LF	2,225	
5	P-605-5.2	SEVERE JOINT AND CRACK REPAIR	LF	120	
6	P-620-5.1a	TEMPORARY PAVEMENT MARKING - 6" DOUBLE SOLID YELLOW LINE	LF	2,830	
7	P-620-5.1b	TEMPORARY PAVEMENT MARKING - 6" SINGLE SOLID WHITE LINE	LF	1,307	
8	P-620-5.1c	TEMPORARY PAVEMENT MARKING - 6" SINGLE DASHED WHITE LINE	LF	1,067	
9	P-620-5.2a	PAINTED PAVEMENT MARKING - 6" DOUBLE SOLID YELLOW LINE	LF	2,830	
10	P-620-5.2b	PAINTED PAVEMENT MARKING - 6" SINGLE SOLID WHITE LINE	LF	1,307	
11	P-620-5.2c	PAINTED PAVEMENT MARKING - 6" SINGLE DASHED WHITE LINE	LF	1,067	
12	P-620-5.3a	PAINTED PAVEMENT MARKING - VEHICLE STOP BAR (WHITE)	EA	1	
13	P-620-5.3b	PAINTED PAVEMENT MARKING - SURFACE PAINTED "STOP" SIGN (RED BACKGROUND WITH WHITE OUTLINE AND LETTERS)	EA	1	
14	P-620-5.4	PAINTED PAVEMENT MARKING - FAA MOVEMENT/NON-MOVEMENT MARKING	SF	154	
15	TDOT-203.01	ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	CY	50	
16	TDOT-203.02	UNDERCUTTING	CY	70	
17	TDOT-304.01	SELECT MATERIAL (SOIL CEMENT BASE 8" THICK FOR FULL DEPTH REPAIR)	SY	200	
18	TDOT-309.01	CEMENT TREATED BASE (8" THICK)	SY	200	
19	TDOT-402.01	PRIME COAT	GAL	67	
20	TDOT-403.01	TACK COAT	GAL	25	
21	TDOT-407.01	HOT MIX ASPHALT (HMA) PAVEMENT - BASE COURSE (4.5" THICK) (TDOT 307 Grading B-M PG 70-22)	TN	55	
22	TDOT-411.01	HOT MIX ASPHALT (HMA) PAVEMENT - SURFACE COURSE (2" THICK) (TDOT 411 Grading D PG 70-22)	TN	545	
23	TDOT-415.01	2" COLD MILLING	SY	4,580	
24	TDOT-716.01	RAISED PAVEMENT MARKER (TWO SIDED REFLECTOR)	EA	75	

CAD FILE: C:\PW\WORKDIR\PW\LEF\VO\W\H\DR62740\G1.01-QUANTITIES.DWG Layout: G1.01 By: HALEY, WILLU

PREPARED FOR:



PREPARED BY:

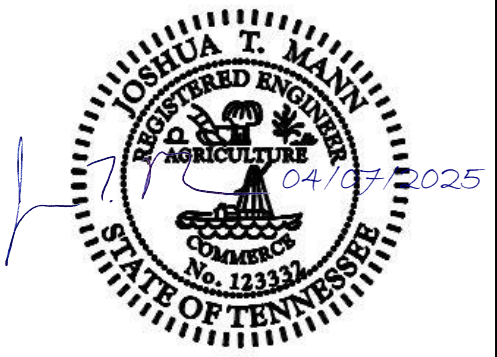


KEY MAP:

NO	DATE	BY	REVISION	
			DESCRIPTION	

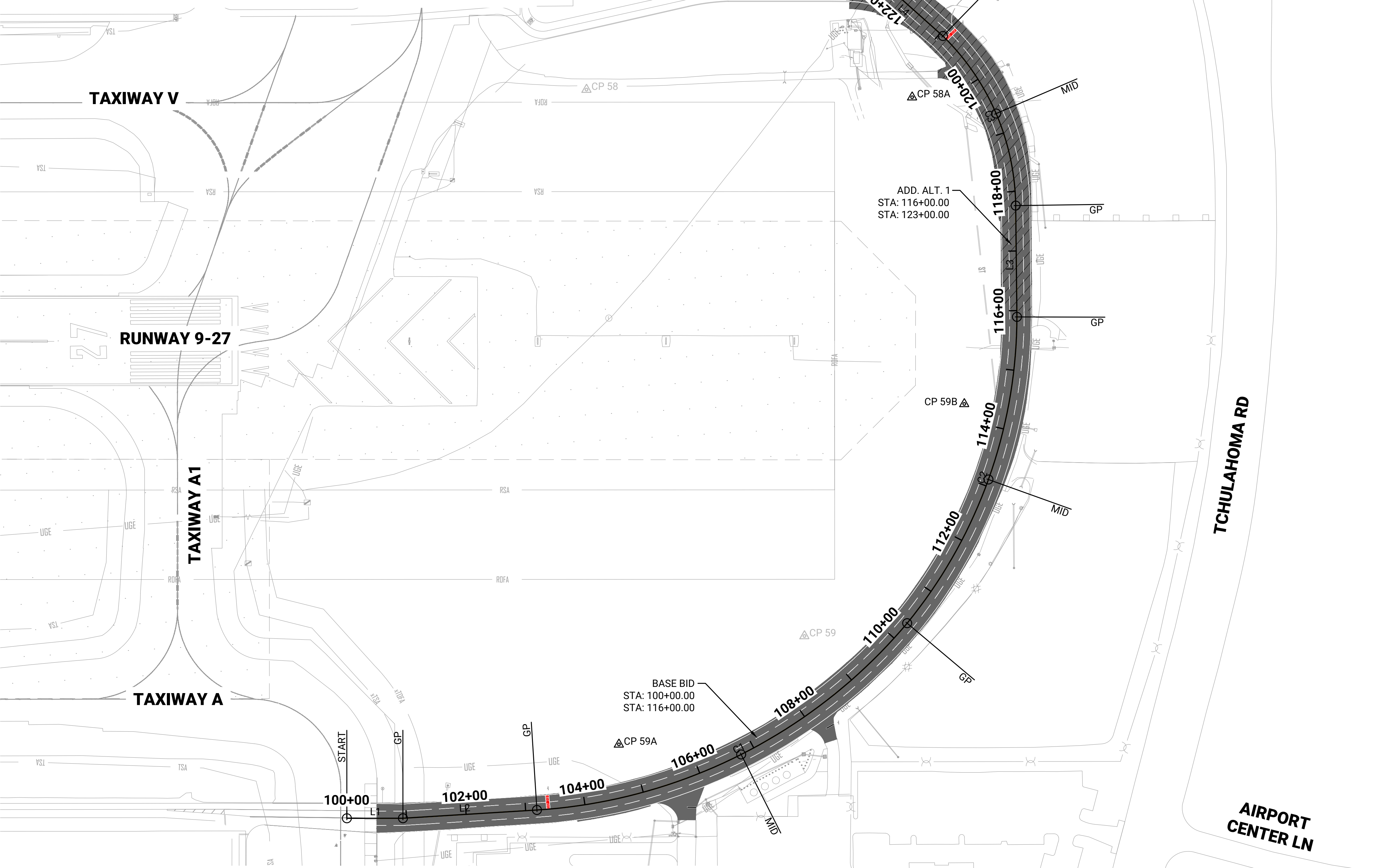
REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

SUMMARY OF QUANTITIES

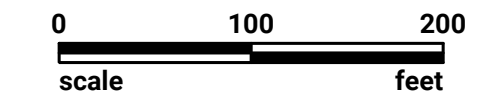


DESIGNED: WUH	DRAWN: WUH	CHECKED: JTM
PROJECT NO: 0024M300.10		
PRINT DATE: 4/8/2025		
SHEET NO.		

CONTROL POINT DATA				
POINT #	NORTHING	EASTING	ELEVATION	RAW DESCRIPTION
58	288011.7000	785439.0660	292.14	MONUMENT ALUMINUM DISK
58A	287957.9180	785982.3570	285.22	1/2" REBAR W/ CAP
59	287070.8050	785734.4760	303.11	MONUMENT ALUMINUM DISK
59A	286913.0180	785410.7710	297.09	1/2" REBAR W/ CAP
59B	287438.6540	786030.8790	295.41	1/2" REBAR W/ CAP



VSR CENTERLINE DATA										
SEGMENT	LENGTH	DIRECTION	START STATION	START NORTHING	START EASTING	END STATION	END NORTHING	END EASTING	PI STATION	RADIUS
L1	93.978	S85° 43' 53.86"E	100+00.00	N: 286823.7306	E: 784946.7496	100+93.98	N: 286823.7306	E: 785040.4670		
L2	225.020	S89° 12' 25.45"E	100+93.98	N: 286816.7360	E: 785040.4670	103+19.00	N: 286816.7360	E: 785265.4650		
C1	714.305	N67° 31' 45.79"E	103+19.00	N: 286813.6220	E: 785265.4650	110+33.30	N: 286813.6220	E: 785907.7655	106+96.89	885.000
C2	556.174	N24° 01' 27.56"E	110+33.30	N: 287079.2857	E: 785907.7655	115+89.48	N: 287079.2857	E: 786129.8294	113+22.71	815.000
L3	186.652	N03° 40' 29.79"E	115+89.48	N: 287577.4801	E: 786129.8294	117+76.13	N: 287577.4801	E: 786141.7930		
C3	317.444	N18° 55' 25.21"W	117+76.13	N: 287763.7480	E: 786141.7930	120+93.57	N: 287763.7480	E: 786041.4580	119+43.51	405.028
L4	206.427	N43° 58' 21.63"W	120+93.57	N: 288056.4080	E: 786041.4580	123+00.00	N: 288056.4080	E: 785898.1326		



LEGEND		
EXISTING	PROPOSED	
[Symbol]	[Symbol]	PAVEMENT - ASPHALT (BASE BID)
[Symbol]	[Symbol]	PAVEMENT - ASPHALT (ADD. ALT. 1)
[Symbol]	N/A	RIPRAP
[Symbol]	N/A	RUNWAY SAFETY AREA
[Symbol]	N/A	RUNWAY OBJECT FREE AREA
[Symbol]	N/A	TAXIWAY SAFETY AREA
[Symbol]	N/A	TAXIWAY OBJECT FREE AREA
[Symbol]	N/A	LOC/ILS CRITICAL AREA
[Symbol]	N/A	SANITARY SEWER
[Symbol]	N/A	STORM SEWER
[Symbol]	N/A	WATER
[Symbol]	N/A	ELECTRICAL - AIRFIELD CIRCUIT
[Symbol]	N/A	ELECTRICAL - UNDERGROUND CABLE
[Symbol]	[Symbol]	PAVEMENT MARKINGS
[Symbol]	[Symbol]	CONTROL POINT
N/A	[Symbol]	ALIGNMENT

NOTES:

1. CONTRACTOR SHALL PROTECT SURVEY MONUMENTS, REFERENCE POINTS, AND BENCHMARKS DURING CONSTRUCTION.
2. PROJECT DATUM NAD83, TENNESSEE US SURVEY FOOT.
3. CONTRACTOR SHALL VERIFY CONTROL POINT LOCATIONS AND ELEVATIONS PRIOR TO CONSTRUCTION.

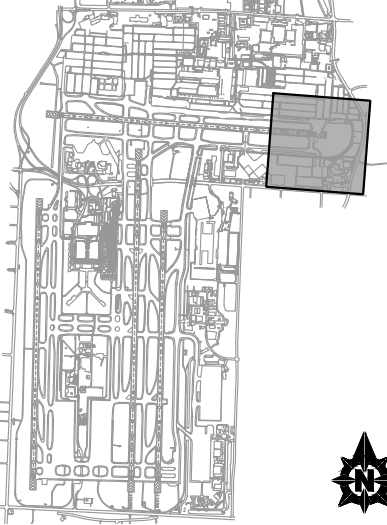
PREPARED FOR:



PREPARED BY:



KEY MAP:



NO.	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

PROJECT OVERVIEW &  
SURVEY CONTROL PLAN



DESIGNED: WUH DRAWN: WUH CHECKED: JTM

PROJECT NO: 0024M300.10  
PRINT DATE: 4/7/2025

SHEET NO.

**GI2.00**

TAXIWAY SAFETY AREA INFORMATION				
TAXIWAY	ADG	WIDTH	TSA	TOFA
A	V	75FT	214FT	285FT

CAD FILE: C:\P\WORK\RP\WUH\0024M300\PROJECT OVERVIEW.DWG LAYOUT: GI2.DWG BY: HALEY, WILLU

**GENERAL SAFETY AND PHASING NOTES:**

THE CONTRACTOR SHALL COMPLY WITH THE FOLLOWING PROVISIONS. NO DIRECT PAYMENT FOR THIS WORK WILL BE MADE EXCEPT FOR WHERE SPECIFICALLY STATED IN THE PLANS AND/OR SPECIFICATIONS. ALL COSTS FOR MATERIAL, LABOR, EQUIPMENT, ETC., TO COMPLY WITH THESE PROVISIONS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

**GENERAL**

1. UTILITY AND EMERGENCY TELEPHONE NUMBERS
  - EMERGENCY: (901) 922-8333
  - TENNESSEE ONE-CALL: (800) 351-1111
  - MSCAA COMMUNICATION/NON-EMERGENCY: (901) 922-8298
  - MSCAA AIRPORT OPERATIONS CENTER (901) 922-8117
2. IN THE EVENT OF A MEDICAL OR POLICE EMERGENCY, THE CONTRACTOR SHALL CALL AIRPORT POLICE FIRST, (901) 922-8333, AND THE AIRPORT OPERATIONS CENTER IMMEDIATELY THEREAFTER. THE OPERATIONS CENTER WILL COORDINATE ANY EMERGENCY RESPONSE.
3. IN THE EVENT OF A DECLARED AIRCRAFT EMERGENCY, AIRPORT OPERATIONS PERSONNEL MAY REQUEST THE CONTRACTOR TO TEMPORARILY SUSPEND OPERATIONS AND VACATE THE PREMISE UNTIL THE EMERGENCY EVENT IS SATISFACTORILY ADDRESSED.
4. IN THE EVENT OF A DECLARED SEVERE WEATHER EVENT, INCLUDING AN EVENT REQUIRING LOW VISIBILITY OPERATIONS/SURFACE MOVEMENT GUIDANCE AND CONTROL SYSTEM (LVO/SMGCS) PROCEDURES, AIRPORT OPERATIONS PERSONNEL MAY REQUEST THE CONTRACTOR TEMPORARILY SUSPEND OPERATIONS AND VACATE THE PREMISE UNTIL THE EVENT IS OVER.
5. NO SMOKING IS ALLOWED WITHIN THE RESTRICTED AREAS OF THE AIRPORT. NO OPEN FLAMES ALLOWED EXCEPT BY SPECIAL PERMISSION FROM AIRPORT OPERATIONS/PROGRAM MANAGER PERSONNEL. WORK ASSOCIATED WITH TAR KETTLES TO MELT JOINT SEALANT SHALL REMAIN A MINIMUM OF 50 FEET FROM AIRCRAFT AND FUEL VEHICLES.
6. THE AIR OPERATIONS AREA (AOA), CONSISTS OF ALL AREAS OF THE AIRPORT LOCATED WITHIN THE PERIMETER SECURITY FENCE. THE CONTRACTOR SHALL NOTIFY THE MSCAA PROJECT ENGINEER AT LEAST 48 HOURS PRIOR TO ANY WORK ON THIS PROJECT.
7. THE CONTRACTOR SHALL COORDINATE ALL OPERATIONS WITH THE MSCAA PROJECT ENGINEER AND SHALL SUBMIT A PROPOSED SCHEDULE AT LEAST 5 DAYS PRIOR TO THE PRE-CONSTRUCTION MEETING. REFER TO THE MSCAA FRONT ENDS FOR ADDITIONAL INFORMATION.
8. ALL WASTE AND REMOVED ITEMS SHALL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE AIRPORT AND DISPOSED OF LEGALLY AND PROPERLY OFFSITE, UNLESS OTHERWISE NOTED IN THE PLANS.
9. ALL UNDERGROUND STRUCTURES (MANHOLES, CATCH BASINS, ETC) ARE CONSIDERED CONFINED SPACE ENTRY. THE CONTRACTOR SHALL TAKE ALL REQUIRED PRECAUTIONS AND COMPLETE THE REQUIRED PERMITS FOR CONFINED SPACE ENTRY.
10. IN THE EVENT OF NIGHT TIME OPERATIONS, THE CONTRACTOR MUST SHIELD ALL FLOOD LIGHTS, KEEP LIGHTS POINTED AWAY FROM THE ATCT, AND INCOMING AND DEPARTING AIRCRAFT.
11. THE AIRPORT MAY TEMPORARILY SUSPEND CONTRACTOR OPERATIONS IN CERTAIN AREAS TO ADDRESS EMERGENCIES.
12. THE PROJECT PHASING PLAN DOES NOT RELIEVE THE CONTRACTOR FROM FOLLOWING OSHA/TOSHA SAFETY REQUIREMENTS.
13. CONTRACTOR IS RESPONSIBLE FOR THE ACTIONS OF THEIR EMPLOYEES AS WELL AS THE ACTIONS OF THEIR SUB-CONTRACTOR'S EMPLOYEES. ANY REQUIREMENT REFERENCE HEREIN TO THE CONTRACTOR'S EMPLOYEES SHALL ALSO MEAN THE REQUIREMENT EXTENDS TO THE SUB-CONTRACTOR'S EMPLOYEES.
14. CONTRACTOR SHALL COORDINATE ANY CRANE USAGE AND ALL EQUIPMENT OVER 20' WITH MSCAA 45 DAYS IN ADVANCE. IF CONTRACTOR ELECTS TO EXCEED THIS HEIGHT, THE CONTRACTOR IS REQUIRED TO SUBMIT FAA FORM 7460 REGARDING CONSTRUCTION EQUIPMENT HEIGHT AND OBTAIN APPROVAL PRIOR TO STARTING CONSTRUCTION WORK ASSOCIATED WITH THIS EQUIPMENT. SUBMITTAL REVIEW A RECEIVING A RESPONSE FROM THE FAA TYPICALLY TAKES 60-90 DAYS AND APPROVAL OF THE REQUEST IS NOT ALWAYS GUARANTEED.

**COORDINATION**

1. WEEKLY CONSTRUCTION PROGRESS MEETINGS WILL BE HELD THROUGHOUT THE DURATION OF THE PROJECT. AT A MINIMUM, REQUIRED ATTENDEES WILL INCLUDE PROJECT SUPERINTENDENT AND FOREMAN OF PRIME CONTRACTOR, AS WELL AS THE PROJECT FOREMAN FOR EACH SUBCONTRACTOR WITH WORK OCCURRING DURING THE CURRENT PERIOD. CONSTRUCTION PHASING AND SAFETY WILL BE A STANDING AGENDA ITEM AT THE WEEKLY CONSTRUCTION PROGRESS MEETINGS.
2. THE CONTRACTOR SHALL PROVIDE, PRIOR TO START OF CONSTRUCTION, THE NAME(S) AND 24 HOUR CONTACT PHONE NUMBERS OF THE FIELD FOREMAN WHO WILL BE IN CHARGE.
3. AT ALL TIMES WHEN CONSTRUCTION ACTIVITIES ARE BEING PERFORMED ON THIS PROJECT THE PRIME CONTRACTOR MUST HAVE A FOREMAN ON-SITE OR IMMEDIATELY AVAILABLE WHO IS AUTHORIZED TO MAKE DECISIONS REGARDING THE OPERATIONS AND SAFETY OF ALL PERSONNEL EMPLOYED BY THE CONTRACTOR AND SUBCONTRACTORS. EACH DAY, THE DESIGNATED FOREMAN MUST COORDINATE THEIR ACTIVITIES WITH THE AIRPORT OPERATIONS MANAGER FOR THE DAYS WORK.
4. CONTRACTOR SHALL COORDINATE WITH THE MSCAA PROJECT ENGINEER AND MSCAA OPERATIONS MANAGER FOR ALL PAVEMENT CLOSURES AND CONSTRUCTION EFFORTS IMPACTING AIRPORT, AIRCRAFT, AND MAINTENANCE OPERATIONS. ADVANCE NOTICE REQUIREMENTS ARE DETAILED ON THE FOLLOWING PAGES AND IN THE CONTRACT DOCUMENTS.
5. CONTRACTOR SHALL COORDINATE WITH MSCAA AND PROGRAM MANAGER A MINIMUM OF 24 HOURS PRIOR TO THE ISSUANCE OF ALL NOTAMS RELATED TO THE PROJECT CONSTRUCTION. AIRPORT AND FAA SHALL GENERATE AND ISSUE NOTAMS BASED ON CONTRACTOR CONSTRUCTION SCHEDULE AND FACILITY IMPACTS.
6. CONTRACTOR MUST PERFORM THIS WORK IN ACCORDANCE WITH THE PROVISIONS INCLUDED WITHIN THE CONTRACT DOCUMENTS.

**NAVIGATIONAL AID (NAVAID) FACILITIES**

1. AIRCRAFT NAVIGATIONAL AID (NAVAIDS) PROVIDE VISUAL AND ELECTRONIC INFORMATION WHICH IS USED BY PILOTS WHO OPERATE AND LAND AIRCRAFT AT THE AIRPORT. CONSTRUCTION ACTIVITIES CAN HAVE NEGATIVE IMPACTS ON THE FUNCTIONALITY AND SERVICEABILITY OF NAVAIDS. THE CONTRACTOR MUST COORDINATE THEIR WORK EFFORT AND LIMIT THEIR OPERATIONS SO THAT NAVAIDS ARE NOT IMPACTED.
2. CONTRACTOR MUST LIMIT OPERATIONS SO THAT MATERIAL, EQUIPMENT, AND PERSONNEL DO NOT ENTER NAVAID CRITICAL AREAS OR DISTURB POWER TO NAVAID FACILITIES WITHOUT PRIOR COORDINATION WITH THE MSCAA OPERATIONS MANAGER AND MSCAA PROJECT ENGINEER.
3. PLANNED NAVAID IMPACTS MUST BE ADDRESSED IN THE CONTRACTORS CONSTRUCTION SCHEDULE. CONTRACTOR IS REQUIRED TO PROVIDE A 45 DAY NOTICE AS WELL AS A MINIMUM 72 HOUR NOTICE TO THE MSCAA OPERATIONS MANAGER AND THE MSCAA PROJECT ENGINEER.

**CONTRACTOR ACCESS**

1. NO CONTRACTOR'S VEHICLES OR PERSONNEL WILL BE ALLOWED TO ENTER ONTO OR CROSS ACTIVE AIRFIELD PAVEMENTS OR THEIR SAFETY AREAS WITHOUT A CLASS 3 ESCORT. INCURSION ONTO OR ACROSS AN ACTIVE RUNWAY, TAXIWAY, OR RUNWAY APPROACHES WITHOUT PRIOR APPROVAL IS A SERIOUS VIOLATION THAT WILL SUBJECT THE CONTRACTOR TO THE MAXIMUM FINE ALLOWED.
2. THE CONTRACTOR SHALL MEET THE SECURITY AND ACCESS REQUIREMENTS LISTED IN THE CONTRACT DOCUMENTS. REFER TO CSPP PLANS AND A-102 SPECIFICATION FOR ADDITIONAL INFORMATION.

GENERAL SAFETY AND PHASING NOTES (CONTINUED):

3. THE CONTRACTOR SHALL KEEP ALL PAVEMENTS USED BY THEIR VEHICLES CLEAN AND FREE OF FOD AT ALL TIMES. AT A MINIMUM, PAVEMENTS SHALL BE SWEEPED AT THE END OF EACH WORKING DAY, OR AS DIRECTED BY THE MSCAA PROJECT ENGINEER.
4. CONTRACTOR SHALL NOT ENTER INTO, OR PLACE STOCKPILES OR STORE MATERIALS AND FUEL IN THE RUNWAY OBSTACLE FREE ZONE OF ANY ACTIVE RUNWAY WITHOUT PRIOR COORDINATION WITH AIRPORT OPERATIONS STAFF. THE GROUND LEVEL PORTION OF THE OBSTACLE FREE ZONE EXTENDS TO 250' FROM THE CENTERLINE OF ANY ACTIVE RUNWAY.
5. VEHICLES DRIVING ON THE AIRPORT MUST GIVE WAY TO ALL AIRCRAFT. DO NOT DRIVE UNDER AIRCRAFT BOARDING BRIDGES. FOLLOW POSTED SPEED LIMIT SIGNS AND DO NOT DRIVE IN A RECKLESS MANNER. REPORT ALL VEHICLE AND AIRCRAFT INCIDENTS, PERSONAL INJURY AND PROPERTY DAMAGE TO AIRPORT OPERATIONS. DRIVE ONLY ON DESIGNATED VEHICLE ROUTES.
6. THE CONTRACTOR MUST LIMIT OPERATION OF THEIR EQUIPMENT AND VEHICLES TO THE DEFINED CONSTRUCTION AREAS. DURING NON-CONSTRUCTION PERIODS, THE CONTRACTOR MUST RELOCATE EQUIPMENT AND VEHICLES FROM THE WORK AREA TO THE STAGING AREA.
7. CONTRACTOR ACCESS ONTO THE AOA IS LIMITED TO THE GATES SHOWN ON THE PROJECT SAFETY AND PHASING PLANS. NO PERSON SHALL ENTER THE AOA, OR ANY OTHER RESTRICTED AREA, EXCEPT AUTHORIZED PERSONNEL AND ESCORTED BY AN APPROPRIATELY BADGED ESCORT. CONTRACTOR OPERATIONS WITHIN THE AOA ARE LIMITED TO THE AREAS SHOWN ON THE PROJECT SAFETY AND PHASING PLANS. CONSTRUCTION VEHICLES AND PERSONNEL MUST NOT LEAVE THE PROJECT AREAS AT ANY TIME WITHOUT AN APPROVED ESCORT.
8. CONTRACTOR EMPLOYEE PERSONAL VEHICLES MAY NOT BE PARKED OR DRIVEN IN THE AOA. CONTRACTOR VEHICLES AND EQUIPMENT ARE ALLOWED INSIDE OF THE PROJECT WORK AREA WITHIN THE AOA. CONTRACTOR VEHICLES AND EQUIPMENT SHALL BE PARKED NO CLOSER THAN 10 FEET FROM ANY SECURITY FENCE.
9. THE PHASING PLAN SHEETS DEPICT HAUL ROUTES AND SITE ACCESS FROM SURROUNDING PUBLIC ROADWAYS AND HAUL ROUTES TO THE PROJECT WORK AREA THROUGH THE AIRPORT PERIMETER FENCE. CONTRACTOR ACCESS AND HAULING OPERATIONS ARE STRICTLY LIMITED TO THE HAUL ROUTES SHOWN. CONTRACTOR IS RESPONSIBLE FOR ANY PERMITS, SIGNAGE, IMPROVEMENTS, AND MAINTENANCE TO HAUL ROUTES AS NEEDED TO EFFICIENTLY PERFORM CONSTRUCTION ACTIVITIES, UNLESS SHOWN OTHERWISE ON THE PLANS. FOLLOWING COMPLETION OF CONSTRUCTION CONTRACTOR IS REQUIRED TO RESTORE HAUL ROUTES AND STAGING AND STORAGE AREAS TO THEIR ORIGINAL CONDITION.
10. DRIVING PRIVILEGES INSIDE THE AOA ARE LIMITED TO VEHICLES AND PERSONNEL WITH AN OPERATIONAL NECESSITY AND WHO HAVE BEEN BADGED AND APPROVED BY THE MSCAA BADGING OFFICE. TO DRIVE ON ANY PART OF THE AIRPORT OTHER THAN THE WORK AREAS AS DEFINED IN THE PROJECT PHASING PLANS THE CONTRACTOR MUST FIRST COORDINATE WITH AIRPORT OPERATIONS PERSONNEL. ALL VEHICLES OPERATING OUTSIDE OF THE PROJECT WORK AREA MUST BE ESCORTED BY BADGED PERSONNEL. VEHICLE OPERATORS ARE EXPECTED TO FAMILIARIZE THEMSELVES WITH AIRPORT SIGNS AND MARKINGS.
11. THE PROJECT PLANS SHOW THE ENTRY POINT(S), CONTRACTOR'S STAGING AREA, AND WORK AREA. THE CONTRACTOR SHALL PROVIDE SECURITY FOR THESE AREAS. THE CONTRACTOR IS TO PROVIDE TO THE AIRPORT, FOR REVIEW AND APPROVAL, ALL SECURITY MEASURES, BARRICADES, AND OTHER MEANS TO BE TAKEN TO SECURE SCHEDULED OPENINGS BETWEEN THE SECURE AND NON-SECURE AREAS, PRIOR TO CREATING THE OPENING. THE AIRPORT PROVIDES SECURITY OVERSIGHT AND PATROLS OF THE AIRPORT, BUT THE CONTRACTOR SHOULD NOT RELY ON THE PATROLS TO PROVIDE FULL-TIME SECURITY.
12. IN THE EVENT THE CONTRACTOR IS REQUIRED TO REMOVE AN EXISTING GATE OR PORTION OF THE AOA FENCE, THE CONTRACTOR MUST PROVIDE PERSONNEL WHO ARE BADGED AND APPROVED BY THE AIRPORT OPERATIONS MANAGER TO POSITIVELY CONTROL THE OPENING UNTIL IT HAS BEEN RE-SECURED AND INSPECTED BY THE OPERATIONS CENTER. UNDER NO CIRCUMSTANCES WILL THE CONTRACTOR BE PERMITTED TO LEAVE AN OPENING IN THE AOA FENCE AT THE END OF A WORK DAY.
13. LICENSED VEHICLES ARE REQUIRED TO DISPLAY THE FOLLOWING SAFETY & SECURITY ITEMS IN ACCORDANCE WITH THE LATEST VERSION OF FAA AC 150/5210-5:
  - SIGNS: COMPANY SIGNS ARE REQUIRED AT ALL TIMES. COMPANY SIGNS MUST BE AFFIXED TO BOTH SIDES OF THE VEHICLE, INCLUDE THE COMPANY LOGO IN A CONTRASTING COLOR, BE A MINIMUM OF 200 SQUARE INCHES, AND BE APPROVED BY THE AIRPORT.
  - BEACONS: BEACONS ARE REQUIRED AT ALL TIMES. A SINGLE BEACON SHALL BE AFFIXED TO THE UPPERMOST PART OF EACH VEHICLE. BEACONS SHALL BE YELLOW/AMBER ROTATING LIGHTS VISIBLE FROM ANY DIRECTION, INCLUDING THE AIR.
  - FLAGS: CONSTRUCTION WARNING FLAGS ARE NOT REQUIRED FOR LICENSED VEHICLES THAT ARE IN CONSTANT TWO-WAY RADIO COMMUNICATION WITH THE ATCT AND PROPERLY EQUIPPED TO OPERATE IN THE AOA WITH SIGNS AND A BEACON.
14. ESCORTED VEHICLES/EQUIPMENT ARE REQUIRED TO DISPLAY THE FOLLOWING SAFETY & SECURITY ITEMS IN ACCORDANCE WITH THE LATEST VERSION OF FAA AC 150/5210-5:
  - FLAGS: CONSTRUCTION WARNING FLAGS ARE REQUIRED AT ALL TIMES FOR ESCORTED VEHICLES/EQUIPMENT. ESCORTED VEHICLES/EQUIPMENT REQUIRING FLAGS INCLUDE BUT ARE NOT LIMITED TO HAUL TRUCKS, CONTRACTOR/SUBCONTRACTOR VEHICLES, SELF-PROPELLED MACHINERY, AND MISCELLANEOUS MOTORIZED EQUIPMENT. FLAGS SHALL BE AFFIXED TO BOTH SIDES OF THE UPPERMOST PART OF THE VEHICLE/EQUIPMENT, BE A MINIMUM OF 3-FOOT BY 3-FOOT WITH AN ORANGE AND WHITE CHECKERED PATTERN, AND HAVE PATTERN SQUARES THAT ARE 1-FOOT BY 1-FOOT, VISIBLE FROM BOTH SIDES. WHEN A BEACON IS PRESENT CONSTRUCTION WARNING FLAGS ARE NOT REQUIRED.

**STOCKPILED MATERIAL**

1. DEMOLISHED MATERIAL SHALL BECOME PROPERTY OF THE CONTRACTOR UPON LEAVING THE AOA FENCE. CONTRACTOR SHALL HAUL DEMOLISHED MATERIAL OFF SITE OR WASTE AT MCKELLAR PARK WITH AIRPORT APPROVAL.
2. CONTRACTOR TO MANAGE STOCKPILES SO THAT THEY DO NOT ATTRACT WILDLIFE. CONTRACTOR TO MANAGE STOCKPILES SO THAT THEY DO NOT CREATE FOD.
3. STOCKPILES SHALL BE NO CLOSER THAN 10 FEET TO A SECURITY FENCE.

**WILDLIFE MANAGEMENT**

1. FOOD SCRAPS MUST BE COLLECTED FROM CONSTRUCTION PERSONNEL ACTIVITY.
2. ANY ACTIVITY TAKING PLACE THAT CREATES A STANDING BODY OF WATER MUST BE REMEDIED WITHIN 24 HOURS.
3. MOWING SCHEDULES HAVE BEEN ESTABLISHED TO MAINTAIN, WHEN POSSIBLE, A HEIGHT OF 6 TO 10 INCHES, TO HELP PREVENT WILDLIFE FROM BECOMING A HAZARD. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO MAINTAIN A MAXIMUM GRASS HEIGHT OF 10 INCHES WITHIN THE CONSTRUCTION WORK AREA. THIS ITEM IS NOT ANTICIPATED TO OCCUR ON THIS PROJECT.

**FOREIGN OBJECT DEBRIS (FOD)**

1. FOREIGN OBJECT DEBRIS AT AIRPORTS INCLUDES ANY OBJECT FOUND IN AN INAPPROPRIATE LOCATION THAT CAN DAMAGE AIRCRAFT, EQUIPMENT, OR AIRPORT PERSONNEL. ON CONSTRUCTION SITES FOD TYPICALLY IS COMPRISED OF LOOSE GRAVEL, BLOWING SAND, WIRE BRISTLES FROM SWEEPER HEADS, FOOD WRAPPERS, OR MATERIAL PACKAGING.
2. THE PRESENCE OF FOD ON AN AIRPORT'S AOA POSES A SIGNIFICANT THREAT TO THE SAFETY OF AIR TRAVEL. FOD HAS THE POTENTIAL TO DAMAGE AIRCRAFT DURING CRITICAL PHASES OF FLIGHT, WHICH CAN LEAD TO CATASTROPHIC LOSS OF LIFE AND AIRFRAME, AND AT THE VERY LEAST INCREASED MAINTENANCE AND OPERATING COSTS.

METHODS OF FOD CONTROL:

1. CONTRACTOR SHALL PROVIDE TRAINING TO ALL EMPLOYEES WORKING WITHIN THE AOA ON EFFECTIVE FOD MANAGEMENT. TRAINING SHALL INCLUDE DESCRIPTION AND CONSEQUENCES OF FOD, FOD AWARENESS, AND GOOD HOUSEKEEPING PROCEDURES.
2. PREVENTING FOD FROM OCCURRING IS THE MOST EFFECTIVE FORM OF FOD MANAGEMENT. CONTRACTOR MUST MONITOR CONSTRUCTION ACTIVITIES AND PROACTIVELY DEVELOP A PLAN TO PREVENT FOD FROM OCCURRING. TYPICAL FOD PREVENTION MEASURES INCLUDE THE USE OF COVERED TRASH CONTAINERS, COVERED LOADS, ZERO TOLERANCE OF LITTERING, AND TYING DOWN ITEMS WHICH MAY BE EASILY WIND BLOWN.
3. CONTRACTOR MUST IMMEDIATELY REMOVE ANY DEBRIS FROM ACTIVE AIRFIELD AND HAUL ROUTE PAVEMENTS THAT RESULTS FROM CONSTRUCTION ACTIVITY.
4. CONTRACTOR MUST ACTIVELY IMPLEMENT DUST CONTROL MEASURES TO MINIMIZE THE AMOUNT OF DUST ORIGINATING FROM THE WORK AREA, HAUL ROUTES, AND STAGING AND STORAGE AREAS. DUST ON THE AIRFIELD PAVEMENTS WILL NOT BE TOLERATED. IF PROPER MEASURES ARE NOT IMPLEMENTED TO CONTROL DUST, WORK WILL BE STOPPED UNTIL PROPER MEASURES ARE IMPLEMENTED.

PREPARED FOR:



PREPARED BY:

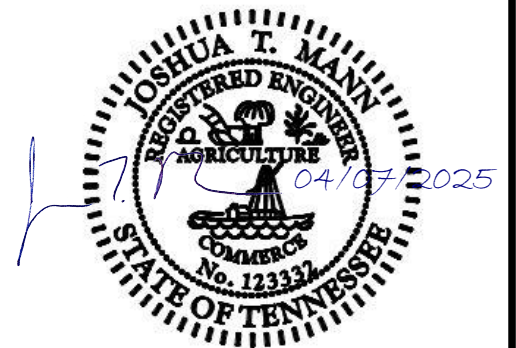


KEY MAP:

NO	DATE	BY	REVISION	
			DESCRIPTION	

REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

**CONSTRUCTION SAFETY & PHASING -  
NOTES**



DESIGNED: WUH	DRAWN: WUH	CHECKED: JTM
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PROJECT NO: 0024M300.10

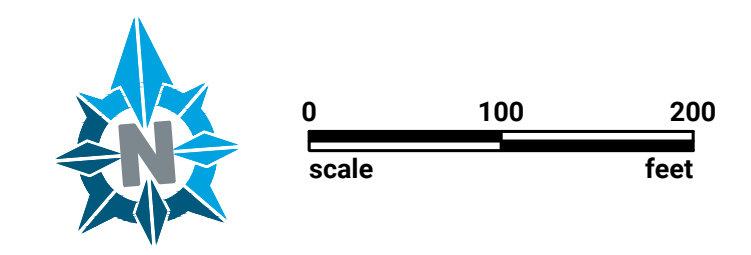
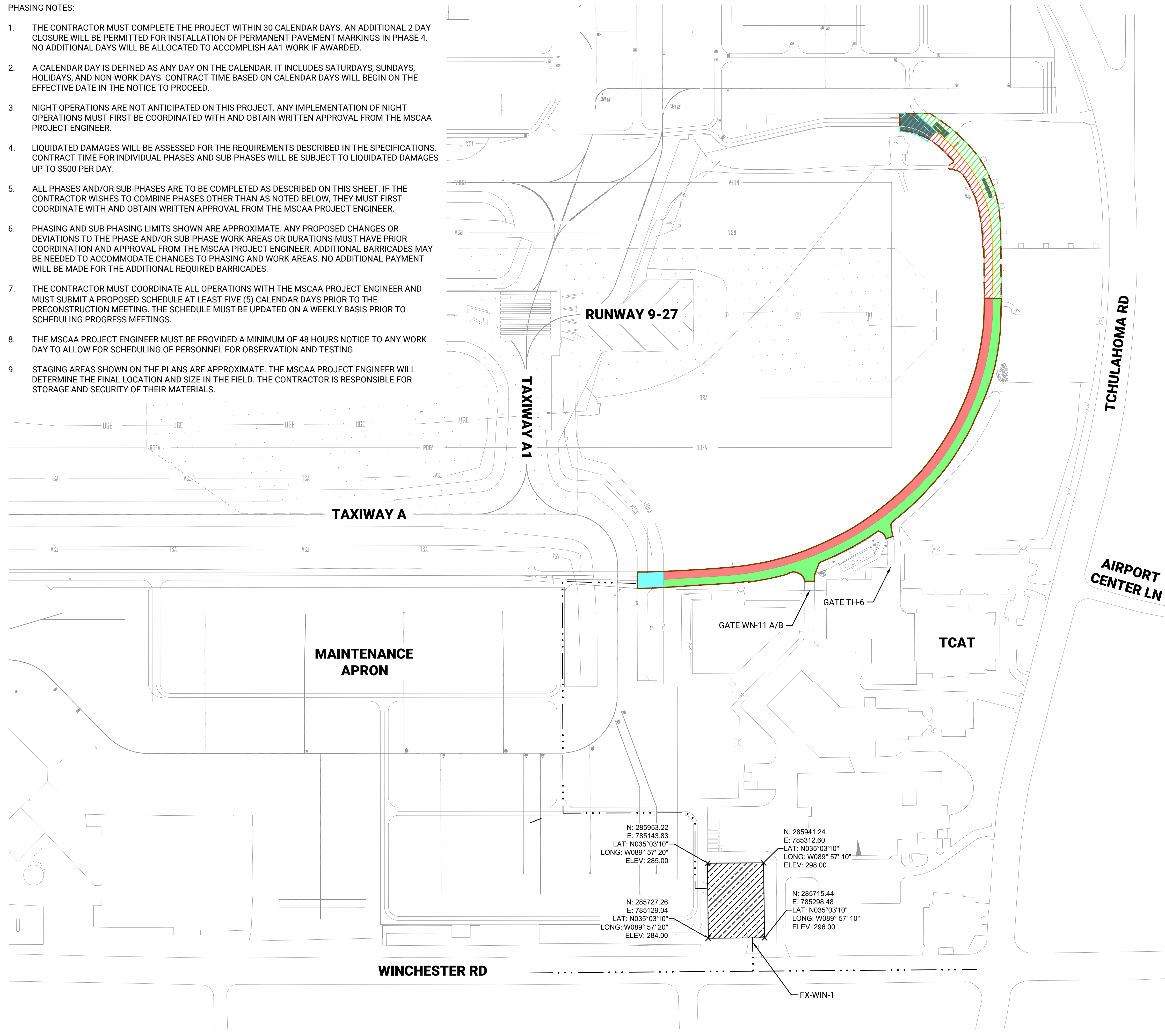
PRINT DATE: 4/7/2025

SHEET NO.

**GC1.00**

PHASING NOTES:

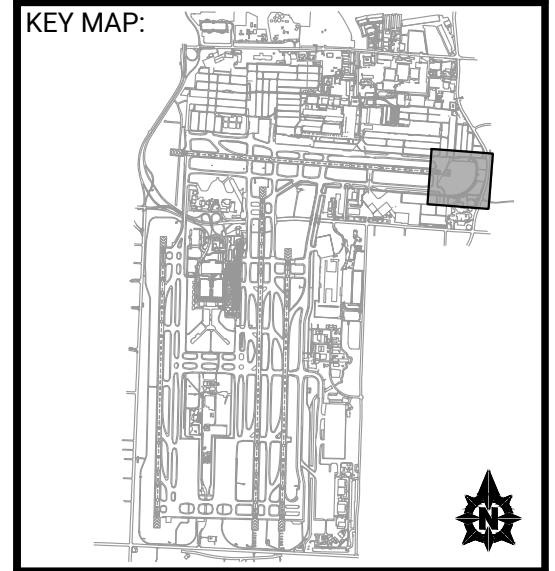
1. THE CONTRACTOR MUST COMPLETE THE PROJECT WITHIN 30 CALENDAR DAYS. AN ADDITIONAL 2 DAY CLOSURE WILL BE PERMITTED FOR INSTALLATION OF PERMANENT PAVEMENT MARKINGS IN PHASE 4. NO ADDITIONAL DAYS WILL BE ALLOCATED TO ACCOMPLISH AA1 WORK IF AWARDED.
2. A CALENDAR DAY IS DEFINED AS ANY DAY ON THE CALENDAR. IT INCLUDES SATURDAYS, SUNDAYS, HOLIDAYS, AND NON-WORK DAYS. CONTRACT TIME BASED ON CALENDAR DAYS WILL BEGIN ON THE EFFECTIVE DATE IN THE NOTICE TO PROCEED.
3. NIGHT OPERATIONS ARE NOT ANTICIPATED ON THIS PROJECT. ANY IMPLEMENTATION OF NIGHT OPERATIONS MUST FIRST BE COORDINATED WITH AND OBTAIN WRITTEN APPROVAL FROM THE MSCAA PROJECT ENGINEER.
4. LIQUIDATED DAMAGES WILL BE ASSESSED FOR THE REQUIREMENTS DESCRIBED IN THE SPECIFICATIONS. CONTRACT TIME FOR INDIVIDUAL PHASES AND SUB-PHASES WILL BE SUBJECT TO LIQUIDATED DAMAGES UP TO \$500 PER DAY.
5. ALL PHASES AND/OR SUB-PHASES ARE TO BE COMPLETED AS DESCRIBED ON THIS SHEET. IF THE CONTRACTOR WISHES TO COMBINE PHASES OTHER THAN AS NOTED BELOW, THEY MUST FIRST COORDINATE WITH AND OBTAIN WRITTEN APPROVAL FROM THE MSCAA PROJECT ENGINEER.
6. PHASING AND SUB-PHASING LIMITS SHOWN ARE APPROXIMATE. ANY PROPOSED CHANGES OR DEVIATIONS TO THE PHASE AND/OR SUB-PHASE WORK AREAS OR DURATIONS MUST HAVE PRIOR COORDINATION AND APPROVAL FROM THE MSCAA PROJECT ENGINEER. ADDITIONAL BARRICADES MAY BE NEEDED TO ACCOMMODATE CHANGES TO PHASING AND WORK AREAS. NO ADDITIONAL PAYMENT WILL BE MADE FOR THE ADDITIONAL REQUIRED BARRICADES.
7. THE CONTRACTOR MUST COORDINATE ALL OPERATIONS WITH THE MSCAA PROJECT ENGINEER AND MUST SUBMIT A PROPOSED SCHEDULE AT LEAST FIVE (5) CALENDAR DAYS PRIOR TO THE PRECONSTRUCTION MEETING. THE SCHEDULE MUST BE UPDATED ON A WEEKLY BASIS PRIOR TO SCHEDULING PROGRESS MEETINGS.
8. THE MSCAA PROJECT ENGINEER MUST BE PROVIDED A MINIMUM OF 48 HOURS NOTICE TO ANY WORK DAY TO ALLOW FOR SCHEDULING OF PERSONNEL FOR OBSERVATION AND TESTING.
9. STAGING AREAS SHOWN ON THE PLANS ARE APPROXIMATE. THE MSCAA PROJECT ENGINEER WILL DETERMINE THE FINAL LOCATION AND SIZE IN THE FIELD. THE CONTRACTOR IS RESPONSIBLE FOR STORAGE AND SECURITY OF THEIR MATERIALS.



LEGEND		
EXISTING	PROPOSED	
		PAVEMENT - ASPHALT (BASE BID)
N/A		PAVEMENT - ASPHALT (ADD. ALT. 1)
	N/A	RIPRAP
N/A		STAGING AREA
N/A		PHASE 1 - BASE BID LIMITS
N/A		PHASE 1 - ADD. ALT. 1 LIMITS
N/A		PHASE 2 - BASE BID LIMITS
N/A		PHASE 2 - ADD. ALT. 1 LIMITS
N/A		PHASE 3 - BASE BID LIMITS
N/A		PHASE 3 - ADD. ALT. 1 LIMITS
N/A		PHASE 4 - BASE BID LIMITS
N/A		PHASE 4 - ADD. ALT. 1 LIMITS
	N/A	RUNWAY SAFETY AREA
	N/A	RUNWAY OBJECT FREE AREA
	N/A	TAXIWAY SAFETY AREA
	N/A	TAXIWAY OBJECT FREE AREA
N/A		HAUL ROUTE
N/A		LOW PROFILE BARRICADE
N/A		MEDIUM PROFILE BARRICADE
N/A		TRAFFIC CONTROL CONE
	N/A	STORM SEWER
	N/A	AIRFIELD FENCE
		PAVEMENT MARKINGS

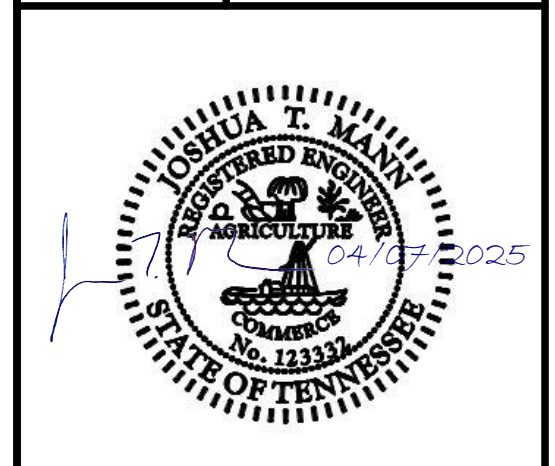
AIRFIELD PHASING CLOSURES		
PHASE	DURATION	CLOSURES
PHASE 1	10 DAYS	VSR EAST
PHASE 2	10 DAYS	VSR WEST
PHASE 3	10 DAYS	TAXIWAY A EAST APRON CONNECTOR
INT.	30 DAYS	OPEN - NO CONSTRUCTION
PHASE 4	2 DAYS (INT.)	TAXIWAY A EAST APRON CONNECTOR VSR SINGLE LANE CLOSURES

NOTE: PHASE 4 SHALL CONSIST OF FINAL MARKINGS ONLY WITH INTERMITTENT SINGLE LANE CLOSURES TO ALLOW PAINT CURING.



NO.	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
 MEMPHIS INTERNATIONAL AIRPORT  
 MEMPHIS, TN  
**CONSTRUCTION SAFETY & PHASING - OVERVIEW**



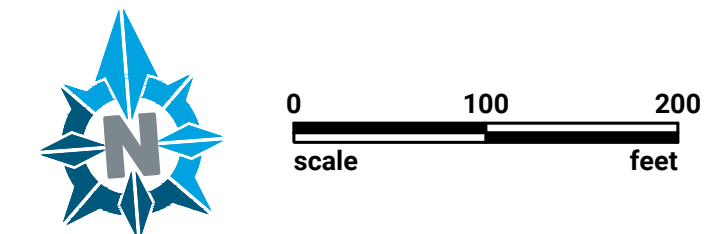
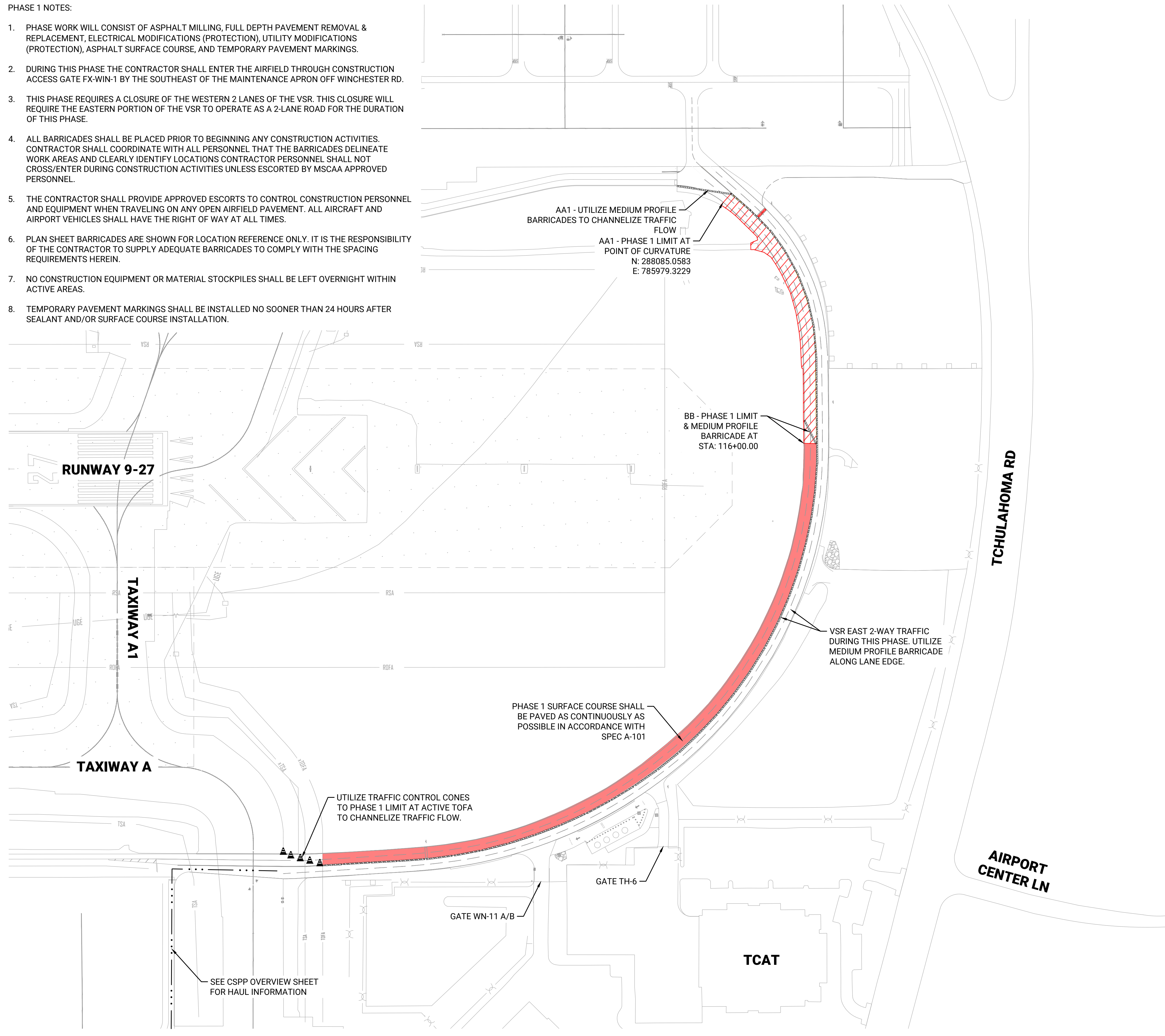
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PROJECT NO: 0024M300.10		
PRINT DATE: 4/7/2025		
SHEET NO.		

**GC2.00**

CAD FILE: C:\PW\WORKDIR\PW\LE\FVD\WUH\024M300\GC2.DWG - CSPP - DWG Layout - CSPP - DWG - BY: HALEY, WILLU

PHASE 1 NOTES:

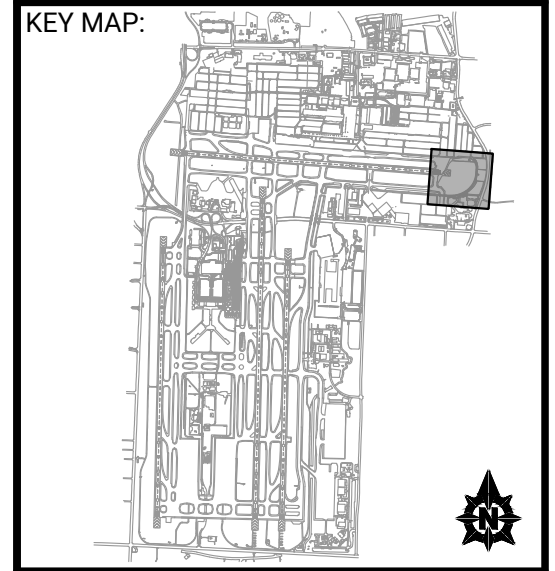
1. PHASE WORK WILL CONSIST OF ASPHALT MILLING, FULL DEPTH PAVEMENT REMOVAL & REPLACEMENT, ELECTRICAL MODIFICATIONS (PROTECTION), UTILITY MODIFICATIONS (PROTECTION), ASPHALT SURFACE COURSE, AND TEMPORARY PAVEMENT MARKINGS.
2. DURING THIS PHASE THE CONTRACTOR SHALL ENTER THE AIRFIELD THROUGH CONSTRUCTION ACCESS GATE FX-WIN-1 BY THE SOUTHEAST OF THE MAINTENANCE APRON OFF WINCHESTER RD.
3. THIS PHASE REQUIRES A CLOSURE OF THE WESTERN 2 LANES OF THE VSR. THIS CLOSURE WILL REQUIRE THE EASTERN PORTION OF THE VSR TO OPERATE AS A 2-LANE ROAD FOR THE DURATION OF THIS PHASE.
4. ALL BARRICADES SHALL BE PLACED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL COORDINATE WITH ALL PERSONNEL THAT THE BARRICADES DELINEATE WORK AREAS AND CLEARLY IDENTIFY LOCATIONS CONTRACTOR PERSONNEL SHALL NOT CROSS/ENTER DURING CONSTRUCTION ACTIVITIES UNLESS ESCORTED BY MSCAA APPROVED PERSONNEL.
5. THE CONTRACTOR SHALL PROVIDE APPROVED ESCORTS TO CONTROL CONSTRUCTION PERSONNEL AND EQUIPMENT WHEN TRAVELING ON ANY OPEN AIRFIELD PAVEMENT. ALL AIRCRAFT AND AIRPORT VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
6. PLAN SHEET BARRICADES ARE SHOWN FOR LOCATION REFERENCE ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY ADEQUATE BARRICADES TO COMPLY WITH THE SPACING REQUIREMENTS HEREIN.
7. NO CONSTRUCTION EQUIPMENT OR MATERIAL STOCKPILES SHALL BE LEFT OVERNIGHT WITHIN ACTIVE AREAS.
8. TEMPORARY PAVEMENT MARKINGS SHALL BE INSTALLED NO SOONER THAN 24 HOURS AFTER SEALANT AND/OR SURFACE COURSE INSTALLATION.



LEGEND		
EXISTING	PROPOSED	
[White Box]	[Dark Grey Box]	PAVEMENT - ASPHALT (BASE BID)
[White Box]	[Hatched Box]	PAVEMENT - ASPHALT (ADD. ALT. 1)
[Stippled Box]	[White Box]	RIPRAP
N/A	[Hatched Box]	STAGING AREA
N/A	[Red Box]	PHASE 1 - BASE BID LIMITS
N/A	[Red Hatched Box]	PHASE 1 - ADD. ALT. 1 LIMITS
N/A	[Green Box]	PHASE 2 - BASE BID LIMITS
N/A	[Green Hatched Box]	PHASE 2 - ADD. ALT. 1 LIMITS
N/A	[Cyan Box]	PHASE 3 - BASE BID LIMITS
N/A	[Cyan Hatched Box]	PHASE 3 - ADD. ALT. 1 LIMITS
N/A	[Orange Box]	PHASE 4 - BASE BID LIMITS
N/A	[Orange Dashed Box]	PHASE 4 - ADD. ALT. 1 LIMITS
[Dashed Line]	N/A	RUNWAY SAFETY AREA
[Dashed Line]	N/A	RUNWAY OBJECT FREE AREA
[Dashed Line]	N/A	TAXIWAY SAFETY AREA
[Dashed Line]	N/A	TAXIWAY OBJECT FREE AREA
N/A	[Dotted Line]	HAUL ROUTE
N/A	[Barricade Symbol]	LOW PROFILE BARRICADE
N/A	[Barricade Symbol]	MEDIUM PROFILE BARRICADE
N/A	[Cone Symbol]	TRAFFIC CONTROL CONE
[Circle with X]	N/A	STORM SEWER
[Line with X]	N/A	AIRFIELD FENCE
[Dashed Line]	[Dark Grey Box]	PAVEMENT MARKINGS

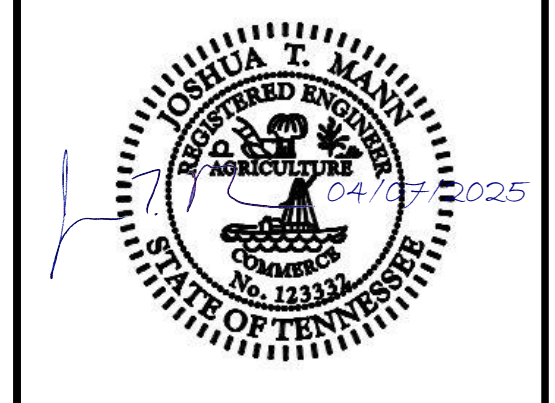
AIRFIELD PHASING CLOSURES		
PHASE	DURATION	CLOSURES
PHASE 1	10 DAYS	VSR EAST
PHASE 2	10 DAYS	VSR WEST
PHASE 3	10 DAYS	TAXIWAY A EAST APRON CONNECTOR
INT.	30 DAYS	OPEN - NO CONSTRUCTION
PHASE 4	2 DAYS (INT.)	TAXIWAY A EAST APRON CONNECTOR VSR SINGLE LANE CLOSURES

NOTE: PHASE 4 SHALL CONSIST OF FINAL MARKINGS ONLY WITH INTERMITTENT SINGLE LANE CLOSURES TO ALLOW PAINT CURING.



NO.	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
 MEMPHIS INTERNATIONAL AIRPORT  
 MEMPHIS, TN  
**CONSTRUCTION SAFETY & PHASING -  
 PHASE 1**



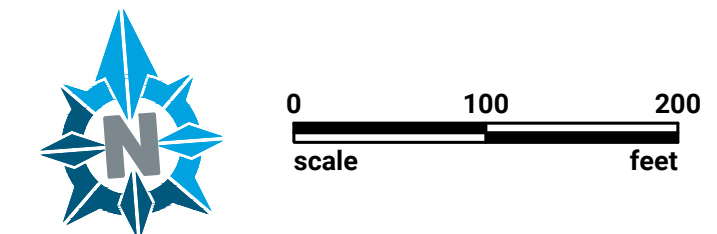
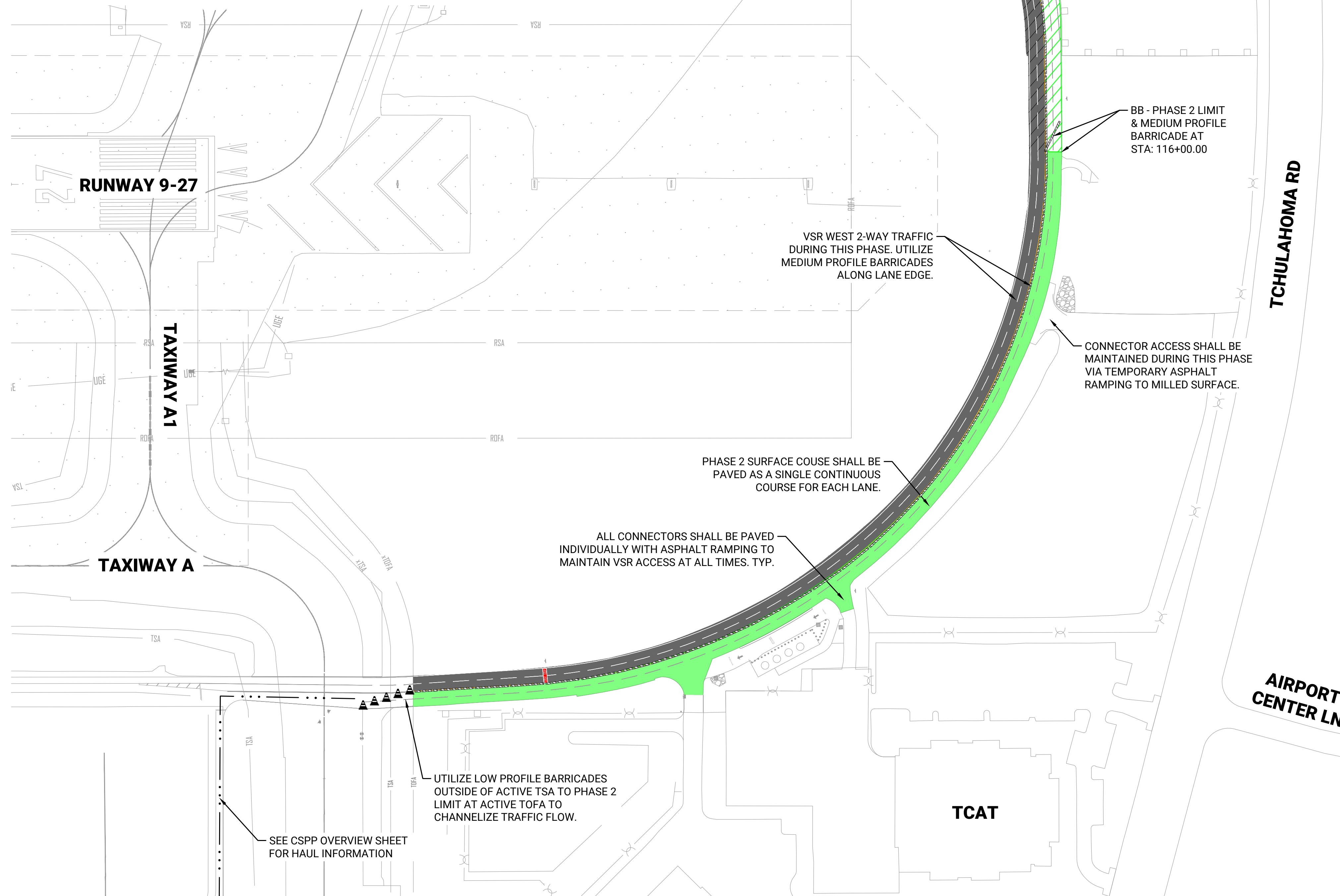
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PROJECT NO: 0024M300.10		
PRINT DATE: 4/7/2025		
SHEET NO.		

**GC2.01**

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PHASE 2 NOTES:

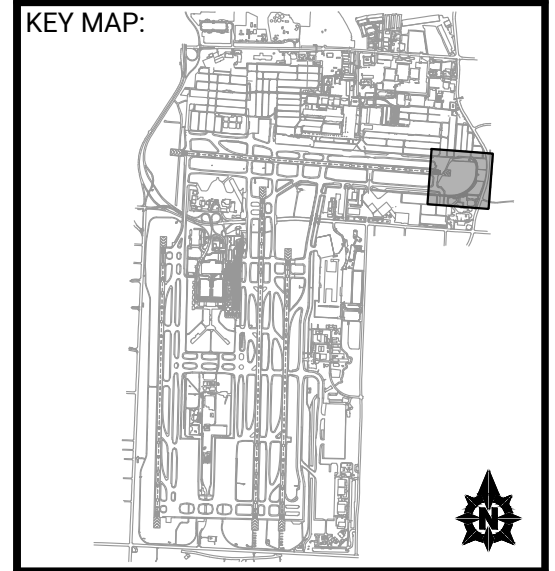
1. PHASE WORK WILL CONSIST OF ASPHALT MILLING, FULL DEPTH PAVEMENT REMOVAL & REPLACEMENT, ELECTRICAL MODIFICATIONS (PROTECTION), UTILITY MODIFICATIONS (PROTECTION), ASPHALT SURFACE COURSE, AND TEMPORARY PAVEMENT MARKINGS.
2. DURING THIS PHASE THE CONTRACTOR SHALL ENTER THE AIRFIELD THROUGH CONSTRUCTION ACCESS GATE FX-WIN-1 BY THE SOUTHEAST OF THE MAINTENANCE APRON OFF WINCHESTER RD.
3. THIS PHASE REQUIRES A CLOSURE OF THE EASTERN 2 LANES OF THE VSR. THIS CLOSURE WILL REQUIRE THE WESTERN PORTION OF THE VSR TO OPERATE AS A 2-LANE ROAD FOR THE DURATION OF THIS PHASE. THROUGH ACCESS FOR ALL CONNECTORS SHALL BE MAINTAINED AT ALL TIMES.
4. ALL BARRICADES SHALL BE PLACED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL COORDINATE WITH ALL PERSONNEL THAT THE BARRICADES DELINEATE WORK AREAS AND CLEARLY IDENTIFY LOCATIONS CONTRACTOR PERSONNEL SHALL NOT CROSS/ENTER DURING CONSTRUCTION ACTIVITIES UNLESS ESCORTED BY MSCAA APPROVED PERSONNEL.
5. THE CONTRACTOR SHALL PROVIDE APPROVED ESCORTS TO CONTROL CONSTRUCTION PERSONNEL AND EQUIPMENT WHEN TRAVELING ON ANY OPEN AIRFIELD PAVEMENT. ALL AIRCRAFT AND AIRPORT VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
6. PLAN SHEET BARRICADES ARE SHOWN FOR LOCATION REFERENCE ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY ADEQUATE BARRICADES TO COMPLY WITH THE SPACING REQUIREMENTS HEREIN.
7. NO CONSTRUCTION EQUIPMENT OR MATERIAL STOCKPILES SHALL BE LEFT OVERNIGHT WITHIN ACTIVE AREAS.
8. TEMPORARY PAVEMENT MARKINGS SHALL BE INSTALLED NO SOONER THAN 324 HOURS AFTER SEALANT AND/OR SURFACE COURSE INSTALLATION.



LEGEND		
EXISTING	PROPOSED	
[White Box]	[Dark Grey Box]	PAVEMENT - ASPHALT (BASE BID)
[White Box]	[Hatched Box]	PAVEMENT - ASPHALT (ADD. ALT. 1)
[Stippled Box]	[White Box]	RIPRAP
[White Box]	[Hatched Box]	STAGING AREA
[White Box]	[Red Box]	PHASE 1 - BASE BID LIMITS
[White Box]	[Red Hatched Box]	PHASE 1 - ADD. ALT. 1 LIMITS
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[White Box]	[Green Hatched Box]	PHASE 2 - ADD. ALT. 1 LIMITS
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[White Box]	[Cyan Hatched Box]	PHASE 3 - ADD. ALT. 1 LIMITS
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[Dashed Line]	[White Box]	RUNWAY SAFETY AREA
[Dashed Line]	[White Box]	RUNWAY OBJECT FREE AREA
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[Symbol]	[White Box]	LOW PROFILE BARRICADE
[Symbol]	[White Box]	MEDIUM PROFILE BARRICADE
[Symbol]	[White Box]	TRAFFIC CONTROL CONE
[Symbol]	[White Box]	STORM SEWER
[Symbol]	[White Box]	AIRFIELD FENCE
[Symbol]	[White Box]	PAVEMENT MARKINGS

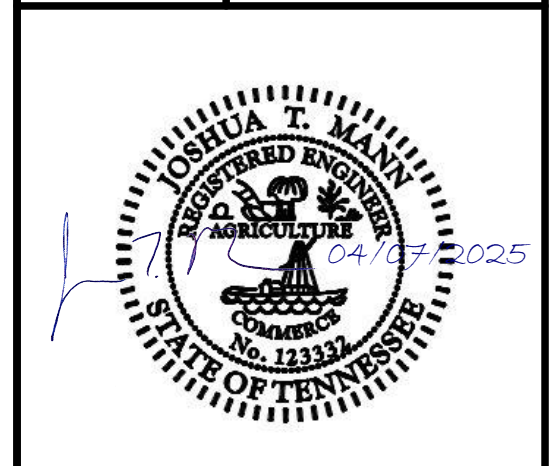
AIRFIELD PHASING CLOSURES		
PHASE	DURATION	CLOSURES
PHASE 1	10 DAYS	VSR EAST
PHASE 2	10 DAYS	VSR WEST
PHASE 3	10 DAYS	TAXIWAY A EAST APRON CONNECTOR
INT.	30 DAYS	OPEN - NO CONSTRUCTION
PHASE 4	2 DAYS (INT.)	TAXIWAY A EAST APRON CONNECTOR VSR SINGLE LANE CLOSURES

NOTE: PHASE 4 SHALL CONSIST OF FINAL MARKINGS ONLY WITH INTERMITTENT SINGLE LANE CLOSURES TO ALLOW PAINT CURING.



NO.	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
 MEMPHIS INTERNATIONAL AIRPORT  
 MEMPHIS, TN  
**CONSTRUCTION SAFETY & PHASING -  
 PHASE 2**



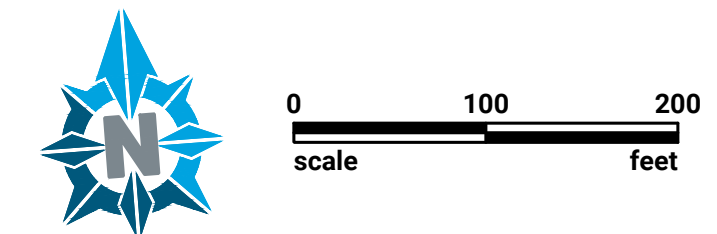
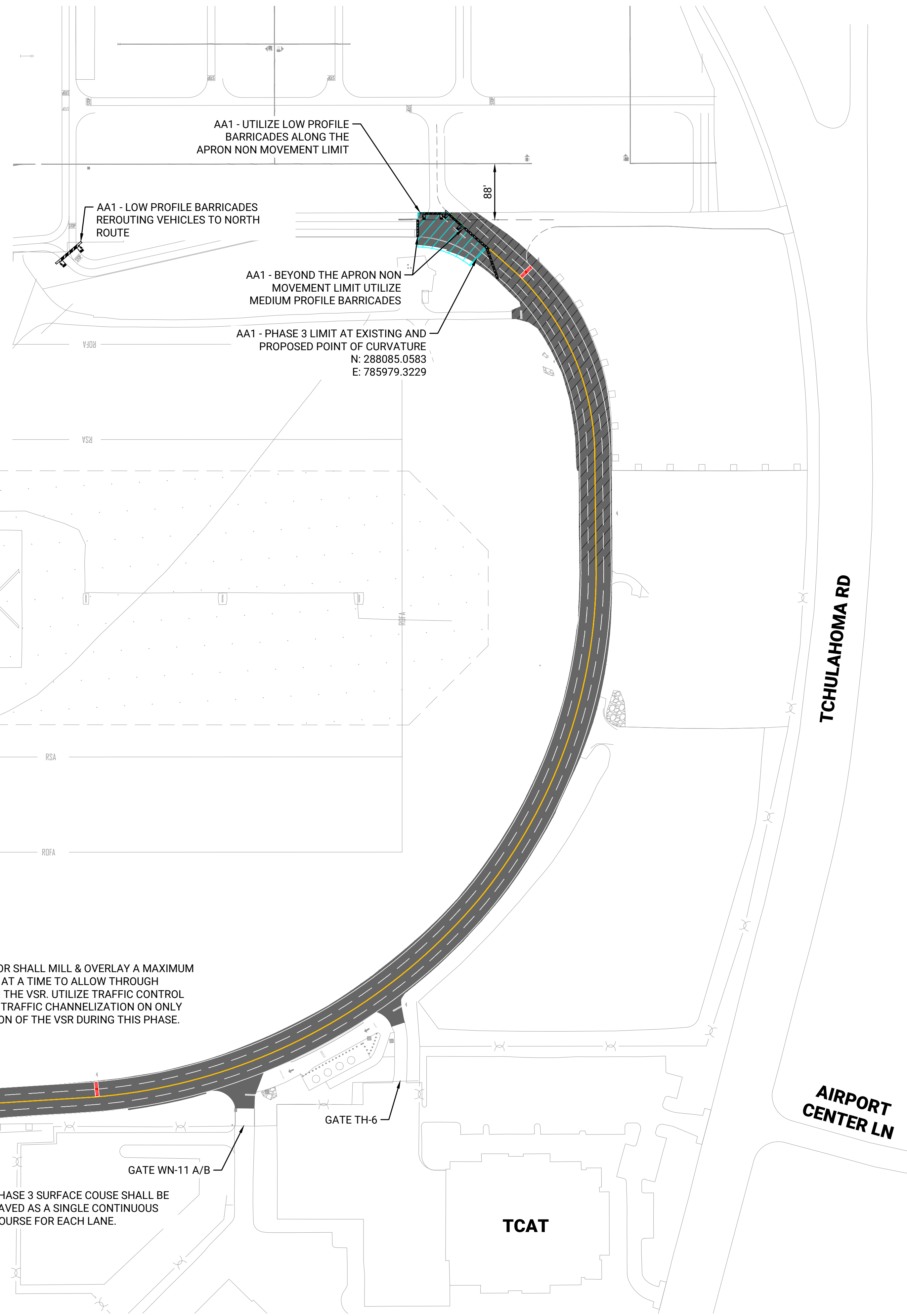
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**GC2.02**

CAD FILE: C:\PW\WORKDIR\PW\LE\FVD\WUH\0262740\GC2.DWG - CSPP - DWG Layout - CSPP#2 By: HALEY, WILLU

PHASE 3 NOTES:

1. PHASE WORK WILL CONSIST OF ASPHALT MILLING, ELECTRICAL MODIFICATIONS (PROTECTION), UTILITY MODIFICATIONS (PROTECTION), ASPHALT SURFACE COURSE, SHOULDER GRADING IMPROVEMENTS, AND TEMPORARY PAVEMENT MARKINGS.
2. DURING THIS PHASE THE CONTRACTOR SHALL ENTER THE AIRFIELD THROUGH CONSTRUCTION ACCESS GATE FX-WIN-1 BY THE SOUTHEAST OF THE MAINTENANCE APRON OFF WINCHESTER RD.
3. THIS PHASE REQUIRES CLOSURES OF THE TAXIWAY A EAST APRON CONNECTOR AND A NORTHEAST PORTION OF THE VSR WEST 2 LANES. THIS CLOSURE WILL REQUIRE THE NORTHEAST AND SOUTHWEST PORTIONS OF THE VSR TO OPERATE AS A 2-LANE ROAD FOR THE DURATION OF THIS PHASE. THROUGH ACCESS FOR ALL CONNECTORS SHALL BE MAINTAINED AT ALL TIMES.
4. CONTRACTOR SHALL CONFIRM THE MSCAA HAS THE NOTAM CLOSING THE WORK AREA PRIOR TO PLACEMENT OF BARRICADES. ALL BARRICADES SHALL BE PLACED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL COORDINATE WITH ALL PERSONNEL THAT THE BARRICADES DELINEATE WORK AREAS AND CLEARLY IDENTIFY LOCATIONS CONTRACTOR PERSONNEL SHALL NOT CROSS/ENTER DURING CONSTRUCTION ACTIVITIES UNLESS ESCORTED BY MSCAA APPROVED PERSONNEL.
5. THE CONTRACTOR SHALL PROVIDE APPROVED ESCORTS TO CONTROL CONSTRUCTION PERSONNEL AND EQUIPMENT WHEN TRAVELING ON ANY OPEN AIRFIELD PAVEMENT. ALL AIRCRAFT AND AIRPORT VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
6. PLAN SHEET BARRICADES ARE SHOWN FOR LOCATION REFERENCE ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY ADEQUATE BARRICADES TO COMPLY WITH THE SPACING REQUIREMENTS HEREIN.
7. NO CONSTRUCTION EQUIPMENT OR MATERIAL STOCKPILES SHALL BE LEFT OVERNIGHT WITHIN ACTIVE AREAS.
8. TEMPORARY PAVEMENT MARKINGS SHALL BE INSTALLED NO SOONER THAN 24 HOURS AFTER SEALANT AND/OR SURFACE COURSE INSTALLATION.



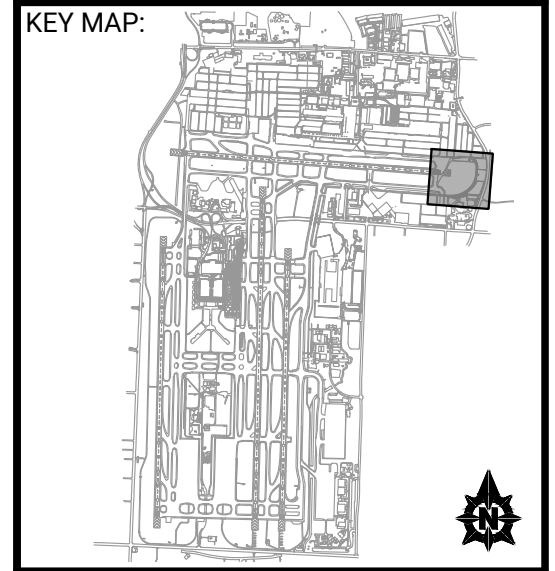
LEGEND		
EXISTING	PROPOSED	
[Symbol]	[Symbol]	PAVEMENT - ASPHALT (BASE BID)
[Symbol]	[Symbol]	PAVEMENT - ASPHALT (ADD. ALT. 1)
[Symbol]	[Symbol]	RIPRAP
N/A	[Symbol]	STAGING AREA
N/A	[Symbol]	PHASE 1 - BASE BID LIMITS
N/A	[Symbol]	PHASE 1 - ADD. ALT. 1 LIMITS
N/A	[Symbol]	PHASE 2 - BASE BID LIMITS
N/A	[Symbol]	PHASE 2 - ADD. ALT. 1 LIMITS
N/A	[Symbol]	PHASE 3 - BASE BID LIMITS
N/A	[Symbol]	PHASE 3 - ADD. ALT. 1 LIMITS
N/A	[Symbol]	PHASE 4 - BASE BID LIMITS
N/A	[Symbol]	PHASE 4 - ADD. ALT. 1 LIMITS
— RSA —	N/A	RUNWAY SAFETY AREA
— ROFA —	N/A	RUNWAY OBJECT FREE AREA
— TSA —	N/A	TAXIWAY SAFETY AREA
— TOFA —	N/A	TAXIWAY OBJECT FREE AREA
N/A	— . . . —	HAUL ROUTE
N/A	[Symbol]	LOW PROFILE BARRICADE
N/A	[Symbol]	MEDIUM PROFILE BARRICADE
N/A	[Symbol]	TRAFFIC CONTROL CONE
[Symbol]	N/A	STORM SEWER
[Symbol]	N/A	AIRFIELD FENCE
[Symbol]	[Symbol]	PAVEMENT MARKINGS

AIRFIELD PHASING CLOSURES		
PHASE	DURATION	CLOSURES
PHASE 1	10 DAYS	VSR EAST
PHASE 2	10 DAYS	VSR WEST
PHASE 3	10 DAYS	TAXIWAY A EAST APRON CONNECTOR
INT.	30 DAYS	OPEN - NO CONSTRUCTION
PHASE 4	2 DAYS (INT.)	TAXIWAY A EAST APRON CONNECTOR VSR SINGLE LANE CLOSURES

NOTE: PHASE 4 SHALL CONSIST OF FINAL MARKINGS ONLY WITH INTERMITTENT SINGLE LANE CLOSURES TO ALLOW PAINT CURING.

PREPARED FOR:

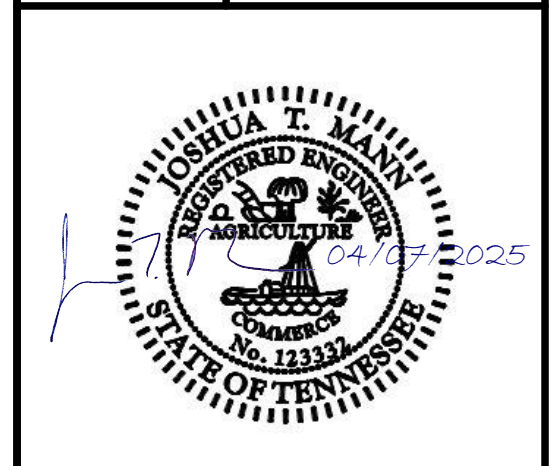
PREPARED BY:



NO	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

CONSTRUCTION SAFETY & PHASING -  
PHASE 3



DESIGNED: WUH	DRAWN: WUH	CHECKED: JTM
PROJECT NO: 0024M300.10		
PRINT DATE: 4/7/2025		
SHEET NO.		

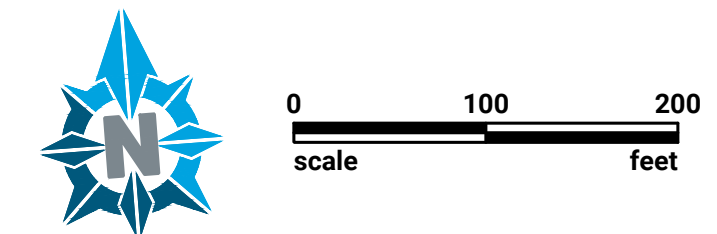
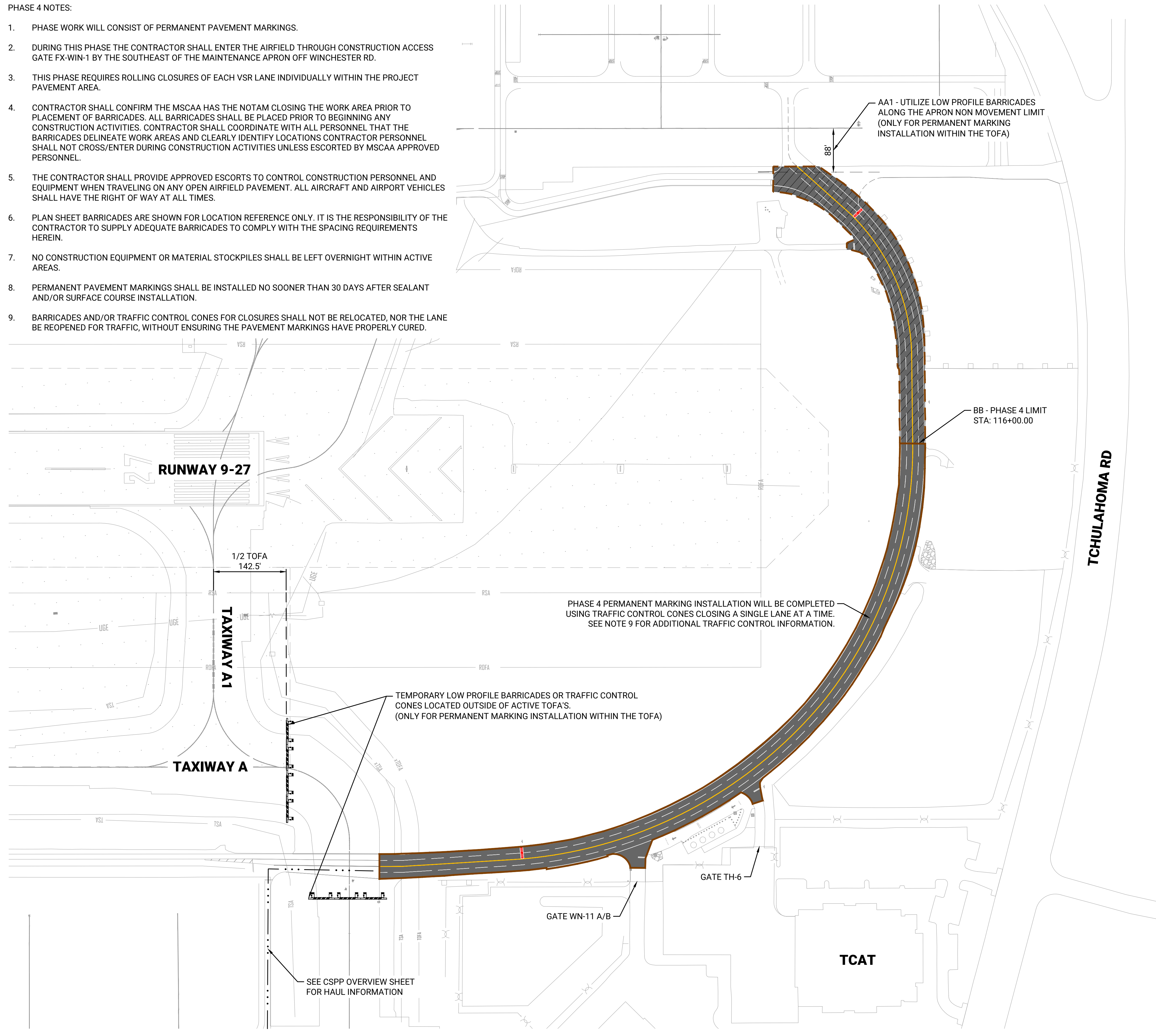
**GC2.03**

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PHASE 4 NOTES:

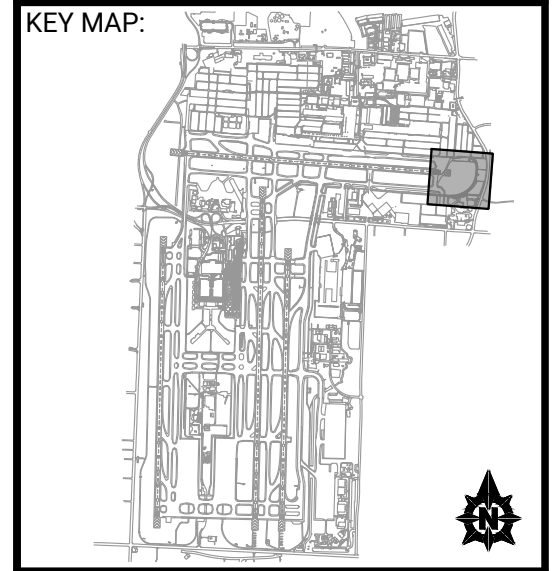
1. PHASE WORK WILL CONSIST OF PERMANENT PAVEMENT MARKINGS.
2. DURING THIS PHASE THE CONTRACTOR SHALL ENTER THE AIRFIELD THROUGH CONSTRUCTION ACCESS GATE FX-WIN-1 BY THE SOUTHEAST OF THE MAINTENANCE APRON OFF WINCHESTER RD.
3. THIS PHASE REQUIRES ROLLING CLOSURES OF EACH VSR LANE INDIVIDUALLY WITHIN THE PROJECT PAVEMENT AREA.
4. CONTRACTOR SHALL CONFIRM THE MSCAA HAS THE NOTAM CLOSING THE WORK AREA PRIOR TO PLACEMENT OF BARRICADES. ALL BARRICADES SHALL BE PLACED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES. CONTRACTOR SHALL COORDINATE WITH ALL PERSONNEL THAT THE BARRICADES DELINEATE WORK AREAS AND CLEARLY IDENTIFY LOCATIONS CONTRACTOR PERSONNEL SHALL NOT CROSS/ENTER DURING CONSTRUCTION ACTIVITIES UNLESS ESCORTED BY MSCAA APPROVED PERSONNEL.
5. THE CONTRACTOR SHALL PROVIDE APPROVED ESCORTS TO CONTROL CONSTRUCTION PERSONNEL AND EQUIPMENT WHEN TRAVELING ON ANY OPEN AIRFIELD PAVEMENT. ALL AIRCRAFT AND AIRPORT VEHICLES SHALL HAVE THE RIGHT OF WAY AT ALL TIMES.
6. PLAN SHEET BARRICADES ARE SHOWN FOR LOCATION REFERENCE ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY ADEQUATE BARRICADES TO COMPLY WITH THE SPACING REQUIREMENTS HEREIN.
7. NO CONSTRUCTION EQUIPMENT OR MATERIAL STOCKPILES SHALL BE LEFT OVERNIGHT WITHIN ACTIVE AREAS.
8. PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED NO SOONER THAN 30 DAYS AFTER SEALANT AND/OR SURFACE COURSE INSTALLATION.
9. BARRICADES AND/OR TRAFFIC CONTROL CONES FOR CLOSURES SHALL NOT BE RELOCATED, NOR THE LANE BE REOPENED FOR TRAFFIC, WITHOUT ENSURING THE PAVEMENT MARKINGS HAVE PROPERLY CURED.



LEGEND		
EXISTING	PROPOSED	
[White Box]	[Grey Box]	PAVEMENT - ASPHALT (BASE BID)
[White Box]	[Hatched Box]	PAVEMENT - ASPHALT (ADD. ALT. 1)
[Stippled Box]	[White Box]	RIPRAP
N/A	[Hatched Box]	STAGING AREA
N/A	[Red Box]	PHASE 1 - BASE BID LIMITS
N/A	[Red Hatched Box]	PHASE 1 - ADD. ALT. 1 LIMITS
N/A	[Green Box]	PHASE 2 - BASE BID LIMITS
N/A	[Green Hatched Box]	PHASE 2 - ADD. ALT. 1 LIMITS
N/A	[Cyan Box]	PHASE 3 - BASE BID LIMITS
N/A	[Cyan Hatched Box]	PHASE 3 - ADD. ALT. 1 LIMITS
N/A	[Orange Box]	PHASE 4 - BASE BID LIMITS
N/A	[Orange Dashed Box]	PHASE 4 - ADD. ALT. 1 LIMITS
[Dashed Line]	N/A	RUNWAY SAFETY AREA
[Dashed Line]	N/A	RUNWAY OBJECT FREE AREA
[Dashed Line]	N/A	TAXIWAY SAFETY AREA
[Dashed Line]	N/A	TAXIWAY OBJECT FREE AREA
N/A	[Dotted Line]	HAUL ROUTE
N/A	[Barricade Symbol]	LOW PROFILE BARRICADE
N/A	[Barricade Symbol]	MEDIUM PROFILE BARRICADE
N/A	[Cone Symbol]	TRAFFIC CONTROL CONE
[Circle Symbol]	N/A	STORM SEWER
[Line Symbol]	N/A	AIRFIELD FENCE
[Dashed Line]	[Grey Box]	PAVEMENT MARKINGS

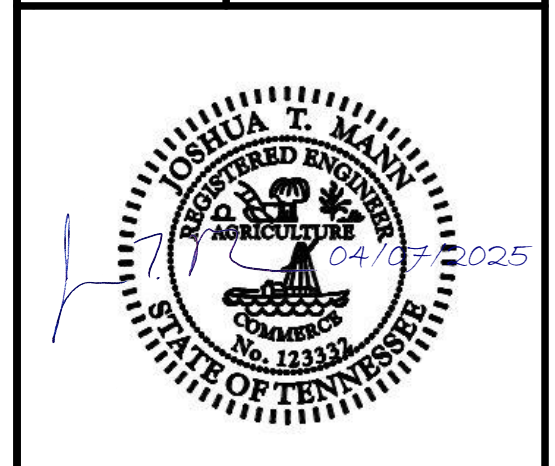
AIRFIELD PHASING CLOSURES		
PHASE	DURATION	CLOSURES
PHASE 1	10 DAYS	VSR EAST
PHASE 2	10 DAYS	VSR WEST
PHASE 3	10 DAYS	TAXIWAY A EAST APRON CONNECTOR
INT.	30 DAYS	OPEN - NO CONSTRUCTION
PHASE 4	2 DAYS (INT.)	TAXIWAY A EAST APRON CONNECTOR VSR SINGLE LANE CLOSURES

NOTE: PHASE 4 SHALL CONSIST OF FINAL MARKINGS ONLY WITH INTERMITTENT SINGLE LANE CLOSURES TO ALLOW PAINT CURING.



NO.	DATE	BY	REVISION DESCRIPTION

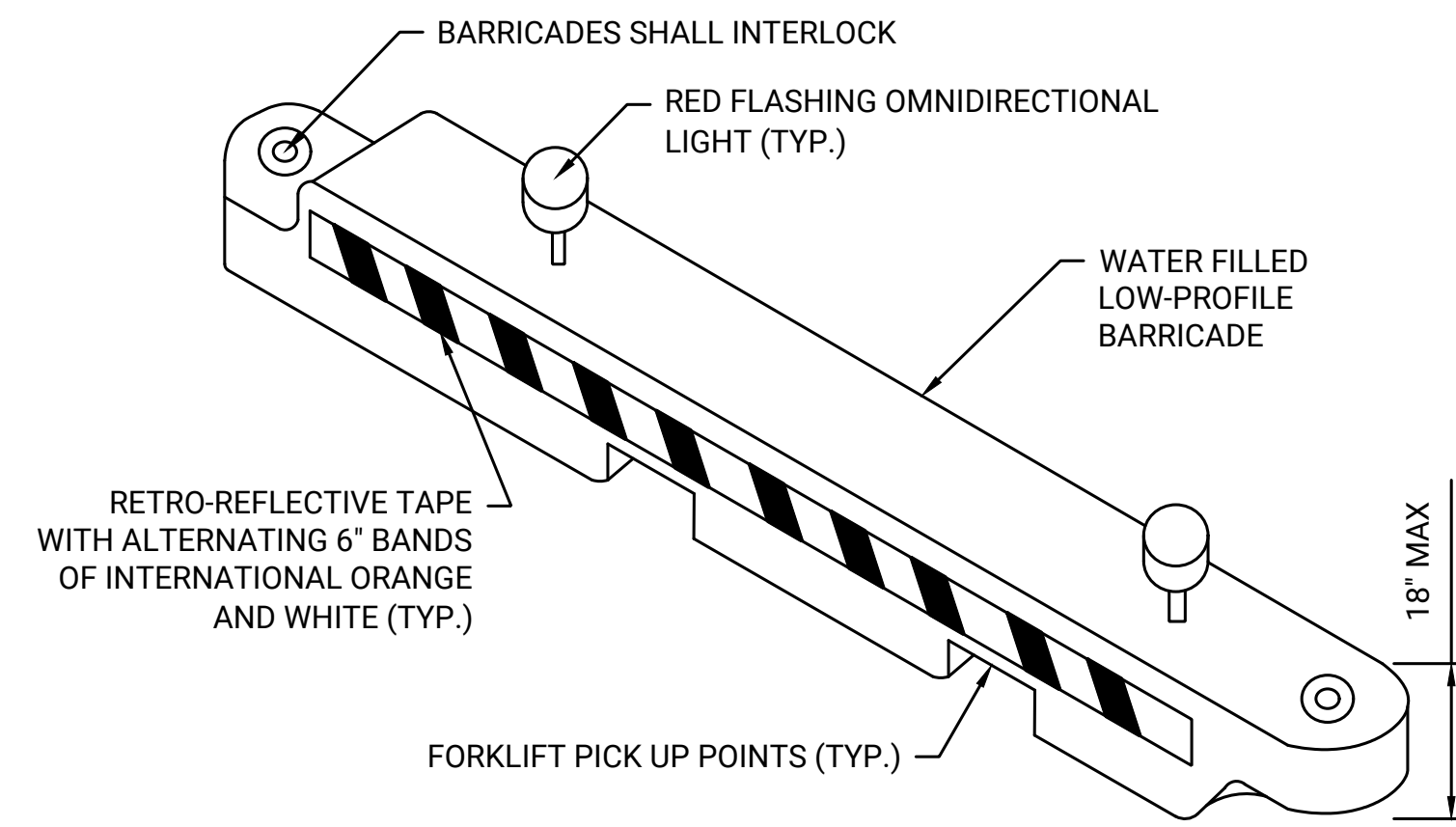
REHABILITATE VEHICLE SERVICE ROAD  
 MEMPHIS INTERNATIONAL AIRPORT  
 MEMPHIS, TN  
**CONSTRUCTION SAFETY & PHASING -  
 PHASE 4**



DESIGNED: WUH	DRAWN: WUH	CHECKED: JTM
PROJECT NO: 0024M300.10		
PRINT DATE: 4/7/2025		
SHEET NO.		

**GC2.04**

CAD FILE: C:\PW\WORKDIR\PW\LE\FV\W\H\H\082740\GC2.DWG - CSPP - DWG Layout - CSPP.rvt By: HALEY, WILLU

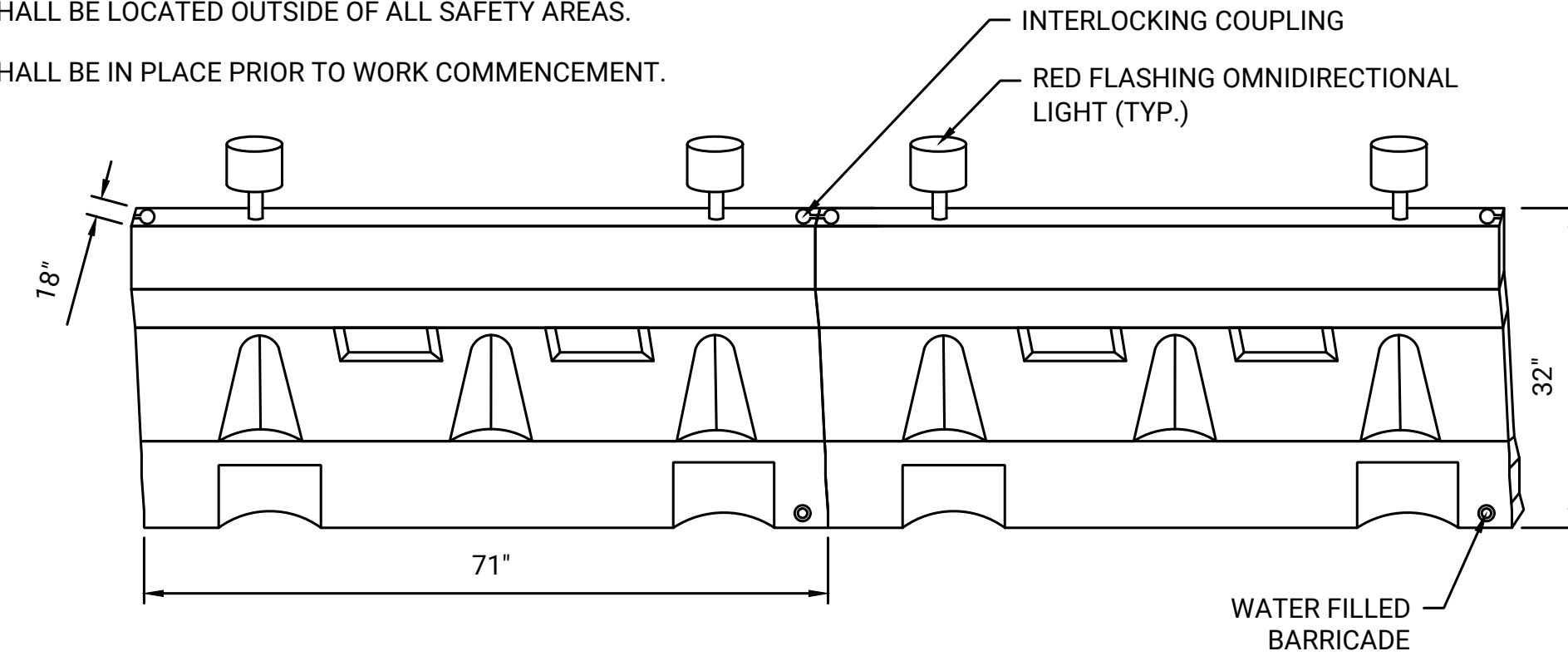


- NOTES:
- ENGINEER APPROVED LOW-PROFILE, LOW MASS IMPACT BARRICADES SHALL BE USED FOR AIRFIELD PAVEMENT CLOSURES ADJACENT TO OR INTERSECTING ACTIVE AIRCRAFT TRAVEL ROUTES.
  - LOW-PROFILE BARRICADES ARE TO BE PROVIDED BY THE CONTRACTOR AND INSTALLED AT ALL CLOSED TAXIWAY LOCATIONS AS SHOWN ON THE SAFETY AND PHASING PLANS OR AS DIRECTED BY THE MSCAA PROJECT ENGINEER.
  - THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE MSCAA PROJECT ENGINEER FOR THE FIELD PLACEMENT OF ALL BARRICADES. LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE.
  - IF THE CONTRACTOR DEVIATES FROM THE SAFETY AND PHASING PLAN, ADDITIONAL BARRICADES MAY BE NEEDED. THE CONTRACTOR SHALL GAIN APPROVAL FROM THE MSCAA PROJECT ENGINEER ON NEW BARRICADE LOCATIONS.
  - BARRICADES ARE TO BE INTERLOCKING AND CONTINUOUS IN ALL LOCATIONS EXCEPT FOR CONSTRUCTION PHASE ACCESS POINT. BARRICADE GAPS SHALL NOT EXCEED MINIMUM WIDTH TO GAIN VEHICLE OR EQUIPMENT ACCESS.
  - THE CONTRACTOR SHALL SECURE BARRICADES BY FILLING WITH WATER. LOW-PROFILE BARRICADES SHALL BE PROPERLY WEIGHTED TO PREVENT MOVEMENT BY WIND, PROP BLAST, OR JET BLAST OF UP TO 74 M.P.H.
  - THE CONTRACTOR SHALL INSPECT BARRICADE LOCATIONS AND LIGHTS DAILY. THE CONTRACTOR SHALL RELOCATE AND REPLACE LIGHTS AS NEEDED FOR PROPER PLACEMENT AND FUNCTION ACCORDING TO THE PROJECT PLANS AND SPECIFICATIONS.
  - BARRICADES SHALL BE LOCATED OUTSIDE OF THE SAFETY AREAS OF OPEN TAXIWAYS AND RUNWAYS.
  - BARRICADES SHALL BE IN PLACE PRIOR TO WORK COMMENCEMENT.

## 01 LOW PROFILE BARRICADE

SCALE: NONE

- NOTES:
- ENGINEER APPROVED MEDIUM PROFILE BARRICADES SHALL BE USED FOR AIRFIELD ROADWAY PAVEMENT CLOSURES.
  - MEDIUM PROFILE BARRICADES SHALL BE ORANGE OR WHITE.
  - MEDIUM PROFILE BARRICADES ARE TO BE PROVIDED BY THE CONTRACTOR AND INSTALLED AT LOCATIONS AS SHOWN ON THE SAFETY AND PHASING PLANS OR AS DIRECTED BY THE MSCAA PROJECT ENGINEER.
  - THE CONTRACTOR SHALL OBTAIN APPROVAL FROM THE MSCAA PROJECT ENGINEER FOR THE FIELD PLACEMENT OF ALL BARRICADES. LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE.
  - IF THE CONTRACTOR DEVIATES FROM THE SAFETY AND PHASING PLAN, ADDITIONAL BARRICADES MAY BE NEEDED. THE CONTRACTOR SHALL GAIN APPROVAL FROM THE MSCAA PROJECT ENGINEER ON NEW BARRICADE LOCATIONS.
  - BARRICADES ARE TO BE INTERLOCKING AND CONTINUOUS IN ALL LOCATIONS EXCEPT FOR CONSTRUCTION PHASE ACCESS POINT. BARRICADE GAPS SHALL NOT EXCEED MINIMUM WIDTH TO GAIN VEHICLE OR EQUIPMENT ACCESS.
  - THE CONTRACTOR SHALL SECURE BARRICADES BY FILLING WITH WATER. MEDIUM PROFILE BARRICADES SHALL BE PROPERLY WEIGHTED TO PREVENT MOVEMENT BY WIND, PROP BLAST, OR JET BLAST OF UP TO 74 M.P.H.
  - THE CONTRACTOR SHALL INSPECT BARRICADE LOCATIONS AND LIGHTS DAILY. THE CONTRACTOR SHALL RELOCATE AND REPLACE LIGHTS AS NEEDED FOR PROPER PLACEMENT AND FUNCTION ACCORDING TO THE PROJECT PLANS AND SPECIFICATIONS.
  - BARRICADES SHALL BE LOCATED OUTSIDE OF ALL SAFETY AREAS.
  - BARRICADES SHALL BE IN PLACE PRIOR TO WORK COMMENCEMENT.



## 02 MEDIUM PROFILE BARRICADE

SCALE: NONE

PREPARED FOR:



PREPARED BY:

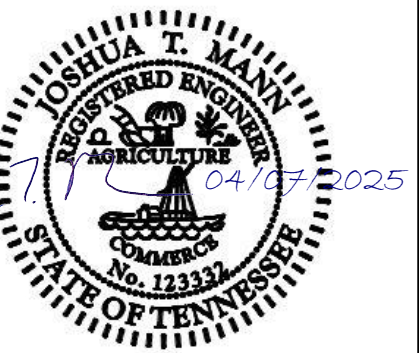


KEY MAP:

NO	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

CONSTRUCTION SAFETY & PHASING -  
DETAILS



DESIGNED: WUH    DRAWN: WUH    CHECKED: JTM

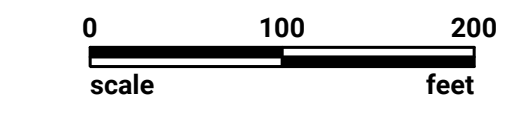
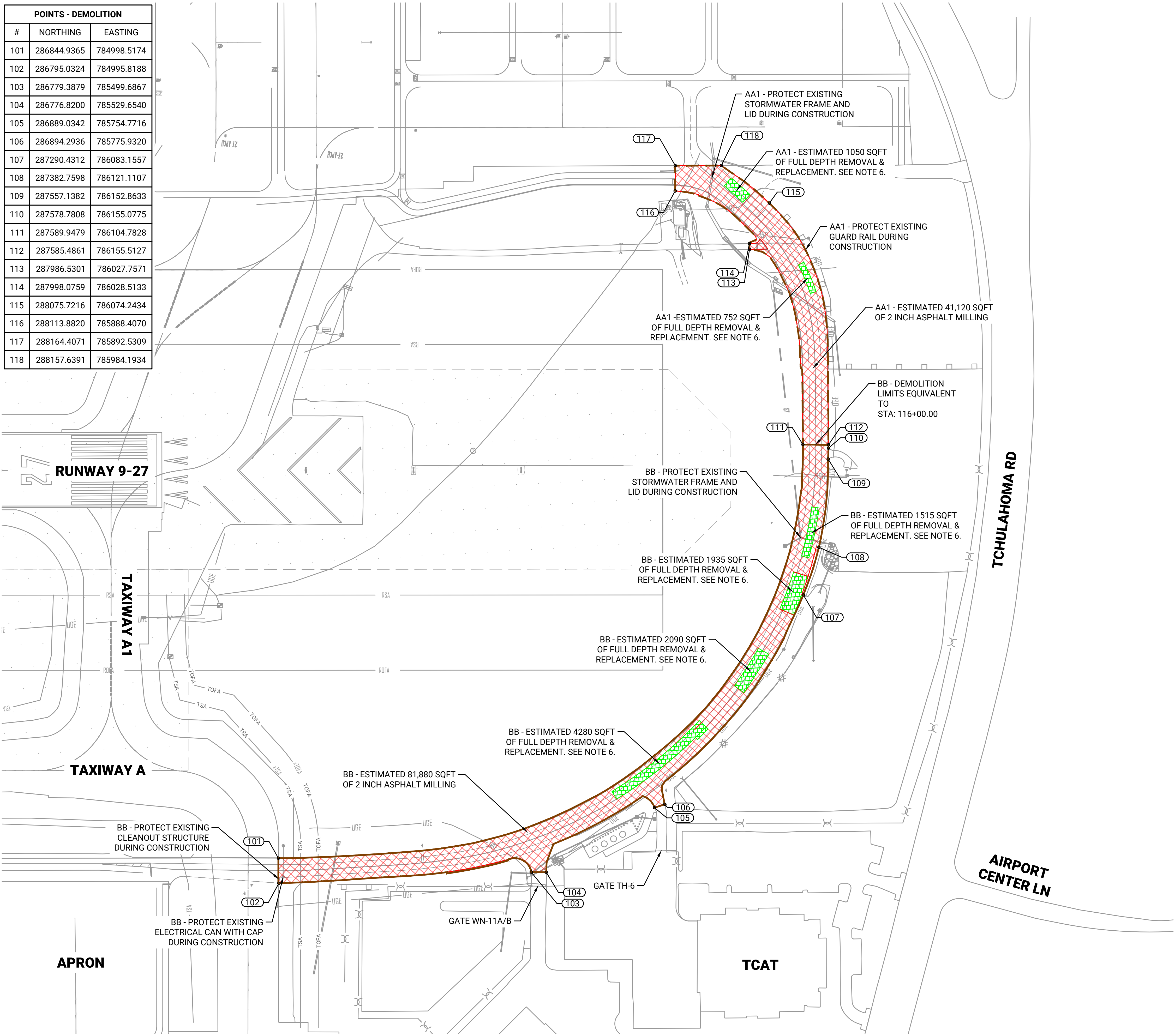
PROJECT NO: 0024M300.10

PRINT DATE: 4/7/2025

SHEET NO.

**GC3.01**

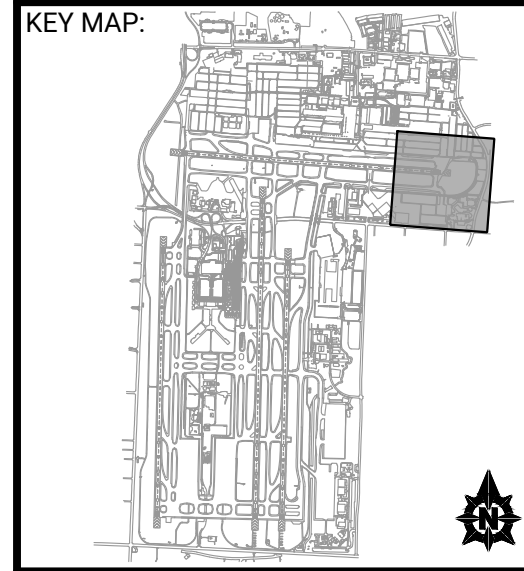
POINTS - DEMOLITION		
#	NORTHING	EASTING
101	286844.9365	784998.5174
102	286795.0324	784995.8188
103	286779.3879	785499.6867
104	286776.8200	785529.6540
105	286889.0342	785754.7716
106	286894.2936	785775.9320
107	287290.4312	786083.1557
108	287382.7598	786121.1107
109	287557.1382	786152.8633
110	287578.7808	786155.0775
111	287589.9479	786104.7828
112	287585.4861	786155.5127
113	287986.5301	786027.7571
114	287998.0759	786028.5133
115	288075.7216	786074.2434
116	288113.8820	785888.4070
117	288164.4071	785892.5309
118	288157.6391	785984.1934



LEGEND		
EXISTING	PROPOSED	DESCRIPTION
N/A	[Red hatched box]	DEMOLITION LIMIT (BASE BID)
N/A	[Dashed red line]	DEMOLITION LIMIT (ADD. ALT. 1)
N/A	[Red cross-hatched box]	PAVEMENT DEMOLITION (2" MILL)
N/A	[Green grid box]	PAVEMENT DEMOLITION (FULL DEPTH)
— RSA —	N/A	RUNWAY SAFETY AREA
— ROFA —	N/A	RUNWAY OBJECT FREE AREA
— TSA —	N/A	TAXIWAY SAFETY AREA
— TOFA —	N/A	TAXIWAY OBJECT FREE AREA
[Dotted pattern]	N/A	LOC/ILS CRITICAL AREA
[Circle with cross]	N/A	STORM SEWER
— LUCE —	N/A	TAXIWAY OBJECT FREE AREA
— X —	N/A	AIRFIELD FENCE
[Double line]	N/A	PAVEMENT MARKINGS

NOTES:

- CONTRACTOR SHALL PROTECT SURVEY MONUMENTS, REFERENCE POINTS, AND BENCHMARKS DURING CONSTRUCTION.
- THE CONTRACTOR SHALL LOCATE ALL EXISTING UNDERGROUND UTILITIES AND SATISFY THEMSELVES OF THEIR LOCATIONS PRIOR TO BEGINNING DEMOLITION. ANY DAMAGE TO THE EXISTING UTILITIES THAT ARE NOT TO BE REMOVED SHALL BE CONSIDERED THE RESPONSIBILITY OF THE CONTRACTOR. THEY SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTORS EXPENSE. REPAIR METHODS SHALL BE APPROVED BY THE MSCAA PROJECT ENGINEER.
- CONTRACTOR SHALL VISIT THE JOB SITE AND COMPLETELY FAMILIARIZE THEMSELVES WITH THE EXISTING CONDITIONS AND SCOPE OF DEMOLITION PRIOR TO BID. CERTAIN MISCELLANEOUS SUBSIDIARY FEATURES ARE NOT SHOWN ON THE PLANS FOR CLARITY. DEMOLITION OF ALL ITEMS NOT SHOWN, BUT MADE VOID BY THE SCOPE OF WORK, SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- ALL REMOVED ITEMS NOT CLAIMED BY THE MSCAA SHALL BECOME PROPERTY OF THE CONTRACTOR AND DISPOSED OF OFF AIRPORT PROPERTY.
- WHERE PAVEMENT DEMOLITION ABUTS EXISTING PAVEMENT, CONTRACTOR SHALL SAW CUT A CLEAN, NEAT, AND STRAIGHT EDGE. CONTRACTOR WILL CORRECT OVERSAW WITH SQUARE PATCHES TO THE SATISFACTION OF THE MSCAA PROJECT ENGINEER.
- THE CONTRACTOR SHALL VERIFY FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT LIMITS WITH THE MSCAA PROJECT ENGINEER. ANY FULL DEPTH REMOVAL AND REPLACEMENT REQUIRED BEYOND THE SHOWN QUANTITY SHALL NOT BE COMPLETED WITHOUT APPROVAL FROM THE MSCAA PROJECT ENGINEER.



REVISION NO.	DESCRIPTION

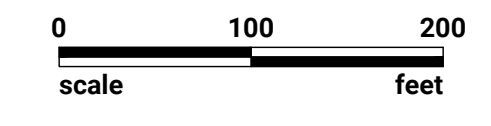
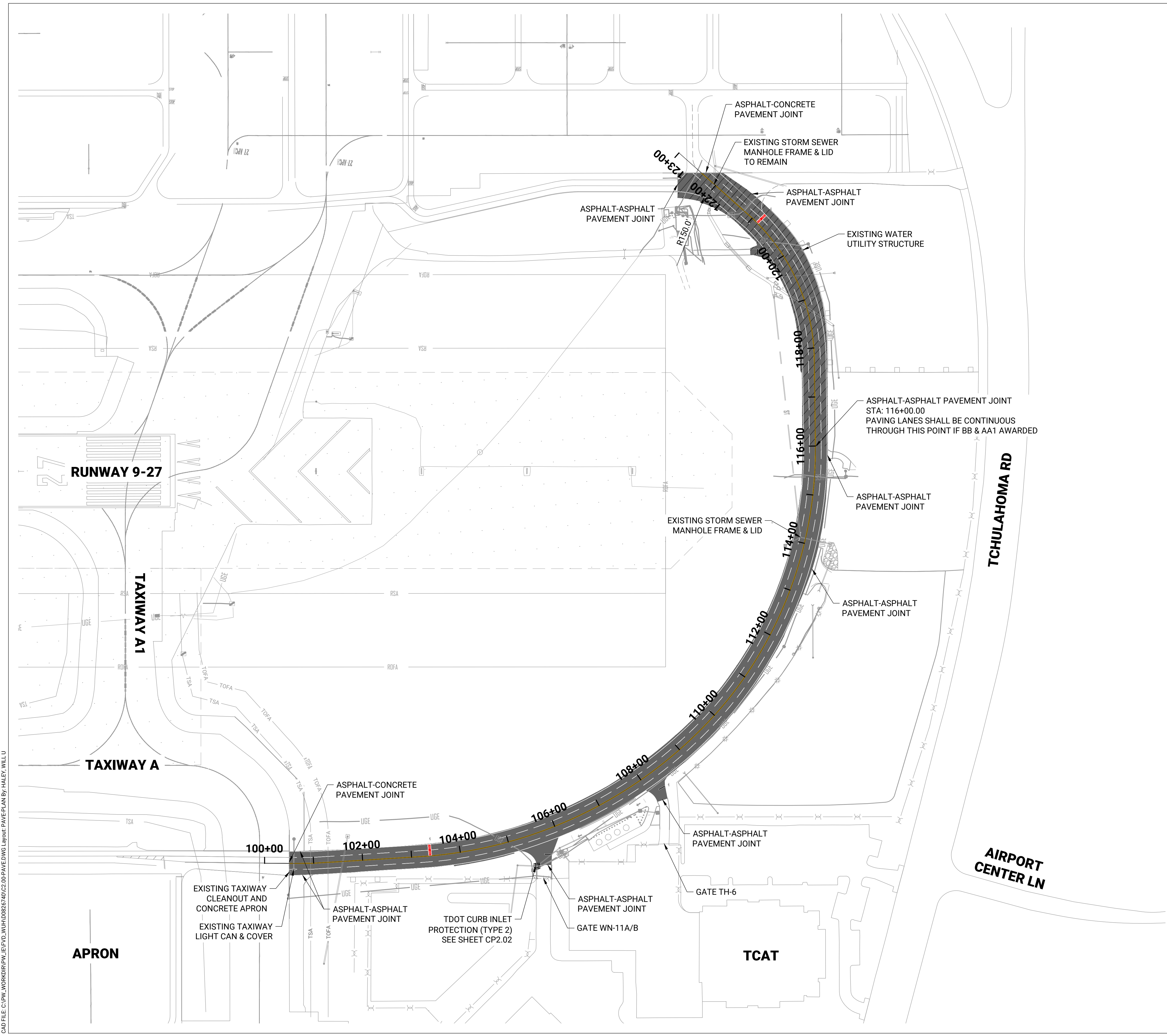
REHABILITATE VEHICLE SERVICE ROAD  
 MEMPHIS INTERNATIONAL AIRPORT  
 MEMPHIS, TN

DEMOLITION PLAN



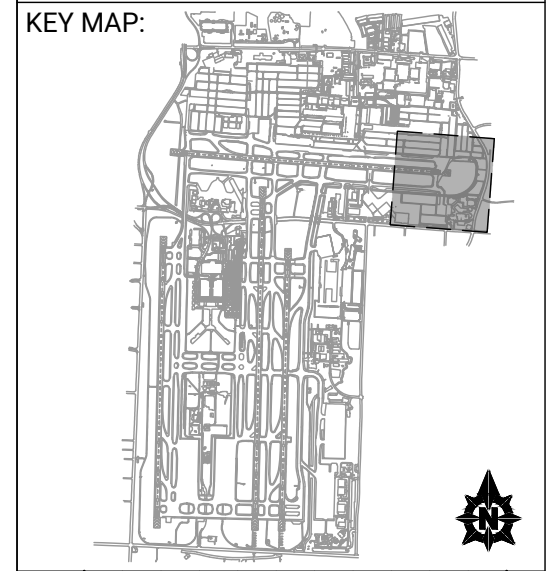
DESIGNED: WUH	DRAWN: WUH	CHECKED: JTM
PROJECT NO: 0024M300.10		
PRINT DATE: 4/7/2025		
SHEET NO.		

**CD1.01**



LEGEND		
EXISTING	PROPOSED	
[Symbol]	[Symbol]	PAVEMENT - ASPHALT (BASE BID)
[Symbol]	[Symbol]	PAVEMENT - ASPHALT (AA1)
[Symbol]	N/A	RUNWAY SAFETY AREA
[Symbol]	N/A	RUNWAY OBJECT FREE AREA
[Symbol]	N/A	TAXIWAY SAFETY AREA
[Symbol]	N/A	TAXIWAY OBJECT FREE AREA
[Symbol]	N/A	LOC/ILS CRITICAL AREA
[Symbol]	N/A	SANITARY SEWER
[Symbol]	N/A	STORM SEWER
[Symbol]	N/A	WATER
[Symbol]	N/A	ELECTRICAL - AIRFIELD CIRCUIT
[Symbol]	N/A	ELECTRICAL - UNDERGROUND CABLE
[Symbol]	N/A	AIRFIELD FENCE
[Symbol]	[Symbol]	PAVEMENT MARKINGS

- NOTES:
- CONTRACTOR SHALL PRESERVE EXISTING UTILITIES ELEVATION, CASING, CONDUIT, AND CRIBBING/BRACING AT ANY LOCATION WHERE EXISTING UTILITIES CONFLICT WITH PROJECT WORK. ANY DAMAGE TO THE EXISTING UTILITIES THAT ARE NOT TO BE REMOVED SHALL BE CONSIDERED THE RESPONSIBILITY OF THE CONTRACTOR. THEY SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTORS EXPENSE. REPAIR METHODS SHALL BE APPROVED BY THE MSCAA PROJECT ENGINEER.
  - ASPHALT SURFACE COURSE SHALL BE A CONTINUOUS LIFT WITHIN EACH PHASE. WHERE TO EXISTING ASPHALT MEETS PROPOSED ASPHALT CONTRACTOR SHALL SAWCUT A CLEAN VERTICAL EDGE AND INSTALL ASPHALT COLD JOINT. ASPHALT JOINTS AT PHASE LIMITS INCLUDED.
  - PROPOSED ROAD EDGES SHALL MATCH EXISTING GEOMETRY.
  - SEE SHEET CD1.01 FOR FULL DEPTH DEMOLITION INFORMATION.
  - PROPOSED ASPHALT PAVEMENT SHALL INCLUDE REFLECTIVE MARKERS. SEE SHEET CM2.01 FOR DETAILS.



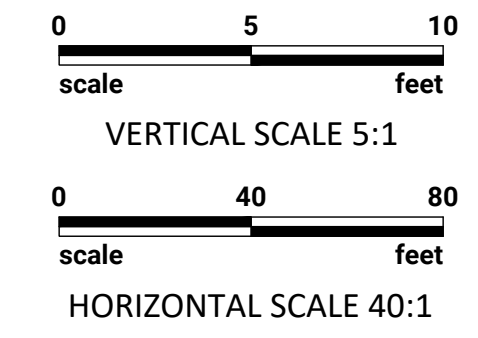
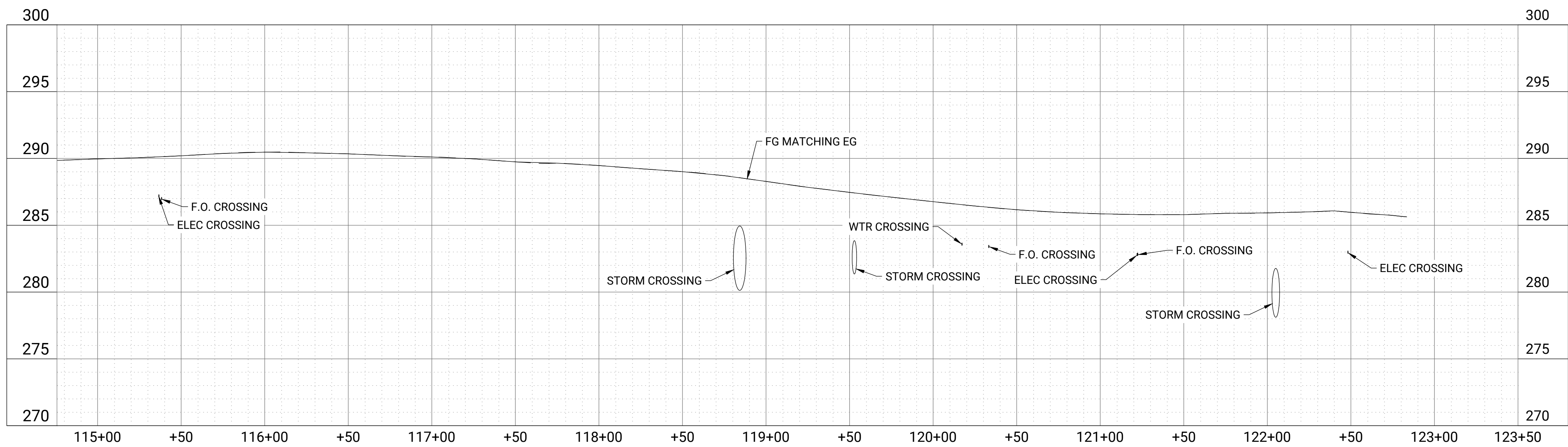
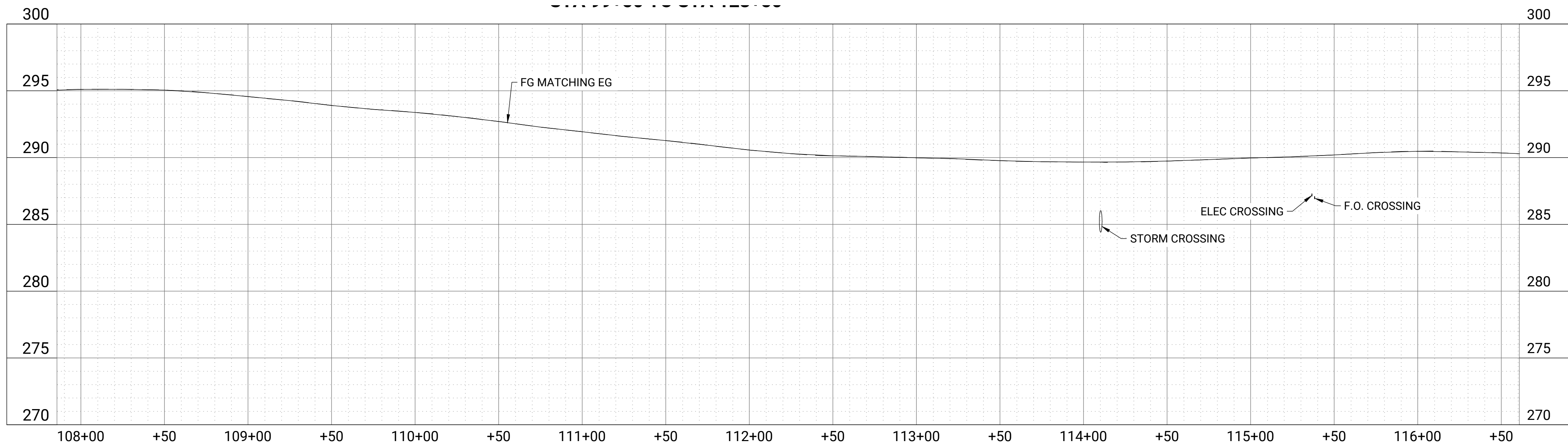
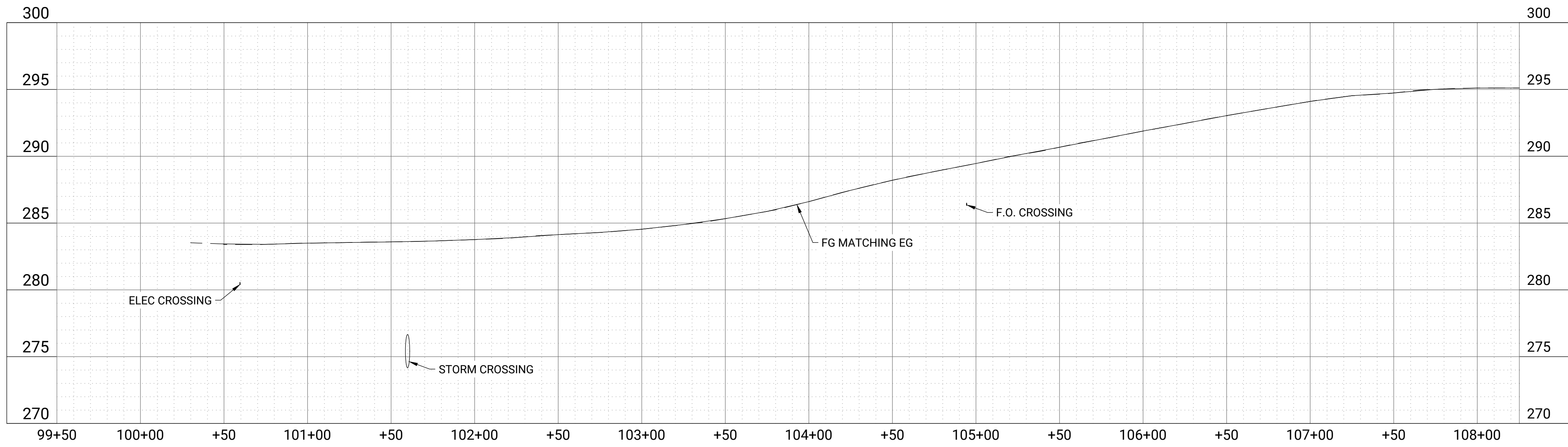
NO	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
 MEMPHIS INTERNATIONAL AIRPORT  
 MEMPHIS, TN  
**PAVING PLAN**



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CAD FILE C:\PW\WORKDIR\PW\LEF\VD\WUH\082740\C2.00\PAVEDWG Layout.PAVEPRP.T By: HALEY, WILLU



PREPARED FOR:



PREPARED BY:



KEY MAP:

NO	DATE	BY	REVISION DESCRIPTION

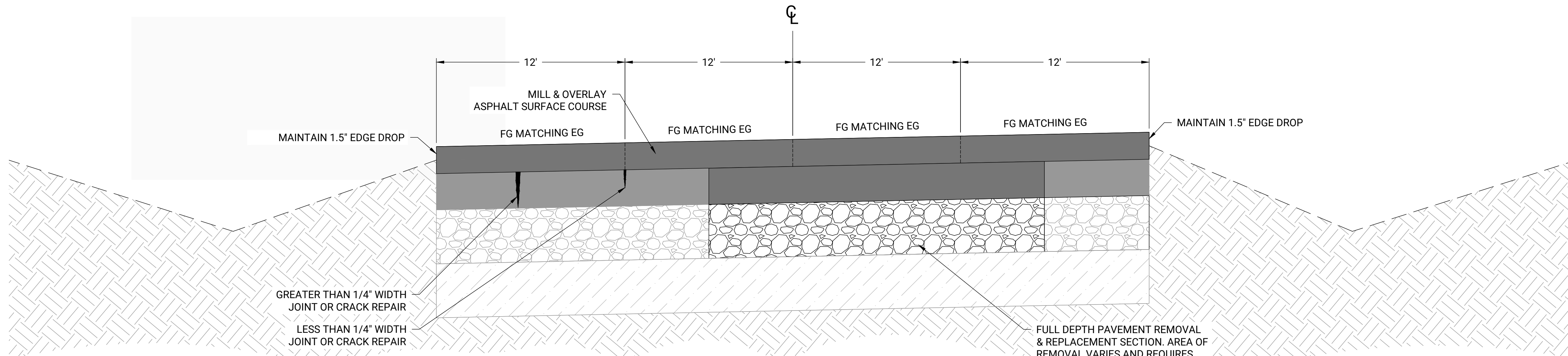
REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

PAVING PLAN & PROFILE



DESIGNED: WUH    DRAWN: WUH    CHECKED: JTM  
 PROJECT NO: 0024M300.10  
 PRINT DATE: 4/7/2025  
 SHEET NO.

CP1.02

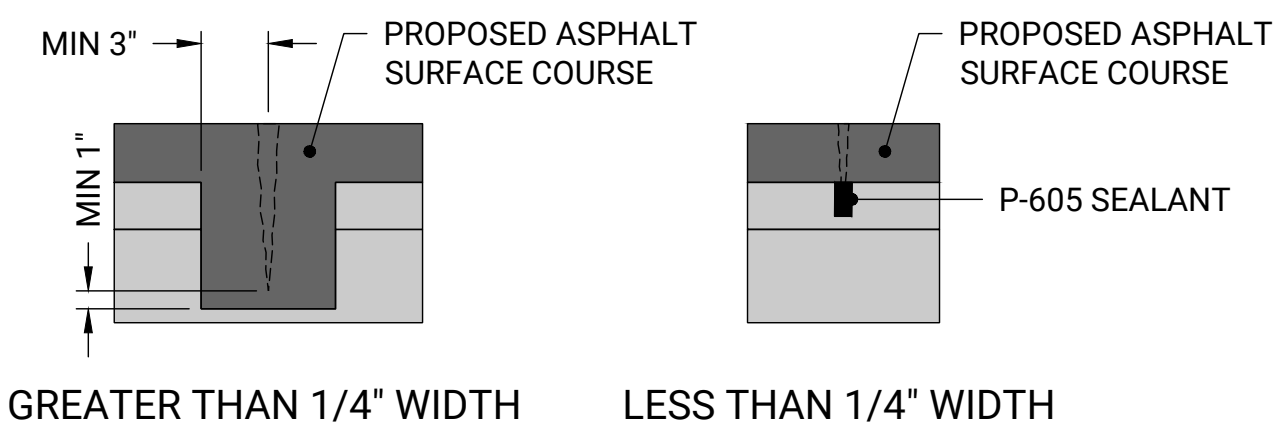


NOTES:

1. OUTER DRIVE LANE WIDTHS VARY DUE TO EXISTING PAVEMENT GEOMETRY. OUTER LANE MARKING WIDTHS MUST REMAIN WITHIN 9'-12' WIDE IN ACCORDANCE WITH TDOT STANDARD DRAWING RD11-TS-1.
2. ASPHALT SURFACE COURSE SHALL BE PAVED IN ONE CONTINUOUS LIFT FOR EACH LANE WITHIN PHASE LIMITS, INCLUDING WHERE FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT IS REQUIRED.
3. FINISHED GRADES SHALL MATCH EXISTING GRADES AT ALL LOCATIONS.

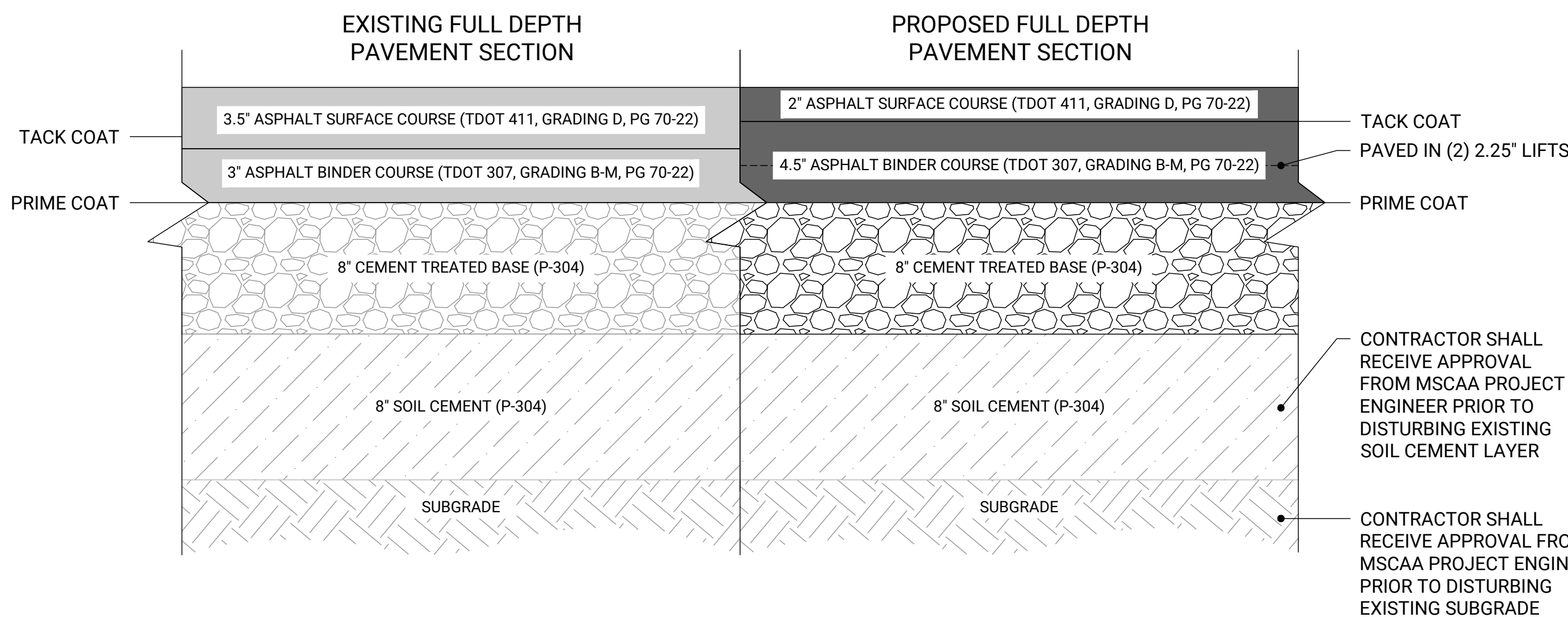
### 01 VSR TYPICAL SECTION

SCALE: NONE



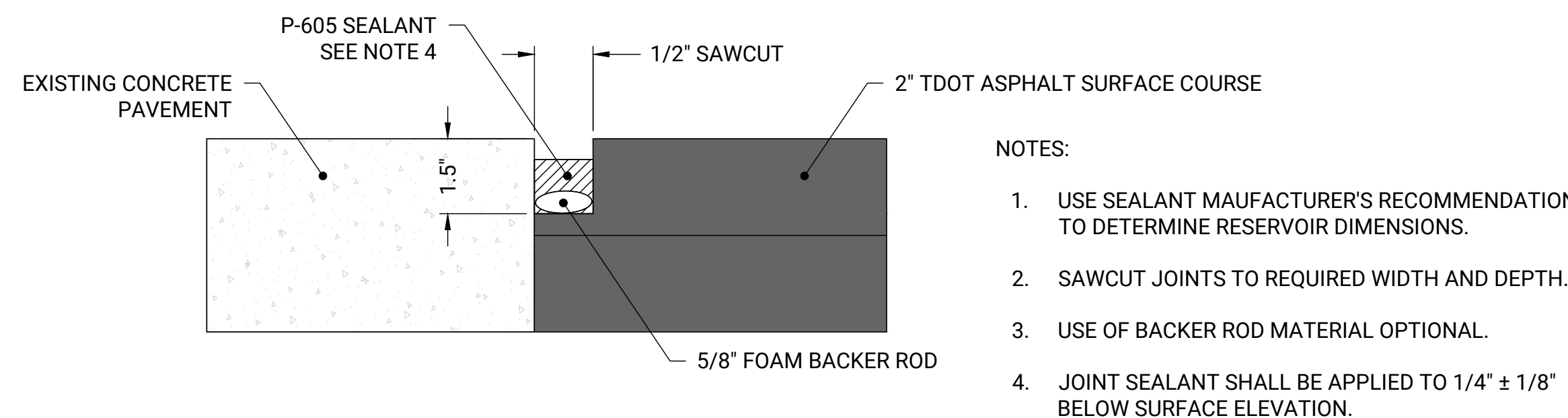
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SCALE: NONE



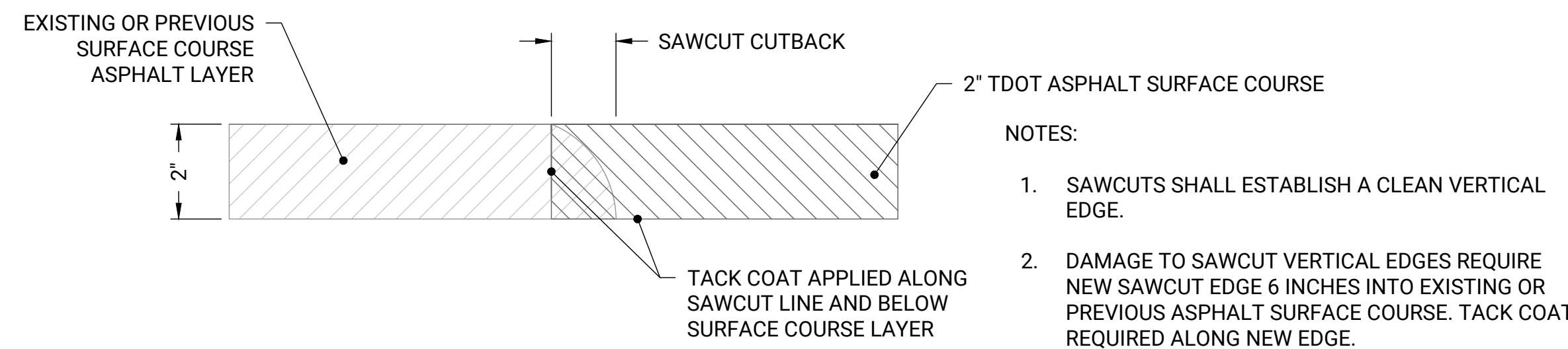
### 03 FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT SECTION

SCALE: NONE



### 04 ASPHALT-CONCRETE PAVEMENT JOINT

SCALE: NONE



### 05 ASPHALT-ASPHALT PAVEMENT JOINT

SCALE: NONE

PREPARED FOR:

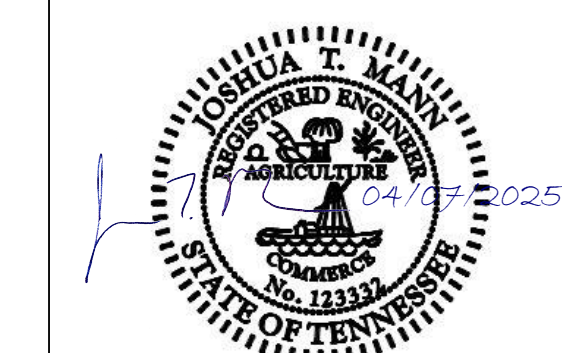


KEY MAP:

NO	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

PAVING DETAILS



DESIGNED: WUH DRAWN: WUH CHECKED: JTM

PROJECT NO: 0024M300.10

PRINT DATE: 4/7/2025

SHEET NO.

**CP2.01**

PREPARED FOR:



PREPARED BY:



KEY MAP:

NO	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

EROSION CONTROL DETAILS

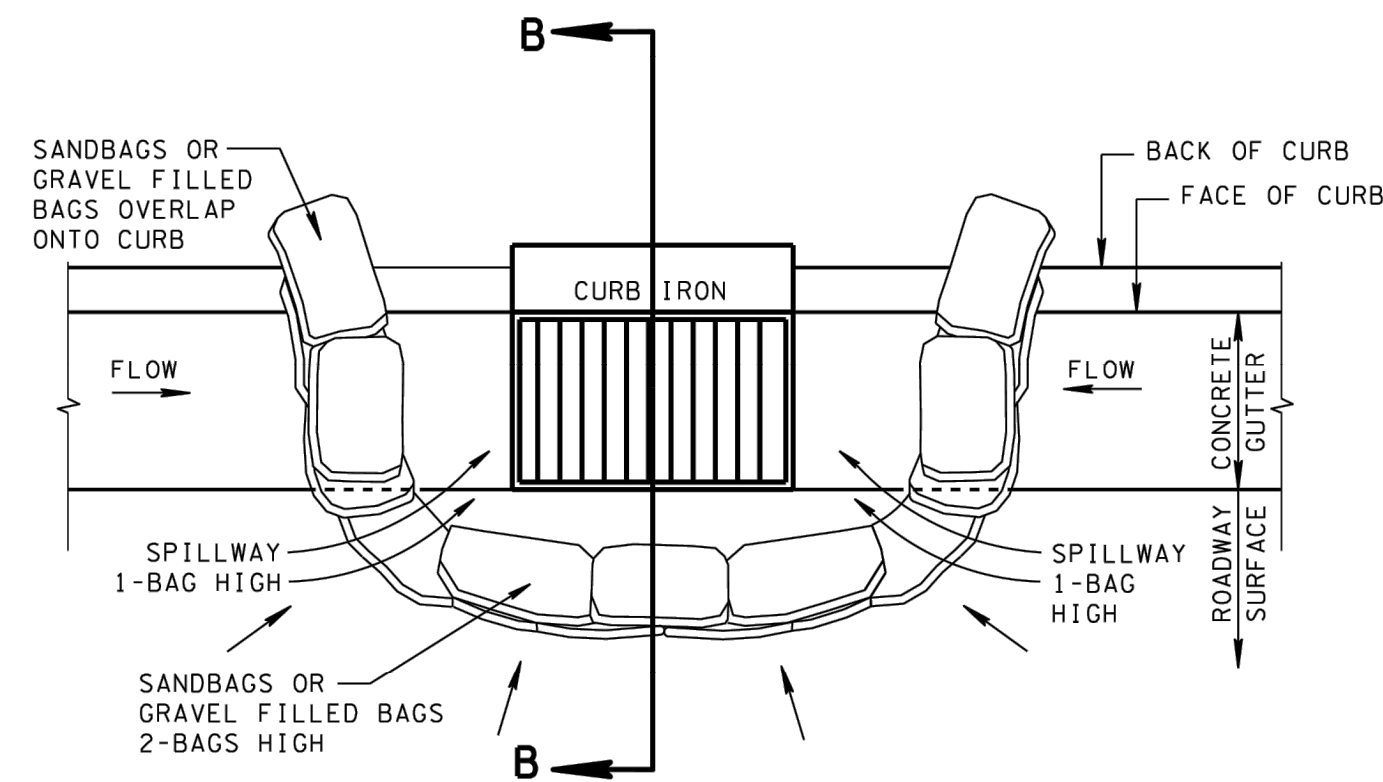


DESIGNED: WUH    DRAWN: WUH    CHECKED: JTM  
 PROJECT NO: 0024M300.10  
 PRINT DATE: 4/7/2025  
 SHEET NO.

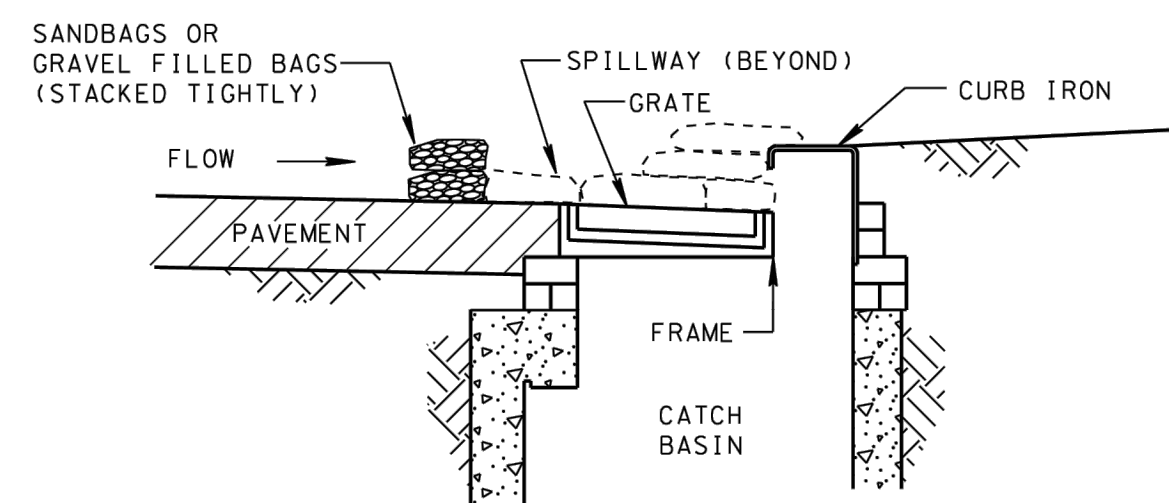
CP2.02

### CURB INLET PROTECTION TYPE 2

LOW VOLUME, LOW SPEED TRAFFIC AREAS ONLY



PLAN VIEW - INLET AT SAG



SECTION B-B

TDOT STANDARD DRAWING: EC-STR-39

NOTE: THIS DETAIL IS TO BE UTILIZED FOR CURB INLETS.

## 01 TDOT CURB INLET & CULVERT PROTECTION (TYPE 2)

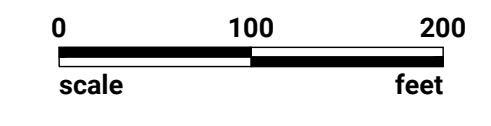
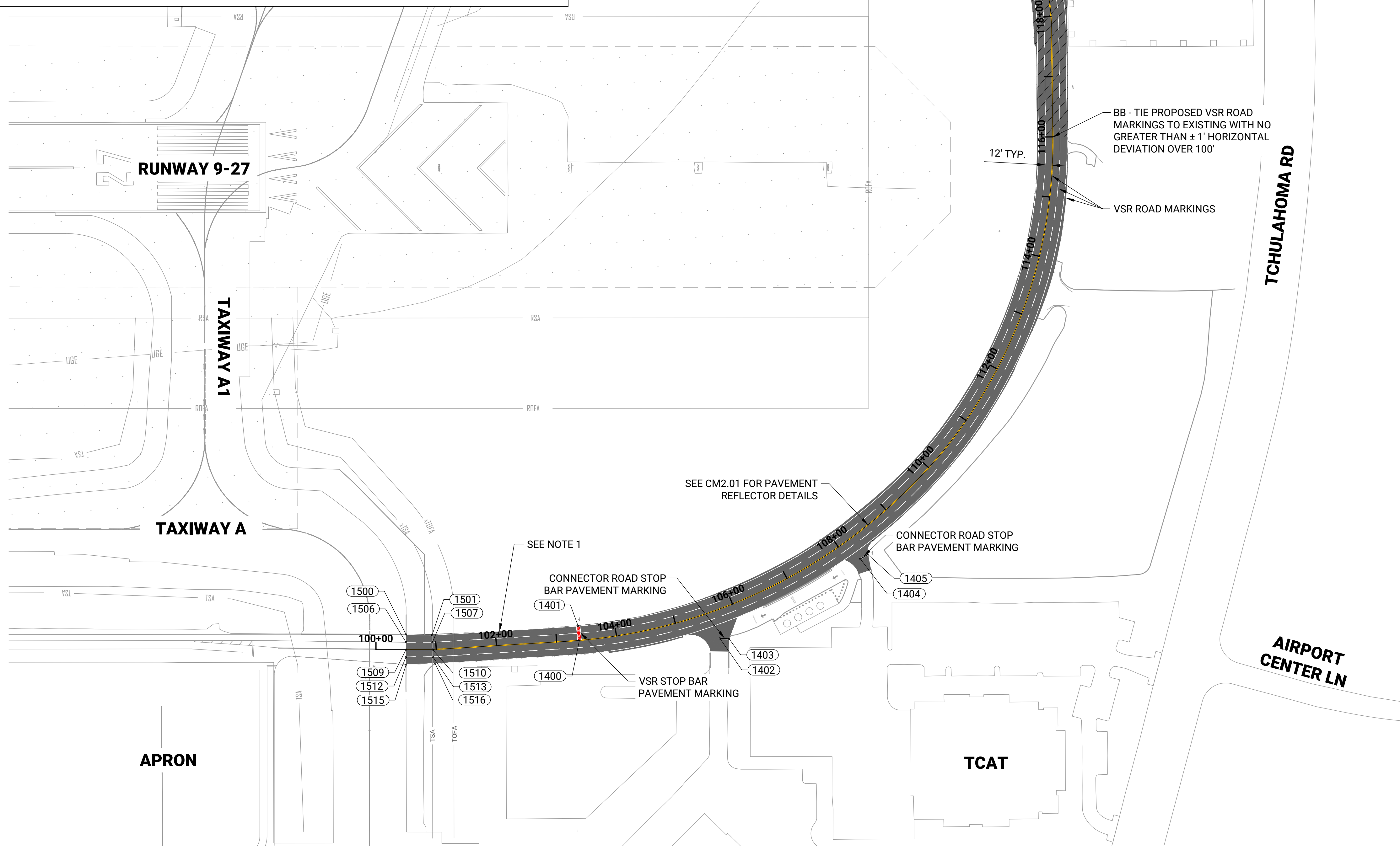
SCALE: NONE

### CURB INLET PROTECTION TYPE 2 GENERAL NOTES

- (B1) CURB INLET PROTECTION (TYPE 2) IS USED TO INTERCEPT SEDIMENT AND PREVENT SEDIMENT LADEN WATER FROM ENTERING STORM SEWER SYSTEMS. THIS DEVICE IS INTENDED AS A SECONDARY SEDIMENT CONTROL MEASURE. CURB INLET PROTECTION (TYPE 2) IS USED IN AREAS WHERE PONDING IS NOT A CONCERN AND ADEQUATE AREA IS AVAILABLE FOR PONDING.
- (B2) MAXIMUM DRAINAGE AREA IS 1 ACRE.
- (B3) MAXIMUM TOP OF SPILLWAY ELEVATION = TOP OF CURB ELEVATION MINUS 1 INCH.
- (B4) BAGS SHALL BE MADE OF EITHER BURLAP OR GEOTEXTILE FABRIC AND FILLED WITH CLEAN MINERAL AGGREGATE (SIZE 57) OR SAND.
- (B5) PACK SAND/GRAVEL FILLED BAGS TIGHTLY TOGETHER END TO END TO ENSURE NO SEDIMENT FLOWS BETWEEN OR UNDERNEATH THE BAGS. WHERE TIGHT FIT IS UNACHIEVABLE, INSTALL GEOTEXTILE FABRIC (TYPE III) ALONG THE UPSTREAM FACE OF THE BAGS LAPPING OVER THE TOP BAGS 6 INCHES AND EXTENDING GEOTEXTILE FABRIC (TYPE III) A MINIMUM OF 18 INCHES UPSTREAM OF THE BAGS. COVER GEOTEXTILE FABRIC (TYPE III) WITH MINERAL AGGREGATE (SIZE 57) STONE WEDGE TO THE TOP OF THE BAGS.
- (B6) ONLY GEOTEXTILE FABRIC (TYPE III) LISTED ON THE QUALIFIED PRODUCTS LIST SHALL BE USED.
- (B7) AN OVERFLOW SPILLWAY SHALL BE PROVIDED BY LEAVING AN OPENING OF ONE SAND OR GRAVEL BAG WIDE AND HIGH AS SHOWN. STORMS GREATER THAN 2-YEAR, 24 HOUR STORM SHOULD NOT OVERTOP THE CURB.
- (B8) CURB INLET PROTECTION (TYPE 2) SHALL BE PAID FOR UNDER THE FOLLOWING ITEM NUMBER:  
  
209-09.41 CURB INLET PROTECTION (TYPE 2) PER EACH  
  
PAYMENT SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR CONSTRUCTION, MAINTENANCE, AND REMOVAL OF CURB INLET PROTECTION (TYPE 2).
- (B9) ANY PRODUCT LISTED ON THE QUALIFIED PRODUCTS LIST AS AN APPROVED ALTERNATE IS ALSO ACCEPTABLE.
- (B10) MAINTENANCE SHALL BE PERFORMED AS NEEDED. FOR PROPER FUNCTION SEDIMENT REMOVAL SHALL BE PERFORMED CONTINUOUSLY AND/OR AFTER EVERY RAIN EVENT AND PAID FOR UNDER ITEM NUMBER 209-05, SEDIMENT REMOVAL, PER CUBIC YARD.

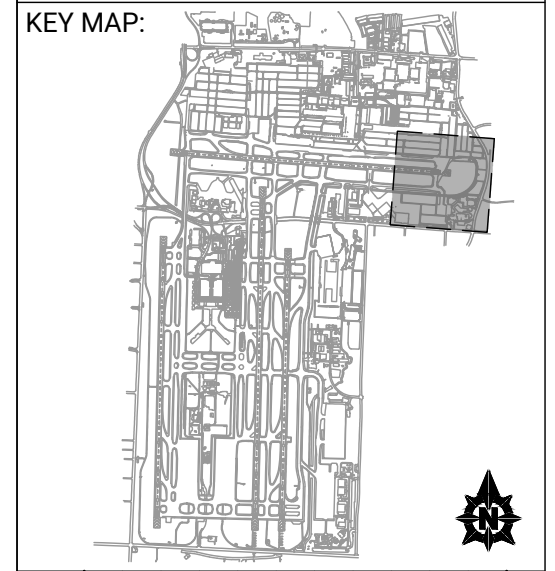
POINTS - MARKING			
#	NORTHING	EASTING	DESCRIPTION
1400	286814.3614	785284.6666	VSR STOP
1401	286837.1104	785284.3566	VSR STOP
1402	286799.3940	785517.0736	CNT STOP
1403	286797.6974	785532.3181	CNT STOP
1404	286913.5133	785759.3443	CNT STOP
1405	286919.7074	785772.6714	CNT STOP
1406	287985.6107	786042.5272	CNT STOP
1407	287998.1072	786043.0203	CNT STOP
1408	288051.2605	786046.8999	VSR STOP
1409	288065.8488	786064.3589	VSR STOP
1410	288153.8066	785891.7664	NON MOVE
1411	287970.4163	785872.5994	VSR STOP
1500	286844.9365	784998.5174	EDGE WEST
1501	286840.7221	785041.5462	EDGE WEST
1502	288085.1778	785979.4389	EDGE WEST
1503	288126.2684	785889.7755	EDGE WEST

POINTS - MARKING			
#	NORTHING	EASTING	DESCRIPTION
1504	288151.4631	785891.5974	EDGE WEST
1505	288163.1468	785907.9215	EDGE WEST
1506	286832.0798	784997.5072	LN WEST
1507	286828.7298	785040.9971	LN WEST
1508	288162.0637	785922.8505	LN WEST
1509	286820.0092	784996.6110	CNT
1510	286816.7360	785040.4670	CNT
1511	288160.8216	785940.7232	CNT
1512	286808.0425	784995.7173	LN EAST
1513	286804.7422	785039.9369	LN EAST
1514	288159.4533	785958.7176	LN EAST
1515	286795.1045	784994.7074	EDGE EAST
1516	286793.2190	785042.9400	EDGE EAST
1517	288072.5729	786058.9799	EDGE EAST
1518	288091.3055	786059.8303	EDGE EAST
1519	288158.1490	785977.1175	EDGE EAST



LEGEND		
EXISTING	PROPOSED	
[White box]	[Dark grey box]	PAVEMENT - ASPHALT (BASE BID)
[White box]	[Hatched box]	PAVEMENT - ASPHALT (ADD. ALT. 1)
[Patterned box]	[White box]	N/A RIPRAP
[Dashed line]	[White box]	N/A RUNWAY SAFETY AREA
[Dashed line]	[White box]	N/A RUNWAY OBJECT FREE AREA
[Dashed line]	[White box]	N/A TAXIWAY SAFETY AREA
[Dashed line]	[White box]	N/A TAXIWAY OBJECT FREE AREA
[Dashed line]	[White box]	N/A LOC/ILS CRITICAL AREA
[Line with dashes]	[Line with dashes]	PAVEMENT MARKINGS

- NOTES:
- VSR OUTER LANE WIDTH SHALL BE STANDARD 12' WIDE, AND A MINIMUM OF 9' WIDE WHERE EXISTING PAVEMENT WIDTH VARIES.
  - MARKINGS NORTH OF STATION 116+00.00 SHALL ONLY BE INSTALLED UPON AWARD OF ADDITIVE ALTERNATE 1.



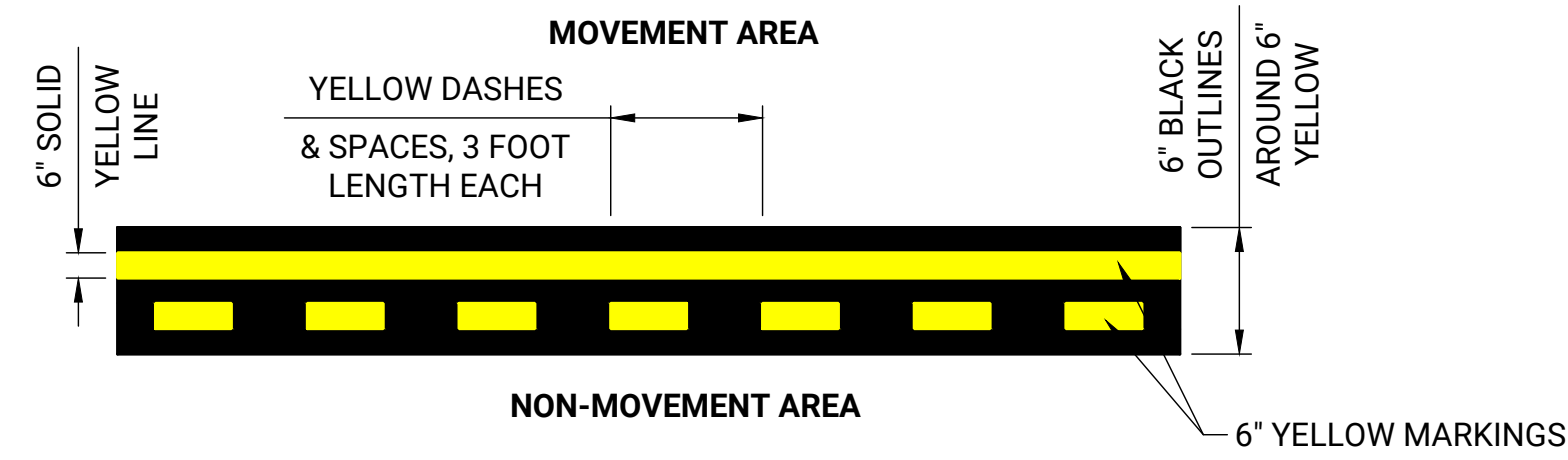
NO	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
 MEMPHIS INTERNATIONAL AIRPORT  
 MEMPHIS, TN

PAVEMENT MARKING PLAN

CAD FILE: C:\PW\WORKDIR\PW\LE\FVD\_WUH\024M300.10\CM1.00-MARKING.DWG Layout: MARKPLAN BY: HALEY, WILLU

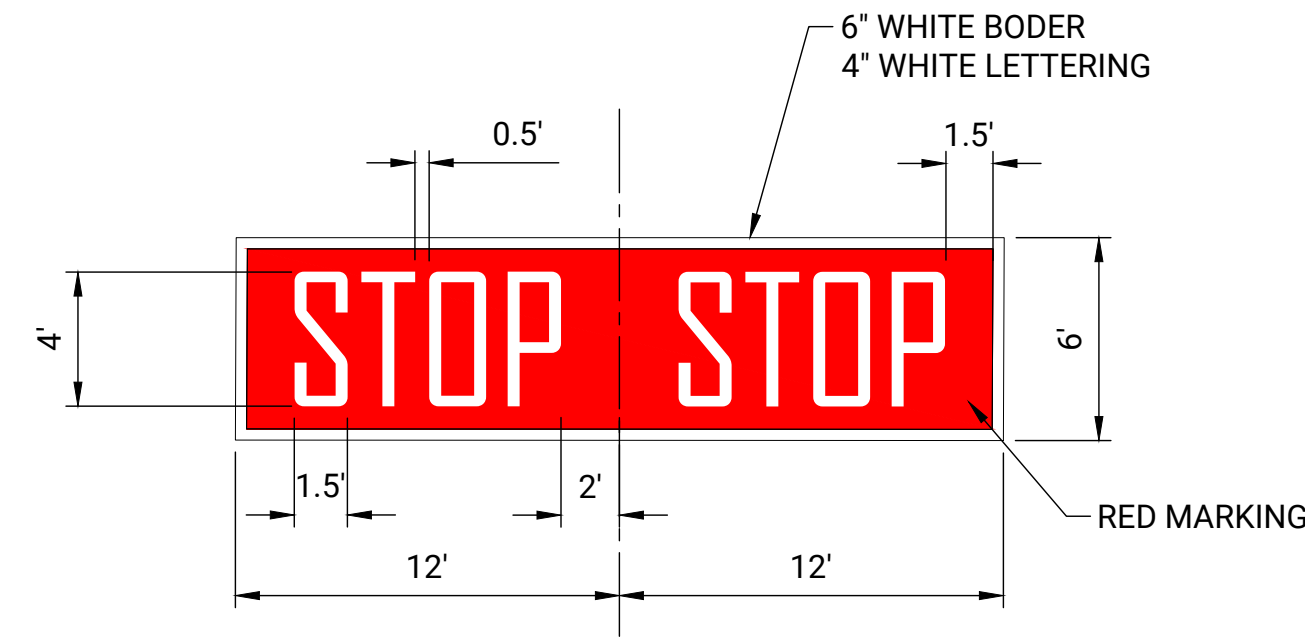




- NOTES:
1. ALL MARKINGS SHALL BE MEASURED AND PAID UNDER SPECIFICATION P-620.
  2. NON-MOVEMENT MARKINGS SHALL BE APPLIED AS PERMANENT MARKINGS ONLY.
  3. PERMANENT MARKINGS REQUIRE GLASS BEADS.

## 01 NON-MOVEMENT MARKING (AA1)

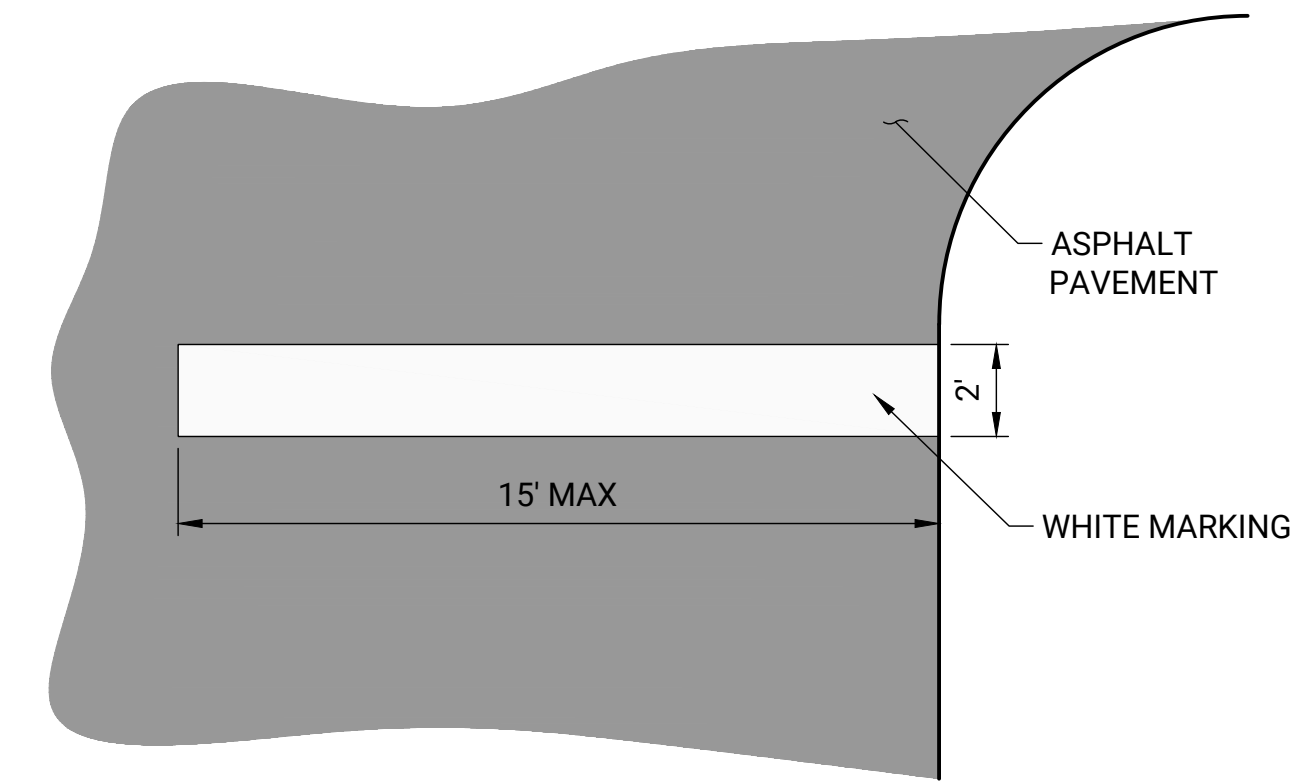
SCALE: NONE



- NOTES:
1. ALL MARKINGS SHALL BE MEASURED AND PAID UNDER SPECIFICATION P-620.
  2. VSR STOP BAR MARKINGS SHALL BE APPLIED AS PERMANENT MARKINGS ONLY.
  3. PERMANENT MARKINGS REQUIRE GLASS BEADS.

## 02 VSR STOP BAR PAVEMENT MARKING

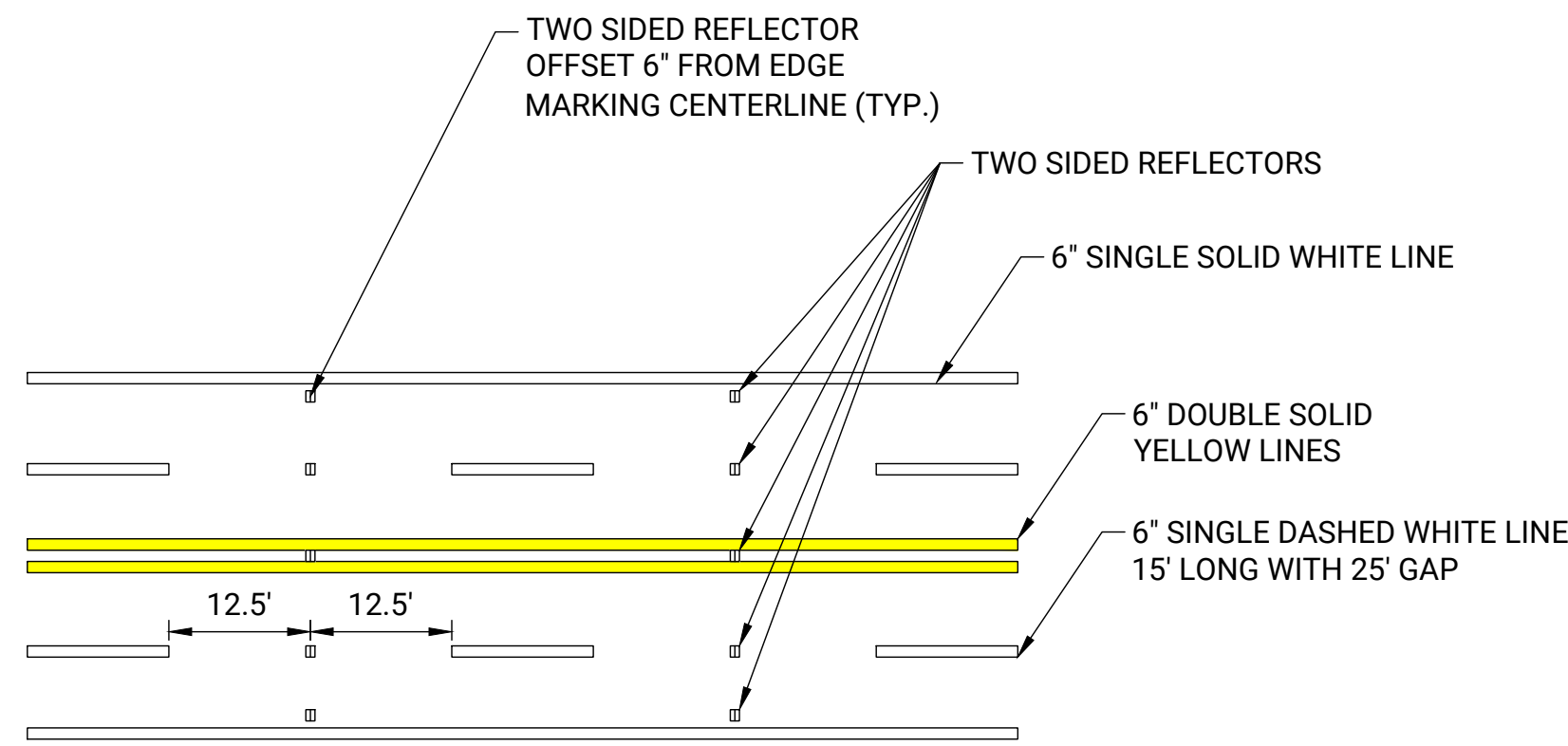
SCALE: NONE



- NOTES:
1. ALL MARKINGS SHALL BE MEASURED AND PAID UNDER SPECIFICATION P-620.
  2. CONNECTOR ROAD STOP BAR MARKINGS SHALL BE APPLIED AS PERMANENT MARKINGS ONLY.
  3. PERMANENT MARKINGS DO NOT REQUIRE GLASS BEADS.

## 03 CONNECTOR ROAD STOP BAR PAVEMENT MARKING

SCALE: NONE



- NOTES:
1. ALL MARKINGS SHALL BE MEASURED AND PAID UNDER SPECIFICATION P-620.
  2. ALL TWO SIDED REFLECTORS SHALL BE MEASURED AND PAID UNDER SPECIFICATION A-101.
  3. VSR ROAD LANE MARKINGS SHALL BE APPLIED AS TEMPORARY AND PERMANENT MARKINGS.
  4. TEMPORARY MARKINGS REQUIRED NO GLASS BEADS.
  5. PERMANENT MARKINGS REQUIRE GLASS BEADS.

## 04 VSR ROAD MARKINGS & REFLECTOR LAYOUT

SCALE: NONE

PREPARED FOR:



PREPARED BY:

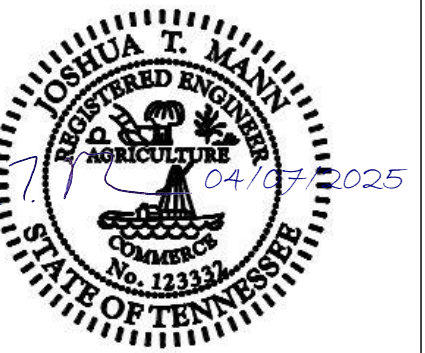


KEY MAP:

NO.	DATE	BY	REVISION DESCRIPTION

REHABILITATE VEHICLE SERVICE ROAD  
MEMPHIS INTERNATIONAL AIRPORT  
MEMPHIS, TN

PAVEMENT MARKING DETAILS



DESIGNED: WUH	DRAWN: WUH	CHECKED: JTM
PROJECT NO: 0024M300.10		
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**CM2.01**